

GOVERNMENT OF THE DISTRICT OF COLUMBIA  
Board of Zoning Adjustment



**BZA APPLICATION NO. 17963**

As Director of the Office of Zoning, I hereby certify and attest that on JUN 29 2010, a copy of the order entered on that date in this matter was mailed first class, postage prepaid or delivered via inter-agency mail, to each party who appeared and participated in the public hearing concerning the matter and to each public agency listed below:

Michael Goss  
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Washington, D.C. 20009

Koo Yuen  
P.O. Box 9492  
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Chairperson  
Advisory Neighborhood Commission 5A  
1322 Irving Street, N.E.  
Washington, D.C. 20017

Single Member District Commissioner 5A03  
Advisory Neighborhood Commission 5A  
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BOARD OF ZONING ADJUSTMENT  
District of Columbia

CASE NO. 17963

EXHIBIT NO. 36

ATTESTED BY:

  
JAMISON L. WEINBAUM  
Director, Office of Zoning

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Board of Zoning Adjustment  
District of Columbia  
CASE NO. 17963A  
EXHIBIT NO. 7

**GOVERNMENT OF THE DISTRICT OF COLUMBIA**  
**Board of Zoning Adjustment**



**Application No. 17963 of Euclid of Virginia, LLC**, for a special exception pursuant to §§ 3104, 706 and 2302, to construct a new convenience store and self-service gasoline station at the premises located at 4975 South Dakota Avenue, N.E., in the C-1 Zone District (Square 3899, Lot 76).

**HEARING DATE:** September 22, 2009

**DECISION DATES:** November 11, 2009 and December 15, 2009

**DECISION AND ORDER**

On April 22, 2009, Euclid of Virginia, LLC, filed an application with the Board of Zoning Adjustment (the "Board") for special exception relief to raze the existing convenience store, and build a new convenience store and self-service gasoline station at the property. Following a public hearing and decision meeting, the Board voted to approve the application.

**PRELIMINARY MATTERS**

**Application**

This application was filed by Euclid of Virginia, LLC, ("Euclid" or the "Applicant"), the developer, on behalf of the property owner, Koo Yuen Park. The original application sought relief under § 3104 (general special exception relief) and § 706 (special exception relief to establish a gasoline service station). (See, Application at Exhibit 1, and Narrative Statement at Exhibit 3). Pursuant to § 706.1, and at the suggestion of the Office of Planning ("OP"), the Applicant also demonstrated compliance with the standards set forth in § 2302 (additional criteria for gasoline service stations) (See, Supplemental Narrative Statement at Exhibit 22).

**Authorization**

The property owner authorized Michael Goss to appear before the Board as his agent during the proceedings. (Exhibit 5).

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**BOARD OF ZONING ADJUSTMENT**  
**District of Columbia**

CASE NO. 17963

EXHIBIT NO. 35

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**Self-Certification**

The zoning relief requested in this case was self-certified pursuant to 11 DCMR § 3113.2 (Exhibit 4).

**Notice of Application and Notice of Hearing**

The application was filed on April 22, 2009. By memoranda dated June 24, 2009, the Office of Zoning notified the following agencies that the application had been filed: the D.C. Office of Planning ("OP"), Advisory Neighborhood Commission ("ANC") 5A, the ANC for the area within which the subject property is located, and the D.C. Department of Transportation (DDOT). Pursuant to 11 DCMR 3113.3, notice of the hearing was sent to the Applicant, all entities owning property within 200 feet of the Applicant's site, the ANC, OP and DDOT. The Applicant posted placards at the property regarding the application and public hearing and submitted an affidavit to the Board to this effect. (Exhibit 25).

**ANC 5A**

The subject site is located within the area served by ANC 5A, which is automatically a party to this application. The ANC filed three written reports with the Board.

(1) In its report dated September 17, 2009, ANC 5A indicated that at a regularly scheduled and legally noticed meeting on July 22, 2009, with a quorum present, the ANC voted to oppose the application. (Exhibit 26). The ANC indicated that it had several issues and concerns with the proposed project: (1) The community does not "need" another gasoline station and an existing station is three blocks away from the proposed site; (2) The site is too small for the proposed project and will result in temporary parking and idling on the street, causing traffic congestion and road hazards; (3) The gasoline station may cause environmental problems to the community; (4) The owners do not maintain their property and there are problems with loitering and trash at the site; (5) The proposed location is already congested due to the McDonald's across the street and the existing gasoline station; (6) The community does not support a 24 hour convenience store; (7) The owners have not posted visible signs or notice for the proposed use of the property; and (8) The owners are not part of the community, do not live in the neighborhood, and are not participants in the ANC.

(2) In its report dated November 4, 2009<sup>1</sup> (following the public hearing), the ANC reiterated some of its previous concerns, with an emphasis on its concerns about traffic, safety, and congestion. The ANC also commented that it was waiting to receive a traffic study from the Applicant.

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<sup>1</sup> The report indicates that after the Board's public hearing, the ANC held additional meetings on September 23, October 5, and October 21. However, there is no indication whether these meetings were noticed in accordance with the law, or whether there was a quorum.

(3) In a report dated December 11, 2009<sup>2</sup>, the ANC indicated that an emergency meeting held on December 7, 2009, with a quorum present, the ANC voted again to oppose the special exception. The ANC again noted its concerns regarding traffic and maintained that the traffic study submitted by the Applicant was flawed.

#### **Requests for Party Status**

There were no requests for party status.

#### **Other Persons/Entities in Opposition/Support**

No persons appeared in opposition or in support at the public hearing. However, the Board received letters in opposition from the North Michigan Park Civic Association (Exhibit 24) and nearby property owners residing at 5015 10<sup>th</sup> Street, N.E. (Exhibit 30), and 4936 11<sup>th</sup> Street, N.E. (Exhibit 27). Among other things, the persons in opposition stated that there was no “need” for a gasoline station at the site, noting the existence of another gasoline station in close proximity to the one which was proposed.

#### **Government Reports**

**OP** OP filed a report indicating that it recommended approval of the special exception, subject to certain specified conditions regarding the hours of operation, the lighting at the premises, and a proposed condition prohibiting an amplified exterior sound system. (Exhibit 23). In addition, OP’s representative, Arthur Jackson, participated in the public hearing, affirming the substance of the OP written report.

**DDOT Report** DDOT had not prepared a report at the time of the public hearing. However, Jeff Jennings (a DDOT representative) testified at the hearing and asserted that the proposed project would not result in congestion or in other objectionable traffic impacts. Specifically, Mr. Jennings opined that he did not believe there would be “cut through” trips through the neighborhood as a result of the proposed gasoline station. (T. p. 144 – 145). Mr. Jennings stated that his testimony was based upon communications with the Applicant and extrapolations from a traffic study at another location. This traffic study, the “Minnesota Avenue study”, had been submitted by the Applicant for the Minnesota Avenue corridor, a location with characteristics which are “very similar” to the South Dakota Avenue location of the subject property. (Transcript, BZA Hearing of September 22, 2009, (“T.”) p. 139). After the public hearing, and at the Board’s request, DDOT submitted a formal report. (Exhibit 31). In this report, DDOT recommended approval of the application, subject to certain specified conditions. The proposed

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<sup>2</sup> The Board waived its rules to allow part of this report into the record. (The Board allowed only the first page and part of the second page of this report into the record. The remaining pages related to federal environmental standards and are beyond the Board’s jurisdiction.) The Board found that the ANC had good cause to submit a late filing, and that the Applicant was not prejudiced by the late filing because it had sufficient time to review the report and respond to it.



conditions related to the hours for fuel delivery, the size of the delivery trucks, and a specific plan for ingress and egress to and from the site, including a condition which restricted the South Dakota Avenue curb cut to a right turn into the site.

## **FINDINGS OF FACT**

### **The Site and Surrounding Area**

1. The site is located at 4975 South Dakota Avenue, N.E. in the C-1 Zone District. It is an irregularly shaped lot with frontage on Emerson Street, South Dakota Avenue, and Delafield Street, N.E.
2. There are currently three existing curb cuts at the site, two at South Dakota Avenue and one at Emerson Street.
3. The site abuts a self-service gasoline station owned by BP Oil. Across the street from the site are a Chinese food restaurant, a McDonald's restaurant, a dry cleaning establishment, and a beauty parlor.
4. There is an existing convenience store at the site.
5. The site is separated from the neighboring residential district (R-2) by Delafield and Emerson Streets. There are semi-detached one-family dwellings to the north and west across Emerson Street and Delafield Street.

### **Proposed Project**

6. The Applicant proposes to raze the existing convenience store and construct a new 1,800 square-foot building for a new convenience store and gasoline station. The gasoline station will have three pumps and a canopy over the pumps.
7. The Applicant made several refinements to the plans in response to comments from OP and DDOT. Both OP and DDOT suggested that Board approval be subject to specific conditions.
8. The Applicant provided traffic, queuing and truck maneuvering diagrams on revised plans submitted to the Board and reviewed by DDOT. The large area of the site provides for ample on-site vehicular circulation and safe pedestrian movement. The orientation of the three pump islands, all of which are parallel to South Dakota Avenue, will direct traffic away from the convenience store.
9. Subject to DDOT approval, the Applicant plans to eliminate one of the three existing curb cuts – the one at South Dakota Avenue closest to the intersection. The two remaining curb

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cuts will be located at least 40 feet from the intersection of South Dakota Avenue and Delafield Street. One of the two remaining curb cuts -- the curb cut retained along South Dakota Avenue -- will allow only a right-turn onto the site.

10. Eight parking spaces will be provided on-site, two of which will be accessible to vehicles displaying the universal symbol of accessibility. There will also be bicycle parking.
11. The hours of operation will be limited to 6:00 a.m. to 10:00 p.m.
12. The lighting at the premises will be recessed and focused downward, so as not to cause any objectionable glares onto the surrounding residential properties.
13. The existing trash enclosure at the site will be enlarged, the premises will be kept clean and free of debris, and there will be no loitering at the premises.

**The Zoning Relief Sought**

14. A convenience store is allowed in the C-1 district as a matter of right. However, a gasoline service station is subject to approval by the Board pursuant to § 706 of the Zoning Regulations.
15. Pursuant to § 706.1, a gasoline service station approved as a special exception is also subject to the provisions of Chapter 23 of the Zoning Regulations.

**Impact of the Proposed Project**

16. The proposed project will not add to traffic and congestion in the area, as patrons of the station will largely be drawn from vehicles already in the stream of traffic. The Board concludes that many of the vehicles entering the site will consist of "pass-by trips" -- they are on the road already -- and those vehicles will not contribute to additional traffic and congestion.
17. The exterior lighting at the site will be adequate for security and safety, yet will not be too intrusive to the residential properties around the site.

**Compliance with Chapter 23 Requirements**

18. The proposed project complies with all relevant provisions in Chapter 23 pertaining to gasoline stations.
  - The subject property is separated from the neighboring residential district by two streets, Delafield Street and Emerson Street. (§ 2302.2)

- The vehicular entrance to the property on Delafield Street is separated from the residential district by Delafield Street. (§ 2302.3)
- Neither of the two curb cuts is within 40 feet from the intersection of South Dakota Avenue and Delafield Street. (§ 2302.4).
- No grease pits or hoists are associated with the proposed project. (§ 2302.5) (OP Report, p. 4 - 5).

### **CONCLUSIONS OF LAW**

The Board is authorized under the Zoning Act of June 20, 1938 (52 Stat. 797, as amended, D.C. Code § 6-641.07(g)(2) (2001), to grant special exceptions as provided in the Zoning Regulations. The Applicant applied under 11 DCMR § 3104.1 for a special exception pursuant to 11 DCMR § 706 to establish a gasoline service station use.

The Board can grant a special exception where, in its judgment, two general tests are met and the special conditions for the particular exception are met. First, the requested special exception must "be in harmony with the general purpose and intent of the Zoning Regulations and Zoning Maps." 11 DCMR § 3104.1. Second, it must "not tend to affect adversely the use of neighboring property in accordance with the Zoning Regulations and Zoning Map." 11 DCMR § 3104.1.

The Board concludes that the gasoline station and convenience store will be in harmony with the general purpose and intent of the Zoning Regulations and Zoning Maps. A gasoline service use is permitted by special exception and there is nothing about this area or this proposed use that would result in disharmony.

Further, the Board finds that the proposed project, as conditioned, will not tend to adversely affect the use of neighboring property in accordance with the Zoning Regulations and Maps. The Board agrees with DDOT and the Applicant's traffic expert that no significant additional traffic will result from the use. As noted in Finding of Fact 16, the vast majority of customers will be in vehicles already on the road. Such traffic as does exist will easily access and egress the facility as a result of the conditions of this order mandating a 20-foot curb cut, specifying the direction of vehicles, limiting fuel delivery times, and regulation the size of delivery vehicles. The lighting will be pointed downward and the canopy lighting recessed. This will effectively avoid light spillage into adjacent areas. As to noise, the Board has included a condition prohibiting any external sound amplification system.

A gasoline station established after May 12, 1958, is permitted by special exception in the C-1 Zone District, provided certain specified conditions are met. Under § 706.1 of the Zoning Regulations, the Board may allow a gasoline station in the C-1 Zone, subject to the following provisions:

**706.3 The station shall not be located within twenty five feet (25 ft.) of a Residence District unless separated from the Residence District by a street or alley.** The subject property is separated from the neighboring R-2 district by Delafield and Emerson Streets.

**706.4 The operation of the use shall not create dangerous or other objectionable traffic conditions.** The subject property is in a heavily trafficked area. But as DDOT found and as detailed in the Findings of Fact, the proposed project, as conditioned, will not generate significant additional traffic. Nor will it result in any objectionable traffic conditions. (Finding of Fact 16). Moreover, the Applicant has made design changes which should improve the circulation and traffic flow in and out of the site. The revisions include, for example, the elimination of one curb cut, the restriction of one curb cut to a right turn entrance only, and limitations on the hours for fuel delivery and the size of fuel delivery trucks.

**706.6 Required parking spaces may be arranged so that all spaces are not accessible at all times. All parking spaces provided under this subsection shall be designed and operated so that sufficient access and maneuvering space is available to permit the parking and removal of any vehicles without moving any other vehicle onto public space.** The building on the site will contain approximately 1,800 square feet of gross floor area. Therefore, it will require six parking spaces. Eight parking spaces will be provided and are designed to be accessible at all times.

#### **Compliance with Chapter 23 provisions**

Subsection 706.1 of the Regulations provides that a gasoline service station established after May 12, 1958 is also subject to the provisions of Chapter 23. As explained in the Findings of Fact, the proposed project complies with all relevant provisions in Chapter 23. Therefore, this condition has been met.

#### **The ANC Issues and Concerns**

The Board is required under § 3 of the Comprehensive Advisory Neighborhood Commissions Reform Act of 2000, effective June 27, 2000 (D.C. Law 13-135, D.C. Code § 1-309.10(d)(3)(A), to give “great weight” to the issues and concerns raised by the affected ANC. To give “great weight,” the Board must articulate with particularity and precision the reasons why the ANC does or does not offer persuasive advice under the circumstances, and make specific findings and conclusions with respect to each of the ANC’s issues and concerns. As will be explained below, the Board does not find the ANC’s advice to be persuasive.

The ANC's primary concerns were potential problems with traffic and congestion. The Board had concerns regarding these issues as well. Ultimately, however, the Board agreed with the Applicant and found that the project would not result in adverse traffic conditions or additional congestion. Following the public hearing, the Board held the record open to receive additional information regarding traffic impacts. Thus, post-hearing submissions included a formal DDOT report and a traffic study performed by an expert traffic engineering firm.

While the ANC claimed that the traffic study was flawed, the ANC provided no evidence to that effect, and the Board found the traffic study to be persuasive. The Board does not believe that the study was flawed, as claimed by the ANC. While the Board does not agree with each and every assumption in the study, the Board finds that the study was thorough and was performed by an expert in the traffic engineering field. (T., December 15, 2009, p. 32 – 33)

Similarly, before the Board relied on Mr. Jennings' opinion (the DDOT representative who testified in support of the application), the Board closely questioned him regarding his testimony and carefully scrutinized the DDOT report. The Board agrees with DDOT that this approval needs to be conditioned in order to mitigate potential traffic impacts and congestion. Therefore, this approval is conditioned upon the elimination of one curb cut, a right turn in and maximum size restriction at one of the curb cuts, a limitation on the truck size for trucks making fuel deliveries, and a limitation on the hours for fuel deliveries.

The other ANC concerns raised were as follows. The ANC claimed that the Applicant had not posted visible signs or public notices regarding the planned development. However, as detailed at the beginning of this Decision, the Applicant complied with the notice requirements under the Zoning Regulations. The ANC asserts that the site does not have "adequate square footage" for the proposed use. However, there are no minimum area requirements for this use in the C-1 Zone District, and the relevant special exception requirements have been met. The ANC raised concerns of possible environmental problems and problems with cleanliness and loitering at the site. Again, these claims were entirely speculative and unsupported by the evidence. Moreover, even if true, loitering and failure to maintain the site are enforcement issues which are beyond the purview of the Board. Finally, the ANC claimed that there is no "need" for another gasoline station since there is a station across the street; and, the property owners do not live in the neighborhood and are not participants in community events or the ANC. Neither of these concerns is relevant to the zoning criteria and will, therefore, not be given great weight. *Bakers Local Union No. 118 v. District of Columbia Bd. of Zoning Adjustment*, 437 A.2d 176 (D.C. 1981).

### **The OP Recommendations**

The Board is also required under D.C. Official Code § 6-623.04 (2001) to give "great weight" to OP recommendations. For reasons explained in this Decision, the Board agrees with the recommendations contained in the OP report.

The Board concludes that the Applicant has satisfied the burden of proof with respect to the application for a special exception under § 706 to allow the construction of a gasoline service station and convenience store. The Board further concludes that, as hereinafter conditioned, the special exception can be granted as being in harmony with the general purpose and intent of the Zoning Regulations and Map and that granting of the requested relief will not tend to adversely affect the use of neighboring property in accordance with the Zoning Regulations and Map.

It is therefore **ORDERED** that the application, pursuant to Exhibit No. 9 (plans), is hereby **GRANTED, SUBJECT** to the following **CONDITIONS**:

1. The hours of operation shall be limited to 6:00 a.m. to 10:00 p.m.
2. Flood lights at the property shall be angled downward and shuttered in order to avoid light spillage onto nearby residential properties.
3. Canopy lighting at the property shall be recessed.
4. There shall be no exterior amplified sound system at the property.
5. Hours for fuel delivery shall be limited to 7:00 p.m. to 7:00 p.m.
6. Fuel deliveries shall be made with trucks that are no larger than 30 feet in length.
7. The South Dakota Avenue curb cut will be 20 feet wide and will be restricted to a right turn onto the property. All vehicles exiting the property will use the curb cut along Emerson Street. The Emerson Street curb cut will serve as a two directional vehicle access point.

**VOTE:**        **4-0-1** (Marc D. Loud, Shane L. Dettman, Meridith H. Moldenhauer, and Peter G. May to Approve; No other Board member (vacant) participating)

Vote taken on December 15, 2009

**BY ORDER OF THE D.C. BOARD OF ZONING ADJUSTMENT**

A majority of the Board members approved the issuance of this Decision and Order.

ATTESTED BY:   
JAMISON L. WEINBAUM  
Director, Office of Zoning

FINAL DATE OF ORDER: JUN 29 2010

UNDER 11 DCMR § 3125.9, "NO DECISION OR ORDER OF THE BOARD SHALL TAKE EFFECT UNTIL TEN DAYS AFTER HAVING BECOME FINAL PURSUANT TO THE SUPPLEMENTAL RULES OF PRACTICE AND PROCEDURE FOR THE BOARD OF ZONING ADJUSTMENT."

PURSUANT TO 11 DCMR § 3130, THIS ORDER SHALL NOT BE VALID FOR MORE THAN TWO YEARS AFTER IT BECOMES EFFECTIVE UNLESS, WITHIN SUCH TWO-YEAR PERIOD, THE APPLICANT FILES PLANS FOR THE PROPOSED STRUCTURE WITH THE DEPARTMENT OF CONSUMER AND REGULATORY AFFAIRS FOR THE PURPOSES OF SECURING A BUILDING PERMIT.

PURSUANT TO 11 DCMR § 3125, APPROVAL OF AN APPLICATION SHALL INCLUDE APPROVAL OF THE PLANS SUBMITTED WITH THE APPLICATION FOR THE CONSTRUCTION OF A BUILDING OR STRUCTURE (OR ADDITION THERETO) OR THE RENOVATION OR ALTERATION OF AN EXISTING BUILDING OR STRUCTURE, UNLESS THE BOARD ORDERS OTHERWISE. AN APPLICANT SHALL CARRY OUT THE CONSTRUCTION, RENOVATION, OR ALTERATION ONLY IN ACCORDANCE WITH THE PLANS APPROVED BY THE BOARD.

PURSUANT TO 11 DCMR § 3205, FAILURE TO ABIDE BY THE CONDITIONS IN THIS ORDER, IN WHOLE OR IN PART, SHALL BE GROUNDS FOR THE REVOCATION OF ANY BUILDING PERMIT OR CERTIFICATE OF OCCUPANCY ISSUED PURSUANT TO THIS ORDER.

D.C. HUMAN RIGHTS ACT OF 1977, AS AMENDED, D.C. OFFICIAL CODE § 2-1401.01 ET SEQ., 9 (ACT) THE DISTRICT OF COLUMBIA DOES NOT DISCRIMINATE ON THE BASIS OF ACTUAL OR PERCEIVED RACE, COLOR, RELIGION, NATIONAL ORIGIN, SEX, AGE, MARITAL STATUS, PERSONAL APPEARANCE, SEXUAL ORIENTATION, FAMILIAL STATUS, FAMILY RESPONSIBILITIES, MATRICULATION, POLITICAL AFFILIATION, DISABILITY, SOURCE OF INCOME, OR PLACE OF RESIDENCE OR BUSINESS. SEXUAL HARASSMENT IS A FORM OF SEX DISCRIMINATION WHICH IS ALSO PROHIBITED BY THE ACT. IN ADDITION, HARASSMENT BASED ON ANY OF THE ABOVE PROTECTED CATEGORIES IS ALSO PROHIBITED BY THE ACT. DISCRIMINATION IN VIOLATION OF THE ACT WILL NOT BE TOLERATED. VIOLATORS WILL BE SUBJECT TO DISCIPLINARY ACTION. THE FAILURE OR REFUSAL OF THE APPLICANT TO COMPLY SHALL FURNISH GROUNDS FOR THE DENIAL OR, IF ISSUED, REVOCATION OF ANY BUILDING PERMITS OR CERTIFICATES OF OCCUPANCY ISSUED PURSUANT TO THIS ORDER.

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**ADVISORY NEIGHBORHOOD COMMISSION 5A****SLOWE SCHOOL DEMOUNTABLE****1322 IRVING STREET, N.E. ▼ WASHINGTON, D.C. 20017****TELEPHONE: 202-635-6563 ▼ FAX: 202-635-6565 ▼ Email: ANC5ADC@aol.com**Officer*Angel Alston*  
*Chair**Janae Grant*  
*Vice- Chair**Sandi Washington*  
*Treasurer**Patricia Roberts*  
*Corresponding Sectary**John Feeley*  
*Recording Secretary**Robert King*  
*Parliamentary*

Date: December 11, 2009

To: Chairman Marc D. Loud  
Board of Zoning Adjustment  
441 4th Street NW, Suite 220  
Washington, DC 20001Ref: # 17963 Application of Euclid of Virginia 4975 South Dakota  
Avenue, N.E. (Square 3899, Lot 76).BOARD OF ZONING ADJUSTMENT  
District of ColumbiaCASE NO. 17963EXHIBIT NO. 34

Advisory Neighborhood Commission 5A held an Emergency Community Meeting on Monday, December 7<sup>th</sup> at 7:30pm and with a quorum unanimously voted 10 to 0 to support the community in its efforts to oppose applied for (Special Exceptions) variances pursuant to 11 DCMR § 3104.1, which would allow the special exception to place a new self-service gasoline station and convenience store under section 706, in the C-1 District at premises 4975 South Dakota Ave, NE, WDC 20017.

Commissioners voting "Yes": Sandi Washington, Angel Alston, Herman Preston, John Feeley, Jr., Timothy Thomas, Shirley Smith, Phillip Blair, Janae Grant, Robert King

Commissioners absent: Carolyn Steptoe and Stephen McCoy

With proper notification, ANC Commissioner Angel Alston held a Joint Single Member District meeting on Friday, December 4<sup>th</sup> June 23, 2009 with 5A03 and 5A04. At this meeting, members of the community had a chance to review and discuss the traffic study that was presented by the applicant and voted against granting the special variances at 4975 South Dakota Avenue, N.E.

Chairman Marc D. Loud ANC 5A request that you support us as we asupport the residents in the affected area based on the following reason:

First, the community has great concerns that the traffic study that was produced was flawed. There is no possible way that the conclusions of the traffic study accurately identifies with the actual traffic patterns of this site. The vehicular traffic that was reported numbers grossly under represent the amount of traffic that we experience on a daily basis. We encourage the Board to pay close attention and notice that the numbers would not even partially represent the traffic on a major thoroughfare such as South Dakota on a holiday. In addition to this flawed study, vehicular traffic is such a huge problem to the extent that residents on every street within a 5 block radius have requested and have been granted speed bumps in an attempt to calm traffic. Granting this special exception would therefore increase traffic and air pollution in the area. The community has issues with the kinds of venting systems on the tanks, their size, who is

Ft. Totten\* Lamond-Riggs\* North Michigan Park\* Michigan Park\* Brookland\* Queens Chapel\* Woodridge\* Gateway\* Fort Lincoln



responsible for clean-ups, possible leaks, and who is financially responsible for these issues and land remediation. Underground leaks; If one of the tanks leaks, who pays for the clean-up - you, or the gas company who sold you the franchise? New equipment; If every station in your state is suddenly required by law to install a new kind of vent for underground tanks, who will have to pay for that equipment if you're an independent? Site remediation; If you sell your station, who pays for removing the underground tanks, cleaning up the soil and getting the certification that states your property's remediation (clean up) has been approved by the state?

- This project would be detrimental to our community and would pose great traffic and pedestrian concerns to our community. (supported by documentation from our PSA Lt. Rogers for 4D)
- We have also requested from DDOT the overhead study that was done and they have not been responsive
- We have also requested the data from DDOT that the Traffic Study was completed and they cannot produce any documentation.
- We have also requested speeding cameras as an alternative traffic calming method and it has been suggested by the police officers in this vicinity.
- Please look at the violations below to note that this company is currently under violations for environmental issues in the tri-state area.

**PHILADELPHIA, Pennsylvania, March 19, 2008 (ENS)** - The U.S. EPA's Environmental Appeals Board has ordered a company that owns gas stations in the mid-Atlantic states to pay a \$3.16 million penalty for violations involving 72 underground fuel storage tanks at 23 gas stations.

In a ruling announced today, the board upheld the agency's enforcement action against the owner of Lowest Price gas stations in Maryland, Virginia and the District of Columbia.

In cooperation with state and DC officials, the EPA filed a complaint in September 2002 against gas station owner Euclid of Virginia, Inc. for violating regulations designed to detect and prevent fuel leaks from underground storage tanks.

In a March 11, 2007 decision, the board ruled against every issue raised in an appeal filed by Euclid of Virginia, Inc.

***A leaking  
underground  
storage tank is  
excavated -  
not at a Euclid-  
owned station.***

*(Photo courtesy EPA)*

The company had appealed an administrative law judge's November 2006 assessment of a \$3.08 million penalty for these violations - the largest penalty ever assessed by an EPA administrative law judge for violations of any federal environmental law.

Instead, the board ruled in favor of EPA's cross-appeal against Euclid, increasing this precedent-setting penalty to \$3,164,555.

"With millions of gallons of gasoline, oil, and other petroleum products stored in underground tanks, leaving them unchecked can cause major soil and groundwater

contamination," said Donald Welsh, regional administrator of EPA's mid-Atlantic region.

Leaking tanks are a major source of soil and groundwater contamination. EPA and EPA-authorized state regulations are designed to reduce the risk of underground leaks, and avoid the costs of major cleanups.

"This decision should send a strong message to owners of underground storage tanks that it is not only in the public's best interest but in their own, too, to comply with leak detection and prevention requirements," said Welsh.

The violations involved 14 gas stations in Maryland, two in Virginia, and seven in the District of Columbia.

The board found that the EPA had proved that Euclid failed to maintain required leak detection and control equipment, failed to perform required leak detection activities and failed to comply with corrosion-prevention standards.

The company also was found to have failed to properly install or maintain equipment to prevent releases of gasoline due to the overfilling of tanks or other spills when tanks are being filled, and failed to maintain required financial assurances.

The size of the penalty is due not only to the large number of facilities and underground storage tanks involved, but also to Euclid's repeated non-compliance with the same regulations over periods that often lasted for several years.

The administrative law judge cited the breadth of the violations, Euclid's "high degree of negligence" and its overall record of non-compliance in allowing violations to continue despite numerous warnings from the EPA and the Maryland, Virginia and District of Columbia state environmental agencies as further justification for the substantial penalty.

The state and DC agencies coordinated with the EPA to conduct numerous inspections of Euclid-owned gas stations, and inspectors from each agency served as witnesses at the trial.

Euclid has the right to appeal the board's decision again, this time in federal circuit court.

<http://www.ens-newswire.com/ens/mar2008/2008-03-19-094.asp>

<http://www.epa.gov/oalj/orders/euclidof-va-id-110906.pdf> 118 pages of

***Precedent-Setting Penalty Upheld for Underground Fuel Storage Tank Violations at 23 Gas Stations in Maryland, Virginia and District of Columbia; Owner of Gas Stations to Pay \$3.16 Million for Violations***

Release date: 03/18/2008

Contact Information: Donna Heron 215-814-5113 /heron.donna@epa.gov

**PHILADELPHIA (March 18, 2008)** -- EPA's Environmental Appeals Board (EAB) has upheld the agency's enforcement action against the owner of Lowest Price gas stations in Maryland, Virginia and the District of Columbia. In cooperation with state and D.C. officials, EPA filed a complaint in September 2002 against gas station owner, Euclid of Virginia, Inc. for violating regulations designed to detect and prevent fuel leaks from underground storage tanks (USTs).

In a March 11, 2008 decision, the EAB ruled against every issue raised in an appeal filed by Euclid of Virginia, Inc. The board ordered the company to pay a \$3,164,555 penalty for violations involving 72 underground storage tanks at 23 gas stations. The company had appealed an administrative law judge's November 2006 assessment of a \$3.08 million penalty for these violations -- the largest penalty ever assessed by an EPA administrative law judge for violations of any federal environmental law.

The board ruled in favor of EPA's cross-appeal against Euclid, increasing this precedent-setting penalty to \$3,164,555. The EAB overturned the administrative law judge's rulings against EPA on three counts involving inventory control violations, and imposed the proposed \$79,262 penalty for these counts.

"With millions of gallons of gasoline, oil, and other petroleum products stored in underground tanks, leaving them unchecked can cause major soil and groundwater contamination," said Donald S. Welsh, regional administrator of EPA's mid-Atlantic region. "This decision should send a strong message to owners of underground storage tanks that it is not only in the public's best interest but in their own, too, to comply with leak detection and prevention requirements."

The violations involved 14 gas stations in Maryland (in Baltimore, Brentwood, Camp Spring, District Heights, Frederick, Hyattsville, two facilities in Landover Hills, Langley Park, Mitchellville, Palmer Park, Pasadena, Silver Spring, Trappe), two in Virginia (located in Chantilly and Ruckersville) and seven in the District of Columbia.

The EAB ruled that EPA had proven that Euclid failed to maintain required leak detection and control equipment, failed to perform required leak detection activities, failed to comply with corrosion-prevention standards and conduct cathodic protection testing, failed to properly install or maintain equipment to prevent releases of gasoline due to the overfilling of tanks or other spills when tanks are being filled, and failed to maintain required financial assurances.

The size of the penalty was due not only to the large number of facilities and underground storage tanks involved, but also to Euclid's repeated non-compliance

with the same regulations over periods that often lasted for several years. The administrative law judge also cited the breadth of the violations, Euclid's "high degree of "negligence" and its overall record of non-compliance in allowing violations to continue despite numerous warnings from EPA and the Maryland, Virginia and District of Columbia state environmental agencies as further justification for a substantial penalty. The state and D.C. agencies coordinated with EPA to conduct numerous inspections of Euclid-owned gas stations, and inspectors from each agency served as witnesses at the trial.

With millions of gallons of gasoline, oil, and other petroleum products stored in underground tanks throughout the U.S., leaking tanks are a major source of soil and groundwater contamination. EPA and EPA-authorized state regulations are designed to reduce the risk of underground leaks, and thus avoid the costs of major cleanups.

EPA's mid-Atlantic region has recently focused enforcement on owners of underground storage tanks at multiple facilities. In several instances, owners of multiple facilities have entered into agreements with EPA to conduct audits of their facilities, with reduced penalties for violations discovered during such audits. For more on EPA's UST program, including compliance assistance information, visit <http://www.epa.gov/swerust1/overview.htm>

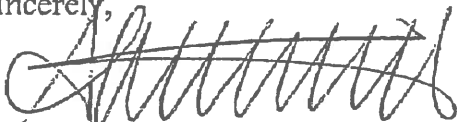
The gas stations involved included: in Maryland (Baltimore, Brentwood, Camp Spring, District Heights, Frederick, Hyattsville, Landover Hills, Langley Park, Mitchellville, Palmer Park, Pasadena, Silver Spring, and Trappe), in Virginia (Chantilly and Ruckersville); and in the District of Columbia.

Euclid has the right to appeal the EAB decision to the federal circuit court. The board's decision is available at:

[http://yosemite.epa.gov/oa/EAB\\_Web\\_Docket.nsf/Recent~Additions/AA99D35BC3FC069085257409006D2BD5/\\$File/Final....pdf](http://yosemite.epa.gov/oa/EAB_Web_Docket.nsf/Recent~Additions/AA99D35BC3FC069085257409006D2BD5/$File/Final....pdf)

In closing, ANC 5A request that the Board of Zoning Adjustment supports the request of the community by denying the request of the applicant their request to place a gas station at this location. The community is adamantly against this project as it is truly not beneficial in anyway. With the recent information regarding the environmental issues that this particular company is guilty of, we cannot afford to make zoning changes to accommodate a company that obviously violates the health and welfare of the communities that surrounds their gas stations and limits the future possibility of land usage at this location.

Sincerely,



Angel Alston, Chair 5A  
202-556-5958 cell  
202-315-6057 cell  
202-635-6565 fax  
[angel.alston@anc.dc.gov](mailto:angel.alston@anc.dc.gov)

2009 DEC 15 AM 9:27

RECEIVED  
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ANC 5A  
1322 IVRIVING STREET NE  
WASHINGTON DC 20017  
202-635-6563 OFFICE  
202-635-6563 FAX

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D.C. OFFICE OF ZONING

2009 DEC 15 AM 9:27

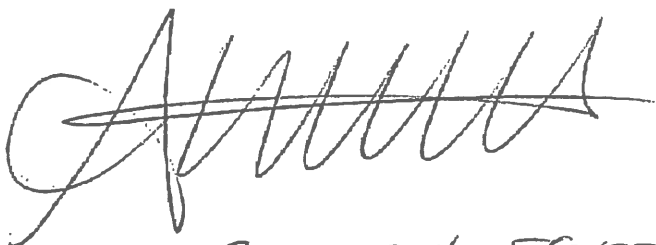
# FAX

To:	BOARD OF ZONING ADJUSTMENT	From:	COMMISSIONER ANGEL ALSTON
Fax:	202 7276072	Pages:	[Type number of pages]
Phone:	202 727 6311	Date:	12.14.2009
Re:	17962 APPLICATION OF EXCLUDED OF VIRGINIA CASE # 17963	CC:	[Type text]

☐ Urgent ☐ For Review ☐ Please Comment ☐ Please Reply ☐ Please Recycle

Comments:

PLEASE ADD THIS TO THE FILE ASAP. THIS IS REGARDING THE HEARING TOMORROW

  
202-556-5958

12.14.2009

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2009 NOV 10 PM 4: 27

BZA # 17963

TRANSPORTATION STUDY  
4975 SOUTH DAKOTA AVENUE, NE  
WASHINGTON, D.C.

Prepared For:  
G2 Design

October 6, 2009

Project Managers: Mike Nalepa

STS Job No.: 5794

BOARD OF ZONING ADJUSTMENT  
District of Columbia

CASE NO. 17963

EXHIBIT NO. 33

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## APPENDICES

- A VEHICLE TURNING MOVEMENT COUNTS
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- F TRUCK TURNING PATH

## INTRODUCTION

G2 Design is proposing to re-development the property at 4975 South Dakota Avenue with a three (3) gas pump gas station with a convenience market. The existing site currently supports a convenience store served with two (2) full movement access points on South Dakota Avenue and one (1) full movement access on Emerson Street. Street Traffic Studies, Ltd. (STSLTD) has prepared this transportation study related to this development.

The objective of the study was to evaluate the impact the proposed development will have on the road network in the vicinity of the subject site. The methodology used for the analysis followed direction provided by the District Department of Transportation (DDOT).

The principal scope of services performed as part of this study included:

- o Field inspection of the road network in the vicinity of the site to determine the existing pavement widths and traffic controls.
- o Conduct manual turning movement traffic counts at the South Dakota Avenue/Emerson Street-Delafield Street intersection as well as at the existing access points.
- o Trip generation and distribution analyses for the proposed use.
- o Analysis of the above data to determine the impact of the proposed development on the road network under study.
- o Evaluate the truck access to the site.
- o Preparation of a report detailing the results of the field studies and analyses performed.

## BASE CONDITIONS

The 4975 South Dakota Avenue site currently supports a convenience store which will be replaced by a gas station/convenience store with three (3) gas pumps. Access to the site is currently provided by two (2) full movement access points on South Dakota Avenue and one (1) full movement access point on Emerson Street. Based on comments from DDOT the driveway access point on South Dakota Avenue nearest the South Dakota Avenue/Emerson Street-Delafield Street intersection will be closed and the remaining South Dakota Avenue access point will be restricted to right turns in only. The full movement access on Emerson Street will remain. The site location is shown in Exhibit 1.

### Existing Road Network

Based upon the direction provided by DDOT, the intersections required to be analyzed as part of this report are as follows:

- South Dakota Avenue @ Emerson Street-Delafield Street
- Site Access Points

Field inspection of the intersections under study revealed the existing lane uses as illustrated in Exhibit 2.

South Dakota Avenue is designated as a Principal Arterial roadway and is posted with a 25 MPH speed limit. The most recent ADT data on the DCDOT website shows that South Dakota Avenue had an ADT of 18,200 vehicles per day at a location north of Gallatin Street which is approximately 1,060 feet north of the site; there were no ADT volumes provided along Emerson Street. A general rule of thumb in calculating ADT volumes from peak hour volumes is to assume that the peak hour volume represents 10% of the daily total. This generally provides a conservative estimation of daily volumes and in this case the projected ADT volume along South Dakota Avenue in front of the site would be 15,660 vehicles per day, while the ADT along Emerson Street would be 1,240 vehicles per day.

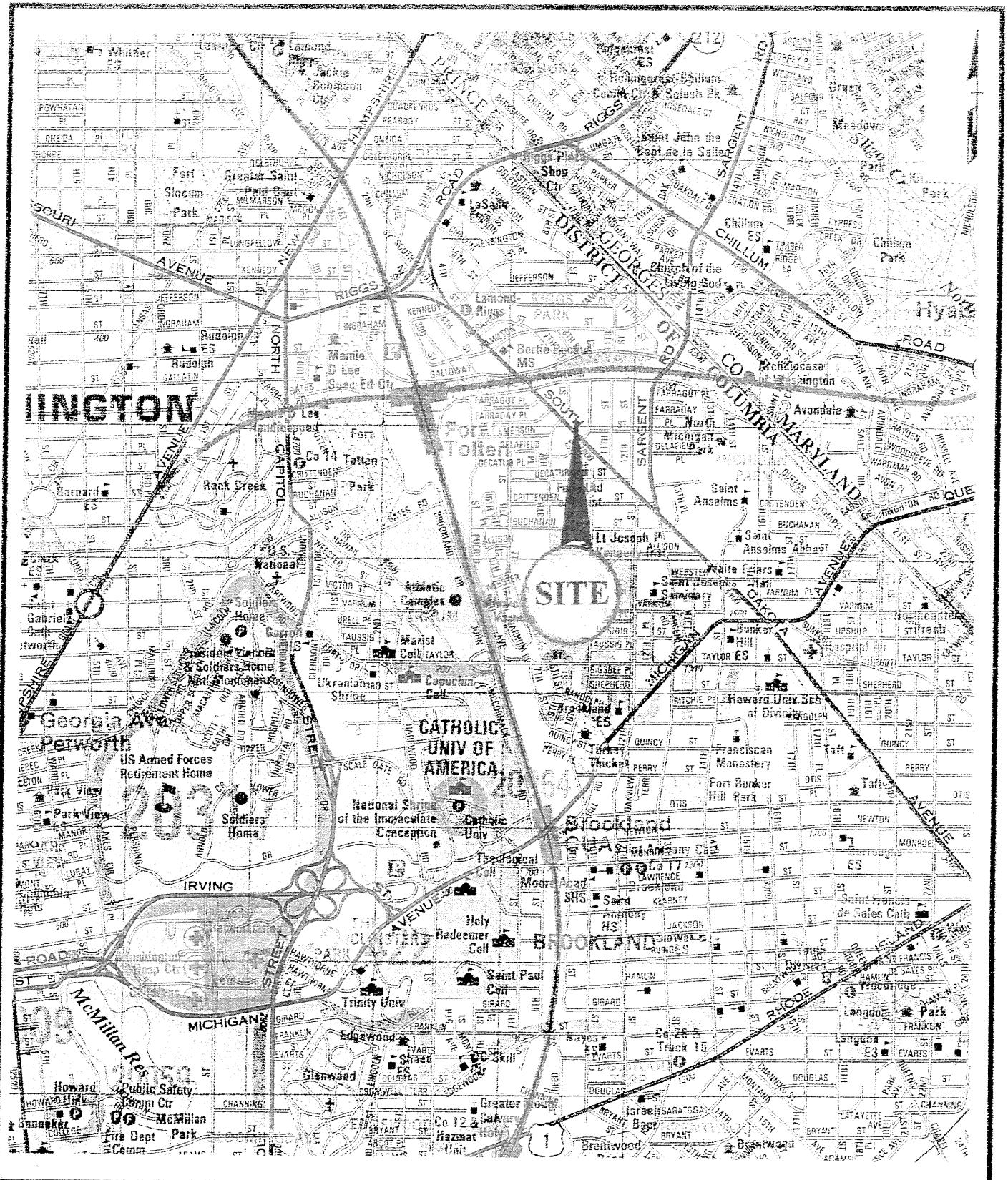
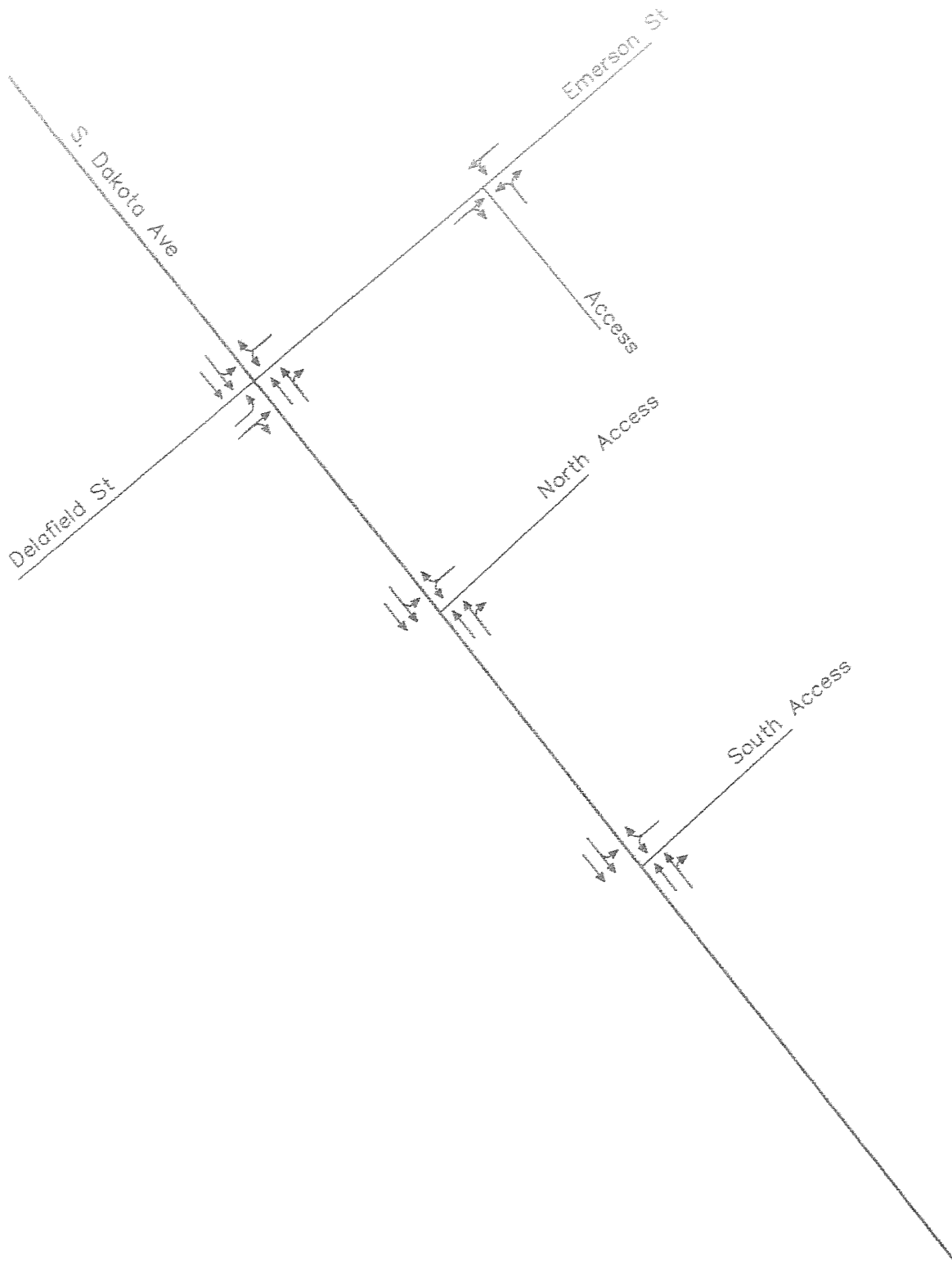


EXHIBIT 1  
SITE LOCATION

SCALE: 1" = 2000'



NO SCALE

EXHIBIT 2  
EXISTING LANE USE

Emerson Street is designated as a Local roadway and also has a posted speed limit of 25 MPH.

DCDOT requested that a review be conducted of the access points along South Dakota Avenue 2,000 feet from the site in both directions. A condition diagram is enclosed in Appendix B which shows all of the access points along both sides of South Dakota Avenue. The field review also revealed that within this area the only area with commercial land uses are along both sides of South Dakota Avenue south of Emerson Street-Delafield Street.

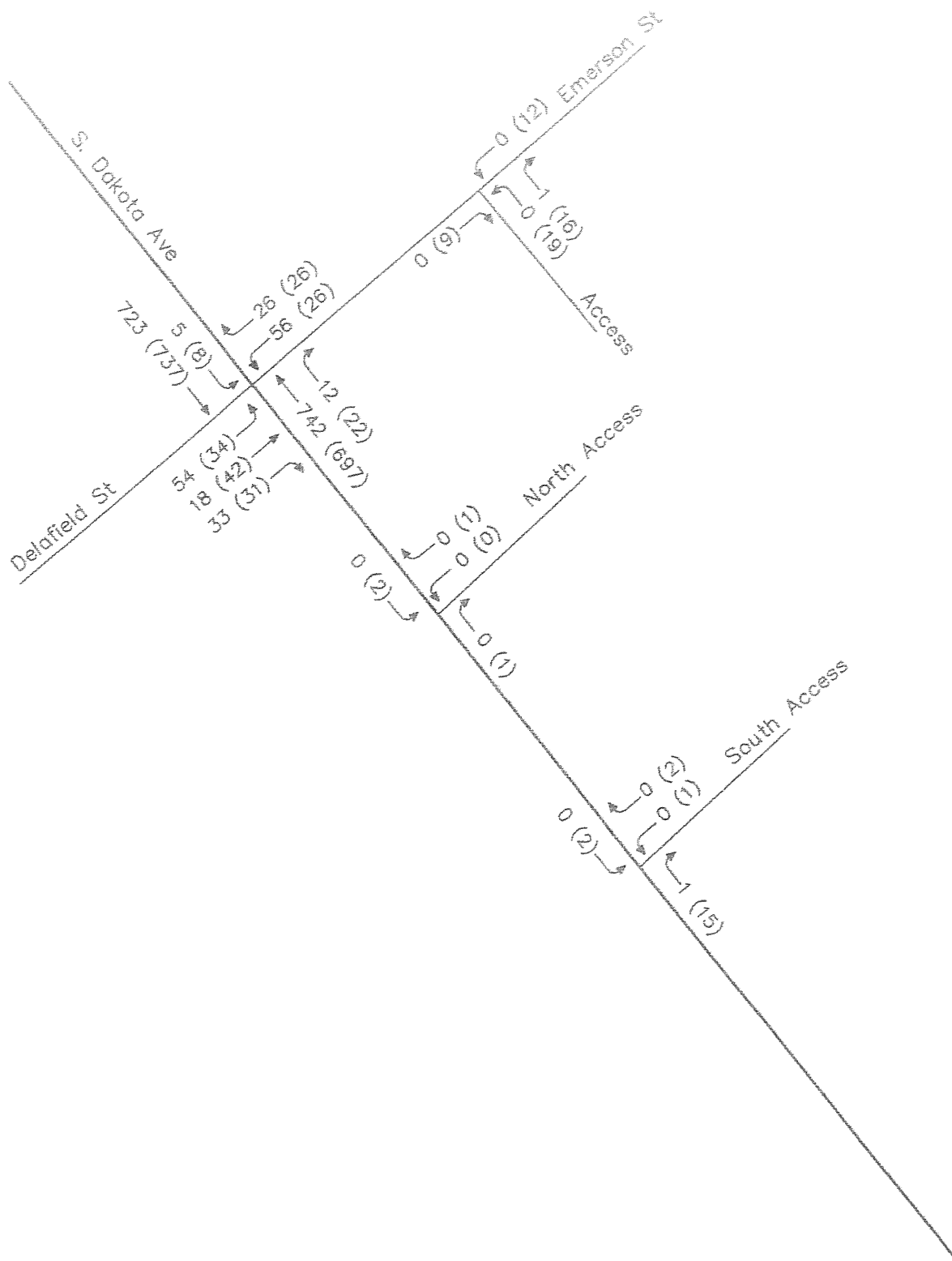
The South Dakota Avenue/Emerson Street-Delafield Street intersection is controlled with signalization. A copy of the signal timing sheets for this intersection as well as the signalized intersections with 12<sup>th</sup> Street, Sargent Road and Webster Street are enclosed in Appendix C.

#### Existing Traffic Volumes

STS LTD conducted manual turning movement counts on Tuesday, September 29, 2009, between the hours of 7:00 - 9:00 AM and 4:00 - 7:00 PM to determine the existing traffic conditions. The existing peak hour volumes are shown in Exhibit 3, and the summarized data sheets showing the total vehicles observed are contained in Appendix A.

#### Analysis of Existing Traffic Conditions

Intersection capacity analyses were performed, applying the Highway Capacity Manual procedures to the existing volumes. The results of the analyses are presented in Table 1.



NO SCALE

0000 - MORNING PEAK HOUR  
(0000) - EVENING PEAK HOUR

### EXHIBIT 3 EXISTING TRAFFIC VOLUMES

TABLE 1  
RESULTS OF CAPACITY ANALYSES  
EXISTING TRAFFIC CONDITIONS

<u>INTERSECTION</u>	<u>MORNING PEAK HOUR</u>	<u>EVENING PEAK HOUR</u>
S. Dakota Ave @ Emerson St		
EB LT	D(40.1)	D(39.5)
EB Thru/Right	D(40.2)	D(41.0)
WB Left/Rights	D(42.6)	D(40.7)
NB Thrus/Rights	A( 7.3)	A( 7.2)
SB Left/Thrus	A( 7.3)	A( 7.5)
Intersection LOS	B(11.1)	B(10.6)

X(0000) - Level of Service(Control Delay)

A review of the results shown in Table 1 reveal that the intersection is currently operating at acceptable levels of service under existing conditions. The capacity calculations are contained in Appendix D.



## SITE TRAFFIC ANALYSIS

The 4975 South Dakota Avenue site currently supports a convenience store use which will be replaced with a gas station/convenience store use with three (3) gas pumps. Access to the site is currently provided by two (2) full movement access points on South Dakota Avenue and one (1) full movement access point on Emerson Street. Based on comments from DDOT the driveway access point nearest the South Dakota Avenue/Emerson Street-Delafield Street intersection will be closed and the remaining access on South Dakota Avenue will be changed to a right turn in only access.

### Trip Generation

To determine the impact of the proposed use, STS LTD reviewed the trip generation categories found in the ITE publication Trip Generation, Eight Edition. Land uses such as gas stations/convenience stores attract a large percentage of their trips from traffic already on the road network. These "Pass-by Trips" were estimated based on the ITE Trip Generation Handbook, Second Edition. Furthermore, the ITE rates are generally based on suburban uses where transit and pedestrian options are limited. In an area such as the proposed site, pedestrian/bicyclists access to at least the convenience store use would in effect reduce the volume of trips expected to be generated by this use. No adjustments were made to the ITE rates to reflect the higher than normal pedestrian volumes in the area or the presence of the bus stops along South Dakota Avenue; therefore the projected trips generated by this site are higher than what would be expected resulting in a conservative analysis of future traffic conditions. The trips generated by the existing convenience store use are also shown. The trip generation rates and the trips projected to be generated by the proposed use are shown in Table 2.

TABLE 2  
TRIP GENERATION  
4975 SOUTH DAKOTA AVENUE SITE

<u>LAND USE</u>	<u>MORNING PEAK HOUR</u>			<u>EVENING PEAK HOUR</u>		
	<u>IN</u>	<u>OUT</u>	<u>TOTAL</u>	<u>IN</u>	<u>OUT</u>	<u>TOTAL</u>
Gas Station/Con. Store (945)						
Trips/Fueling Station	5.08	5.08	10.16	6.69	6.69	13.38
Trips/6 Fueling Stations	31	30	61	40	40	80
Pass-by Trips (66%)	20	20	40	26	26	52
New Trips (34%)	11	10	21	14	14	28
Existing Site Trips	1	1	2	40	39	79
Net Change	+30	+29	+59	NC	+1	+1

#### Trip Distribution

Since the existing convenience store and the proposed gas station/convenience store are similar "convenience" type uses; the existing trip distributions should be representative of the proposed trip distributions. Although the existing convenience store did not open until approximately 9:00 AM on the day the counts were conducted; it was open during the evening peak hour and these distributions were used for both the morning and evening peak hours of the proposed use.

The neighborhood surrounding the South Dakota Avenue/Emerson Street-Delafield Street intersection are well established and do not lend themselves to cut-through commuter traffic. The signal timing at the intersection and along South Dakota Avenue in general, gives preference to moving traffic along South Dakota Avenue which in and of itself would discourage commuters from cutting through the neighborhood. In addition, Delafield Street is restricted to one-way traffic flow approaching South Dakota Avenue so even residents are discouraged from traveling from one side of the neighborhood to the other.

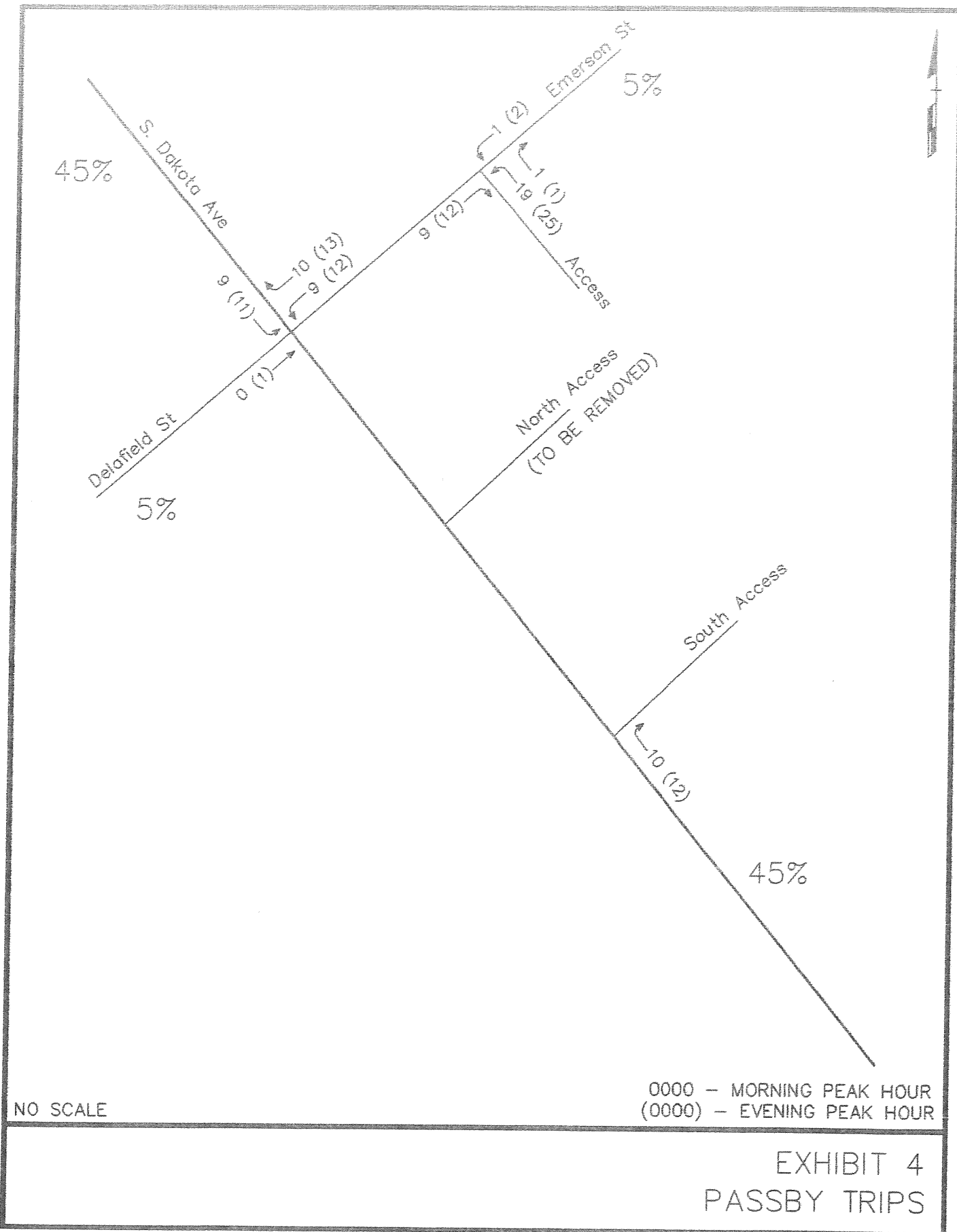
Riggs Road and Sargent Road are the main commuter routes in the area there are no readily available short cuts for these motorists through the residential streets. Furthermore, the proposed use is not likely to attract motorists from either of these roadways as there are other options available to these motorists along their respective routes. Therefore, the likelihood that the proposed use would contribute to any cut through traffic through the neighborhood is remote.

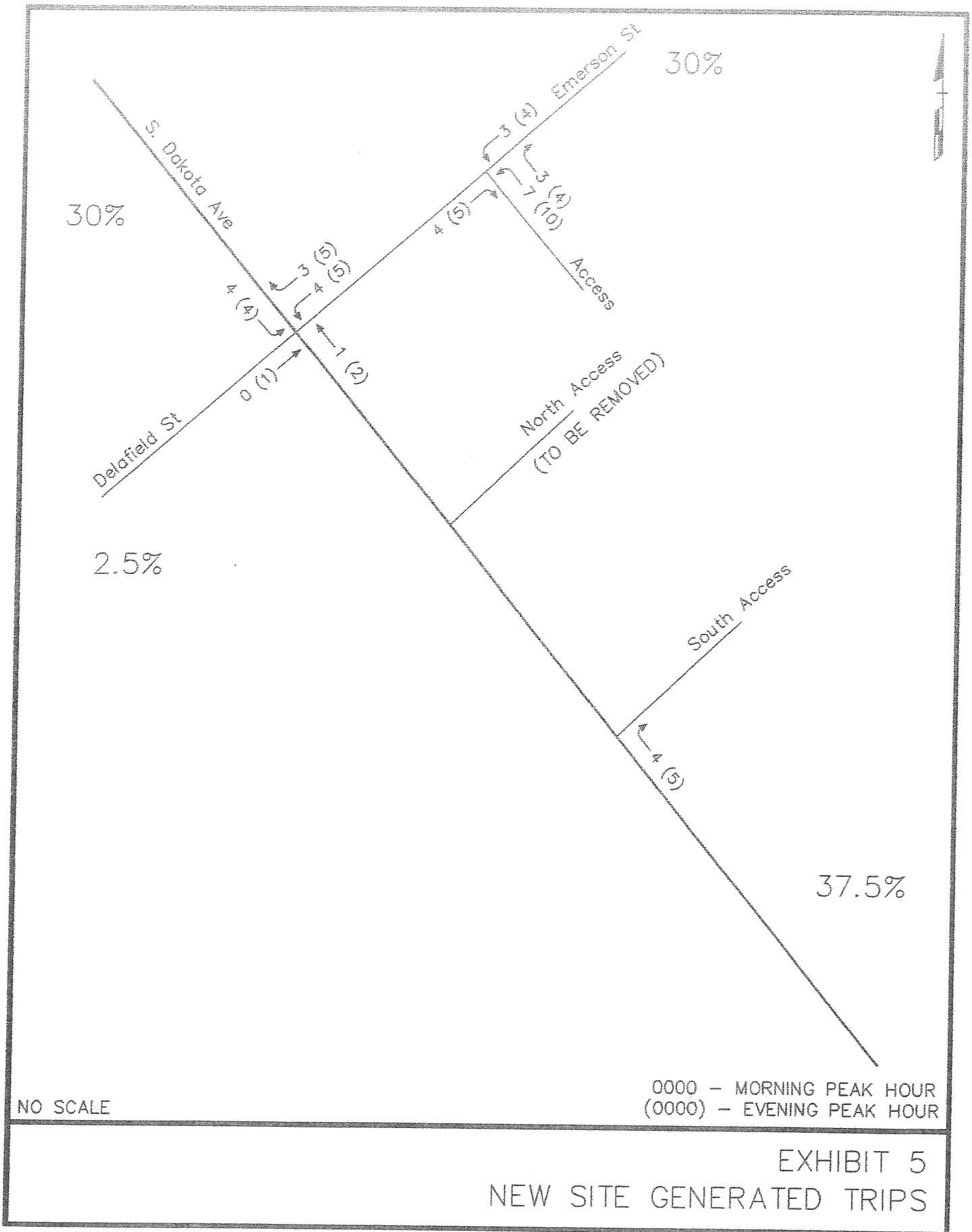
The effect of the pass-by trips are shown in Exhibit 4 and are based largely on the existing traffic flows around the site. The new trips generated by the site were assigned to the road network as shown in Exhibit 5 and were based on the existing trip distributions at the site. The Total Traffic Volumes are shown in Exhibit 6.

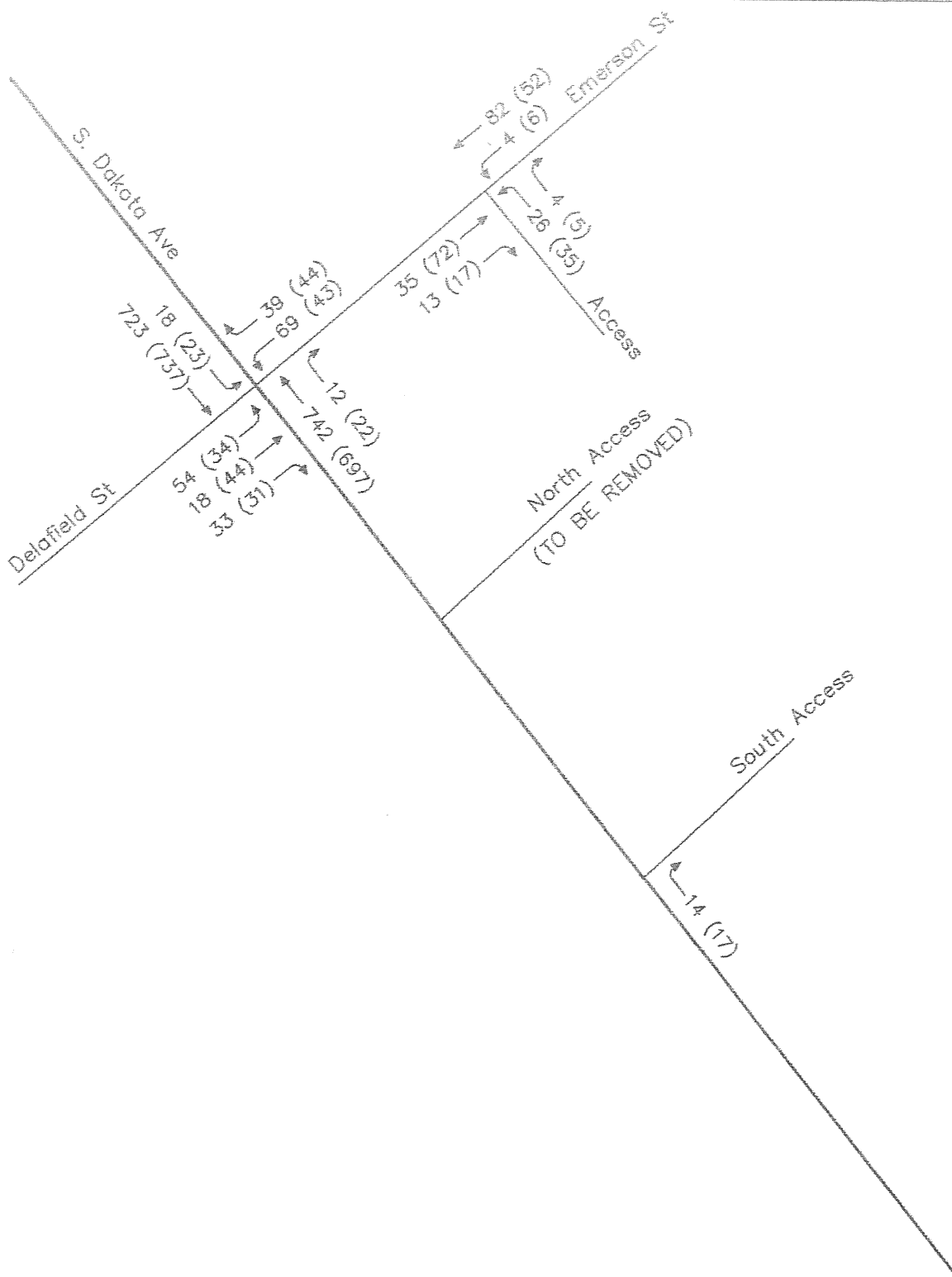
It should be noted that no adjustment was made to the volumes at the South Dakota Avenue/Emerson Street-Delafield Street intersection to remove the volumes currently generated by the convenience store use. Therefore the analyses presented are conservative.

#### **Analysis of Total Traffic Conditions**

Intersection capacity analyses were performed, applying the Highway Capacity Manual procedures to the total projected traffic volumes. The results of the analysis are presented in Tables 3 and 4.







NO SCALE

0000 - MORNING PEAK HOUR  
(0000) - EVENING PEAK HOUR

## EXHIBIT 6 TOTAL TRAFFIC VOLUMES

**TABLE 3**  
**RESULTS OF CAPACITY ANALYSES**  
**TOTAL TRAFFIC CONDITIONS**

<u>INTERSECTION</u>	<u>MORNING PEAK HOUR</u>	<u>EVENING PEAK HOUR</u>
S. Dakota Ave @ Emerson St		
EB Left Turns	D(40.1)	D(39.5)
EB Thru/Rights	D(40.2)	D(41.0)
WB Left/Rights	D(44.2)	D(43.0)
NB Thrus/Rights	A( 7.3)	A( 7.2)
SB Lefts/Thrus	A( 7.4)	A( 7.6)
Intersection LOS	B(11.7)	B(11.4)

X(0000) - Level of Service(Control Delay)

**TABLE 4**  
**RESULTS OF CAPACITY ANALYSES**  
**TOTAL TRAFFIC CONDITIONS**

<u>INTERSECTION</u>	<u>MORNING PEAK HOUR</u>	<u>EVENING PEAK HOUR</u>
Emerson St @ Site Access		
EB Left Turn	A( 7.3)	A( 7.4)
SB Left/Rights	A( 9.2)	A( 9.4)

X(0000) - Level of Service(Control Delay)

A review of the results shown in Tables 3 and 4 reveal that both intersections will continue to operate at acceptable levels of service with the addition of the trips generated by the gas station/convenience market. The capacity calculations are contained in Appendix E.

Since the remaining access point on South Dakota Avenue will be restricted to right turn in movements no capacity issues are anticipated this access point.

The access points will provide safe and efficient on-site circulation and the fueling stations are located in a manner where a queued vehicle would not impact traffic flow on South Dakota Avenue or Emerson Street nor would it obstruct pedestrian movement on the sidewalks.

### **Truck Turning Path**

A site plan is enclosed which shows the turning path of a gasoline tank truck. The plan was prepared by g2 design collective and shows that the truck could access the site via the South Dakota Avenue access, serve the site and exit via the Emerson Street access.



## CONCLUSIONS

The field studies and analyses performed as part of this study demonstrate that the proposed addition of a six (6) fueling station gas station with convenience store will not result in any adverse traffic impacts on the surrounding area road network.

## APPENDIX A

### VEHICLE TURNING MOVEMENT COUNTS

STSLTD STSLTD STSLTD STSLTD STSLTD STSLTD  
 VEHICLE TURNING MOVEMENT COUNT - SUMMARY  
 Intersection of: South Dakota Avenue  
 and: Emerson Street  
 Counted by: MN

STSLTD STSLTD STSLTD STSLTD STSLTD STSLTD  
 Location: District of Columbia  
 Date: 06/29/09  
 Weather: Clear  
 Entered by: MN

STSLTD STSLTD STSLTD STSLTD STSLTD  
 STREET  
 TRAFFIC  
 STUDIES  
 LTD

TIME	TRAFFIC FROM NORTH on: S. Dakota Ave				TRAFFIC FROM SOUTH on: S. Dakota Ave				TRAFFIC FROM WEST on: Delafield St				TRAFFIC FROM EAST on: Emerson St				TOTAL N + S + E + W
	RIGHT	THRU	LEFT	TOTAL	LEFT	THRU	RIGHT	TOTAL	RIGHT	THRU	LEFT	TOTAL	LEFT	THRU	RIGHT	TOTAL	
AM																	
07:00-15	0	120	1	121	0	152	1	153	4	3	7	14	3	0	3	11	299
15-30	0	172	1	173	0	196	1	197	7	9	11	24	15	0	7	22	416
30-45	0	171	0	171	0	197	5	202	7	4	17	28	14	0	7	21	422
45-00	0	203	4	207	0	174	3	177	6	3	13	24	14	0	6	20	428
08:00-15	5	177	0	182	0	175	3	178	11	5	13	29	13	0	6	19	408
15-30	0	198	0	198	0	174	2	176	6	3	8	19	10	0	3	13	406
30-45	0	185	2	187	0	194	2	196	5	3	16	24	16	0	3	19	426
45-00	0	166	3	169	0	197	2	199	11	5	9	25	7	0	1	8	341
AM																	
2 HOUR																	
TOTALS	5	1392	11	1408	0	1399	19	1418	61	32	94	187	97	0	36	133	3146
1 HOUR																	
TOTALS																	
07-08	0	666	6	672	0	719	10	729	26	16	48	90	51	0	23	74	1565
715-815	5	723	5	733	0	742	12	754	33	18	54	105	56	0	26	82	1674
730-830	5	749	4	758	0	720	13	733	34	15	51	100	51	0	22	73	1664
745-845	5	763	6	774	0	717	10	727	32	14	50	96	53	0	18	71	1668
08-09	5	726	5	736	0	680	9	689	35	15	46	97	46	0	13	59	1581
PEAK HOUR																	
715-815	5	723	5	733	0	742	12	754	33	18	54	105	56	0	26	82	1674
PM																	
04:00-15	0	176	5	181	0	152	2	154	2	9	3	14	4	0	8	12	361
15-30	0	184	2	186	0	141	4	145	2	4	16	22	5	0	4	9	372
30-45	0	177	3	180	0	182	6	188	8	7	10	25	7	0	7	14	407
45-00	0	193	0	193	0	155	5	160	2	12	6	20	8	0	6	14	387
05:00-15	0	170	3	173	0	162	2	164	11	14	5	30	10	0	3	13	380
15-30	0	192	1	193	0	192	5	197	2	9	14	25	3	0	9	12	427
30-45	0	198	2	200	0	168	8	176	7	10	5	22	3	0	6	9	407
45-00	0	177	2	179	0	175	7	182	11	9	10	30	10	0	8	18	409
PM																	
2 HOUR																	
TOTALS	0	1477	18	1495	0	1327	39	1366	45	74	69	188	50	0	51	101	3150
1 HOUR																	
TOTALS																	
04-05	0	740	10	750	0	630	17	647	14	32	35	81	24	0	25	49	1527
415-515	0	734	8	742	0	640	17	657	23	37	37	97	30	0	20	50	1546
430-530	0	732	7	739	0	691	18	709	23	42	35	100	28	0	25	53	1601
445-545	0	753	6	759	0	677	20	697	22	45	30	97	24	0	24	48	1601
05-06	0	737	8	745	0	697	22	719	31	42	34	107	26	0	26	52	1623
PEAK HOUR																	
05-06	0	737	8	745	0	697	22	719	31	42	34	107	26	0	26	52	1623

STSLTD STSLTD STSLTD STSLTD STSLTD STSLTD

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PNP  
0.98

PNP  
0.95

STSLTD STSLTD STSLTD STSLTD STSLTD ST: STSLTD STSLTD STSLTD S STSLTD STSLTD STSLTD STSLTD  
 PEDESTRIAN COUNT - SUMMARY  
 Location: District of Columbia  
 Intersection of: South Dakota Avenue  
 Date: 09/29/09 Day: Tuesday  
 and: Emerson Street  
 Weather: Clear  
 Counted by: MN  
 Entered By: MN

	NORTH LEG		EAST LEG		SOUTH LEG		WEST LEG		
TIME	EB	WB	NB	SB	EB	WB	NB	SB	TOTAL
AM									
07:00-15	3	0	0	2	0	2	3	1	11
15-30	0	0	0	0	0	0	7	1	8
30-45	6	0	2	3	1	0	1	0	13
45-00	2	0	0	4	2	0	1	0	9
08:00-15	0	1	0	0	2	0	4	2	9
15-30	0	0	0	2	1	0	1	0	4
30-45	2	0	0	0	1	0	0	0	3
45-00	2	0	2	0	0	0	2	0	6
AM									
2 HOUR									
TOTALS	15	1	4	11	7	2	19	4	63
1 HOUR									
TOTALS									
07-08	11	0	2	9	3	2	12	2	41
715-815	8	1	2	7	5	0	13	3	39
730-830	8	1	2	9	6	0	7	2	35
745-845	4	1	0	6	6	0	6	2	25
08-09	4	1	2	2	4	0	7	2	22
PEAK HOUR									
07-08	11	0	2	9	3	2	12	2	41
PM									
04:00-15	0	0	0	0	1	1	0	0	2
15-30	1	1	2	0	8	1	4	3	20
30-45	1	0	0	1	5	1	1	1	10
45-00	0	0	0	0	2	6	5	1	14
05:00-15	1	1	1	0	5	3	1	1	13
15-30	0	0	8	0	5	4	2	2	21
30-45	2	0	4	3	5	5	4	3	26
45-00	0	0	0	1	7	4	1	6	19
PM									
2 HOUR									
TOTALS	5	2	15	5	38	25	18	17	125
1 HOUR									
TOTALS									
04-05	2	1	2	1	16	9	10	5	46
415-515	3	2	3	1	20	11	11	6	57
430-530	2	1	9	1	17	14	9	5	58
445-545	3	1	13	3	17	18	12	7	74
05-06	3	1	13	4	22	16	8	12	79
PEAK HOUR									
05-06	3	1	13	4	22	16	8	12	79

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## VEHICLE TURNING MOVEMENT COUNT - SUMMARY

Intersection of: South Dakota Avenue  
and: North Driveway  
Counted by: MN

Location: District of Columbia  
Date: 09/29/09  
Weather: Clear  
Entered by: MN

Day: Tuesday

STREET  
TRAFFIC  
STUDIES  
LTD

TIME	TRAFFIC FROM NORTH on: S Dakota Ave				TRAFFIC FROM SOUTH on: S Dakota Ave				TRAFFIC FROM WEST on:				TRAFFIC FROM EAST on: North Driveway				TOTAL N+S + E+W
	RIGHT	THRU	LEFT	TOTAL	LEFT	THRU	RIGHT	TOTAL	RIGHT	THRU	LEFT	TOTAL	LEFT	THRU	RIGHT	TOTAL	
AM																	
07:00-15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
15-30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
30-45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
45-00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00-15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
15-30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
30-45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
45-00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
AM																	
2 HOUR																	
TOTALS	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1 HOUR																	
TOTALS																	
07-08	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
715-815	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
730-830	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
745-845	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08-09	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PEAK HOUR																	
07-08	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PM																	
04:00-15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	1
15-30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
30-45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	1
45-00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	2	2
05:00-15	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	1
15-30	0	0	1	1	0	0	0	0	0	0	0	0	0	0	1	1	2
30-45	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	1
45-00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PM																	
2 HOUR																	
TOTALS	0	0	2	2	0	0	1	1	0	0	0	0	0	0	5	5	8
1 HOUR																	
TOTALS																	
04-05	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	4	4
415-515	0	0	0	0	0	0	1	1	0	0	0	0	0	0	3	3	4
430-530	0	0	1	1	0	0	1	1	0	0	0	0	0	0	4	4	6
445-545	0	0	2	2	0	0	1	1	0	0	0	0	0	0	3	3	6
05-06	0	0	2	2	0	0	1	1	0	0	0	0	0	0	1	1	4
PEAK HOUR																	
04-05	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	4	4

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VEHICLE TURNING MOVEMENT COUNT - SUMMARY

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Intersection of  
and:  
Counted by:

South Dakota Avenue  
South Driveway  
MN

(Driveway Volumes Only)

Location : District of Columbia  
Date : 09/29/09  
Weather : Clear  
Entered by: MN

Day: Tuesday

STREET  
TRAFFIC  
STUDIES  
LTD

TIME	TRAFFIC FROM NORTH on: S. Dakota Ave				TRAFFIC FROM SOUTH on: S. Dakota Ave				TRAFFIC FROM WEST on:				TRAFFIC FROM EAST on: South Driveway				TOTAL N + S + E + W
	RIGHT	THRU	LEFT	TOTAL	LEFT	THRU	RIGHT	TOTAL	RIGHT	THRU	LEFT	TOTAL	LEFT	THRU	RIGHT	TOTAL	
AM																	
07:00-15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
15-30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
30-45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
45-00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00-15	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	1
15-30	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
30-45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
45-00	0	0	0	0	0	0	2	2	0	0	0	0	0	0	0	0	2
AM																	
2 HOUR																	
TOTALS	0	0	0	0	0	0	3	3	0	0	0	0	1	0	0	1	4
1 HOUR																	
TOTALS																	
07-08	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
715-815	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	1
730-830	0	0	0	0	0	0	1	1	0	0	0	0	1	0	0	1	2
745-845	0	0	0	0	0	0	1	1	0	0	0	0	1	0	0	1	2
08-09	0	0	0	0	0	0	3	3	0	0	0	0	1	0	0	1	4
PEAK HOUR																	
08-09	0	0	0	0	0	0	3	3	0	0	0	0	1	0	0	1	4
PM																	
04:00-15	0	0	0	0	0	0	3	3	0	0	0	0	1	0	0	1	4
15-30	0	0	0	0	0	0	4	4	0	0	0	0	1	0	0	1	5
30-45	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	1
45-00	0	0	0	0	0	0	4	4	0	0	0	0	1	0	2	3	7
06:00-15	0	0	0	0	0	0	3	3	0	0	0	0	0	0	1	1	4
15-30	0	0	1	1	0	0	3	3	0	0	0	0	0	0	0	0	4
30-45	0	0	0	0	0	0	4	4	0	0	0	0	1	0	0	1	5
45-00	0	0	1	1	0	0	5	5	0	0	0	0	0	0	1	1	7
PM																	
2 HOUR																	
TOTALS	0	0	2	2	0	0	27	27	0	0	0	0	4	0	4	8	37
1 HOUR																	
TOTALS																	
04-05	0	0	0	0	0	0	12	12	0	0	0	0	3	0	2	5	17
415-515	0	0	0	0	0	0	12	12	0	0	0	0	2	0	3	5	17
430-530	0	0	1	1	0	0	11	11	0	0	0	0	1	0	3	4	16
445-545	0	0	1	1	0	0	14	14	0	0	0	0	2	0	3	5	20
05-06	0	0	2	2	0	0	15	15	0	0	0	0	1	0	2	3	20
PEAK HOUR																	
04-05	0	0	0	0	0	0	12	12	0	0	0	0	3	0	2	5	17

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## VEHICLE TURNING MOVEMENT COUNT - SUMMARY

Intersection of:  
and:  
Counted by:Emerson Street  
Driveway  
MN

(Driveway Volumes Only)

Location :

District of Columbia

Date :

09/28/09

Day: Tuesday

Weather :

Clear

Entered by:

MN

STREET  
TRAFFIC  
STUDIES  
LTD

TIME	TRAFFIC FROM NORTH on:				TRAFFIC FROM SOUTH on: Driveway				TRAFFIC FROM WEST on: Emerson St				TRAFFIC FROM EAST on: Emerson St				TOTAL N + S + E + W
	RIGHT	THRU	LEFT	TOTAL	LEFT	THRU	RIGHT	TOTAL	RIGHT	THRU	LEFT	TOTAL	LEFT	THRU	RIGHT	TOTAL	
AM																	
07:00-15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
15-30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
30-45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
45-00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00-15	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	1
15-30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
30-45	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
45-00	0	0	0	0	3	0	1	4	0	0	0	0	1	0	0	1	5
AM																	
2 HOUR																	
TOTALS	0	0	0	0	3	0	2	5	0	0	0	0	2	0	0	2	7
1 HOUR																	
TOTALS																	
07-08	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
715-815	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	1
730-830	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	1
745-845	0	0	0	0	0	0	1	1	0	0	0	0	1	0	0	1	2
08-09	0	0	0	0	3	0	2	5	0	0	0	0	2	0	0	2	7
PEAK HOUR																	
08-09	0	0	0	0	3	0	2	5	0	0	0	0	2	0	0	2	7
PM																	
04:00-15	0	0	0	0	8	0	4	12	2	0	0	2	3	0	0	3	17
15-30	0	0	0	0	4	0	3	7	3	0	0	3	3	0	0	3	13
30-45	0	0	0	0	5	0	2	7	3	0	0	3	2	0	0	2	12
45-00	0	0	0	0	7	0	1	8	7	0	0	7	1	0	0	1	16
05:00-15	0	0	0	0	4	0	7	11	1	0	0	1	2	0	0	2	14
15-30	0	0	0	0	6	0	2	8	2	0	0	2	4	0	0	4	14
30-45	0	0	0	0	3	0	3	6	5	0	0	5	3	0	0	3	14
45-00	0	0	0	0	6	0	4	10	1	0	0	1	3	0	0	3	14
PM																	
2 HOUR																	
TOTALS	0	0	0	0	43	0	26	69	24	0	0	24	21	0	0	21	114
1 HOUR																	
TOTALS																	
04-05	0	0	0	0	24	0	10	34	15	0	0	15	9	0	0	9	58
415-515	0	0	0	0	20	0	13	33	14	0	0	14	8	0	0	8	55
430-530	0	0	0	0	22	0	12	34	13	0	0	13	9	0	0	9	56
445-545	0	0	0	0	20	0	13	33	15	0	0	15	10	0	0	10	58
05-06	0	0	0	0	19	0	16	35	9	0	0	9	12	0	0	12	56
PEAK HOUR																	
04-05	0	0	0	0	24	0	10	34	15	0	0	15	9	0	0	9	58

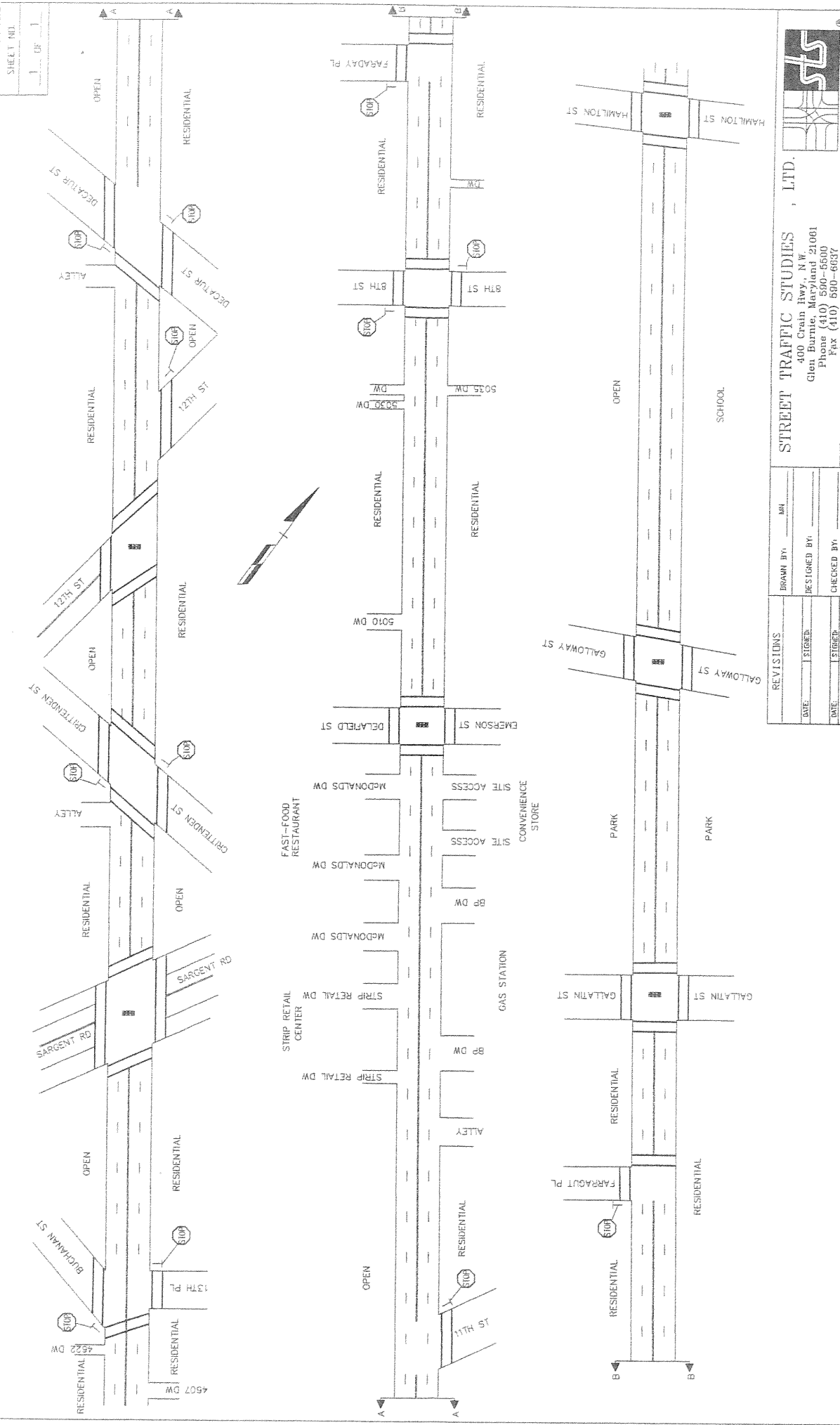
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**APPENDIX B**  
**CONDITION DIAGRAM**





REVISIONS		DRAWN BY: MAL	
DATE:	SIGNED:	DESIGNED BY:	
DATE:	SIGNED:	CHECKED BY:	
DATE:	SIGNED:	DATE:	OCTOBER 2009
DATE:	SIGNED:	SCALE:	1" = 40'

STREET TRAFFIC STUDIES, LTD.  
 400 Crain Hwy., N.W.  
 Glen Burnie, Maryland 21061  
 Phone (410) 590-5500  
 Fax (410) 590-6637

SOUTH DAKOTA AVENUE  
 CONDITION DIAGRAM  
 STATE: \_\_\_\_\_ COUNTY: \_\_\_\_\_

**APPENDIX C**  
**SIGNAL TIMING SHEETS**

## TIMING PLAN SCHEDULE

TS- 1240-A

SOUTH DAKOTA AVENUE AND DELAFIELD STREET, N.E.

PLAN	PERIOD	DATE
------	--------	------

1 OFF Peak - 70

2 AM Peak - 80

3 PM Peak - 80

4 OFF Peak 100

5 AM Peak - 100

6 PM Peak - 100

7 AM Peak - 120

8 PM Peak - 120

CONTROLLER	ISNUM	ACISA
170	678	3206

DEPARTMENT OF TRANSPORTATION

WASHINGTON, D.C.

TRAFFIC SERVICES ADMINISTRATION

SOUTH DAKOTA AVENUE

DELAFIELD STREET

N

Int Sketch

TIMING PLAN NUMBER:

DESCRIPTION ( INTERSECTION / STREET / DIRECTION )

INTERVAL

NUMBER

TIME

F

V

F

F

F

F

F

V

F

F

F

F

F

F

F

F

F

F

F

F

F

F

F

F

F

F

F

F

F

F

F

F

F

F

F

F

F

F

F

F

SOUTH DAKOTA AVENUE GREEN + W (NS, SS)

SOUTH DAKOTA AVENUE GREEN + W (NS, SS)

SOUTH DAKOTA AVENUE GREEN + FDW (NS, SS)

SOUTH DAKOTA AVENUE YELLOW + DW (NS, SS)

ALL RED + DW (NS, SS)

DELAFIELD STREET GREEN + W (ES, WS)

DELAFIELD STREET GREEN + W (ES, WS)

DELAFIELD STREET GREEN + FDW (ES, WS)

DELAFIELD STREET YELLOW + DW (ES, WS)

ALL RED + DW (ES, WS)

CYCLE LENGTH

70

10

PSI

PSI

PSI

PSI

PSI

PSI

PSI

PSI

PSI

PSI

PSI

PREPARED BY:

DATE TO SHOP:

WORK OR SHOP ORDER NO:

APPROVED BY:

DATE INSTALLED:

INSTALLED BY:

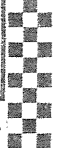
S=Seconds

C=Cumulative secs

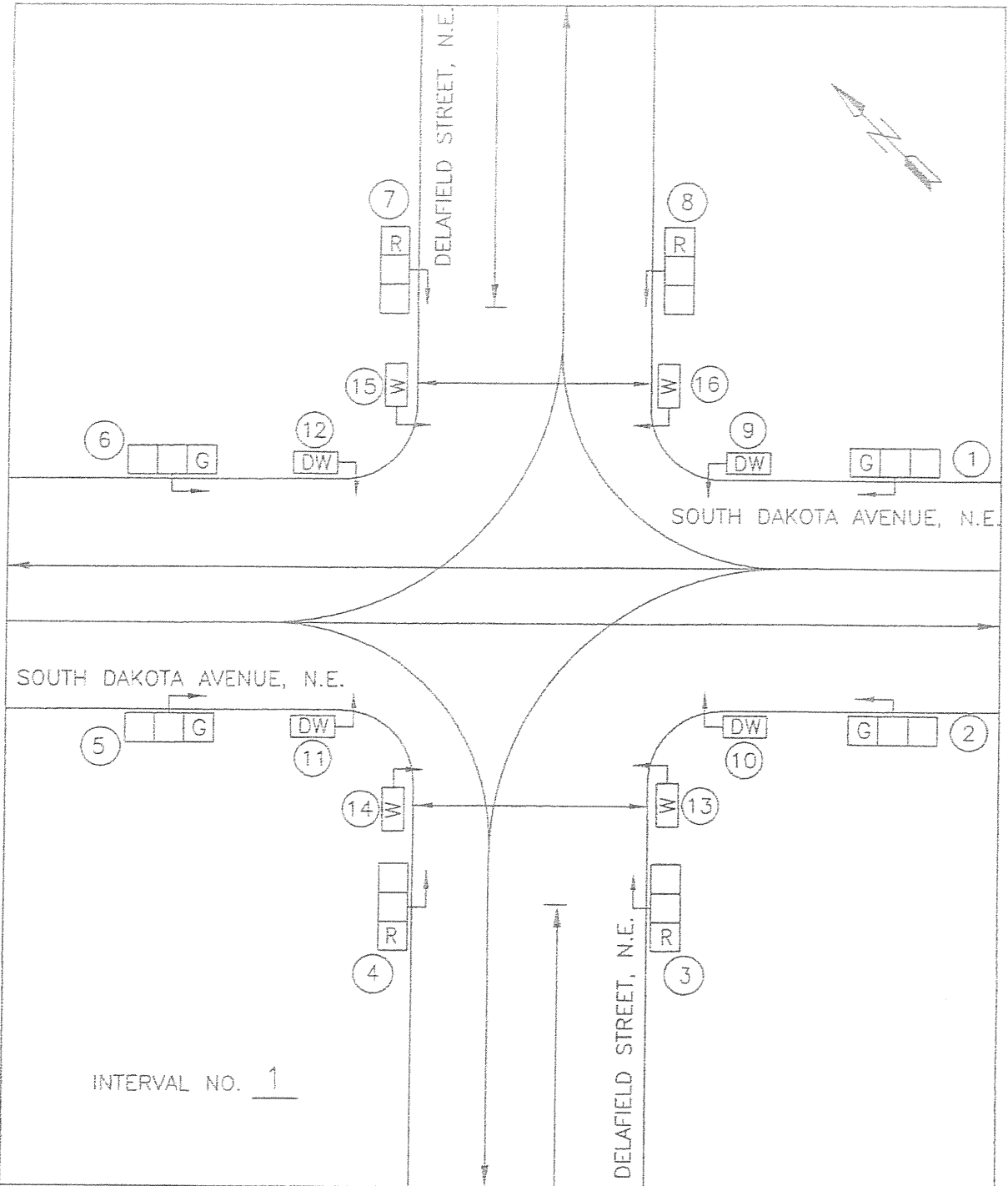
F=Fixed Interval

V=Variable Interval

= Force Off (circle the interval)

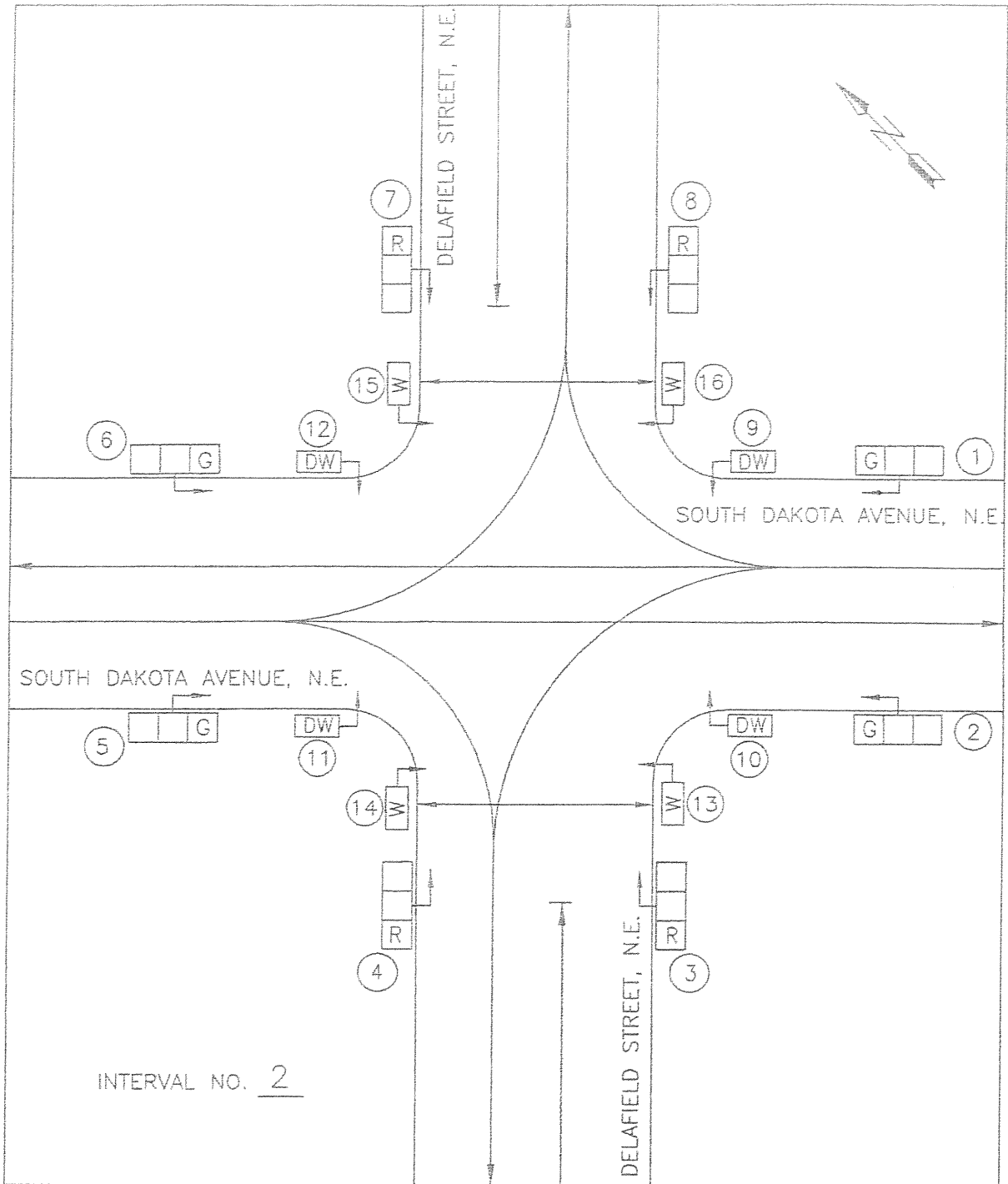


t 05 09 04:24p

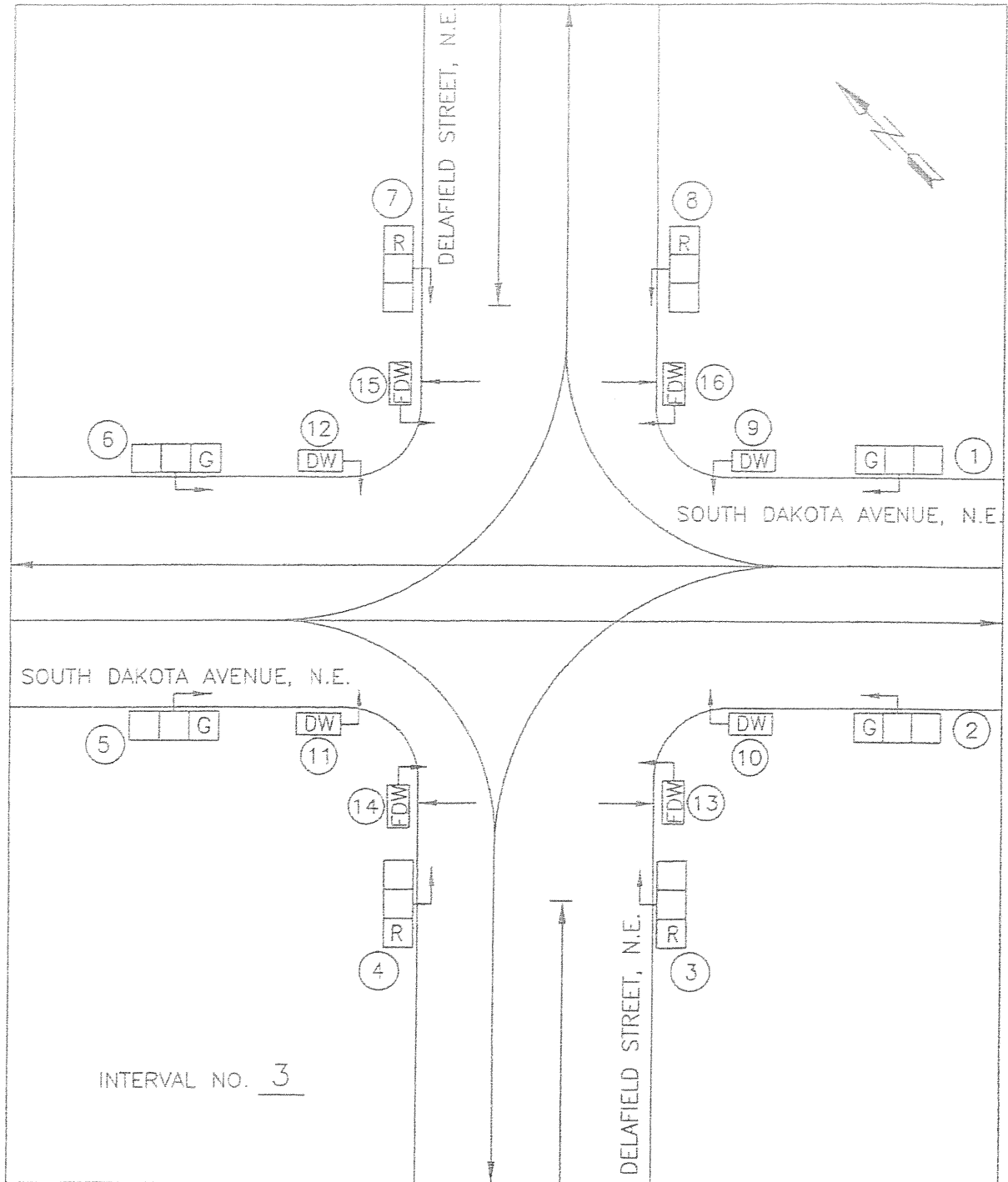


INTERVAL NO. 1

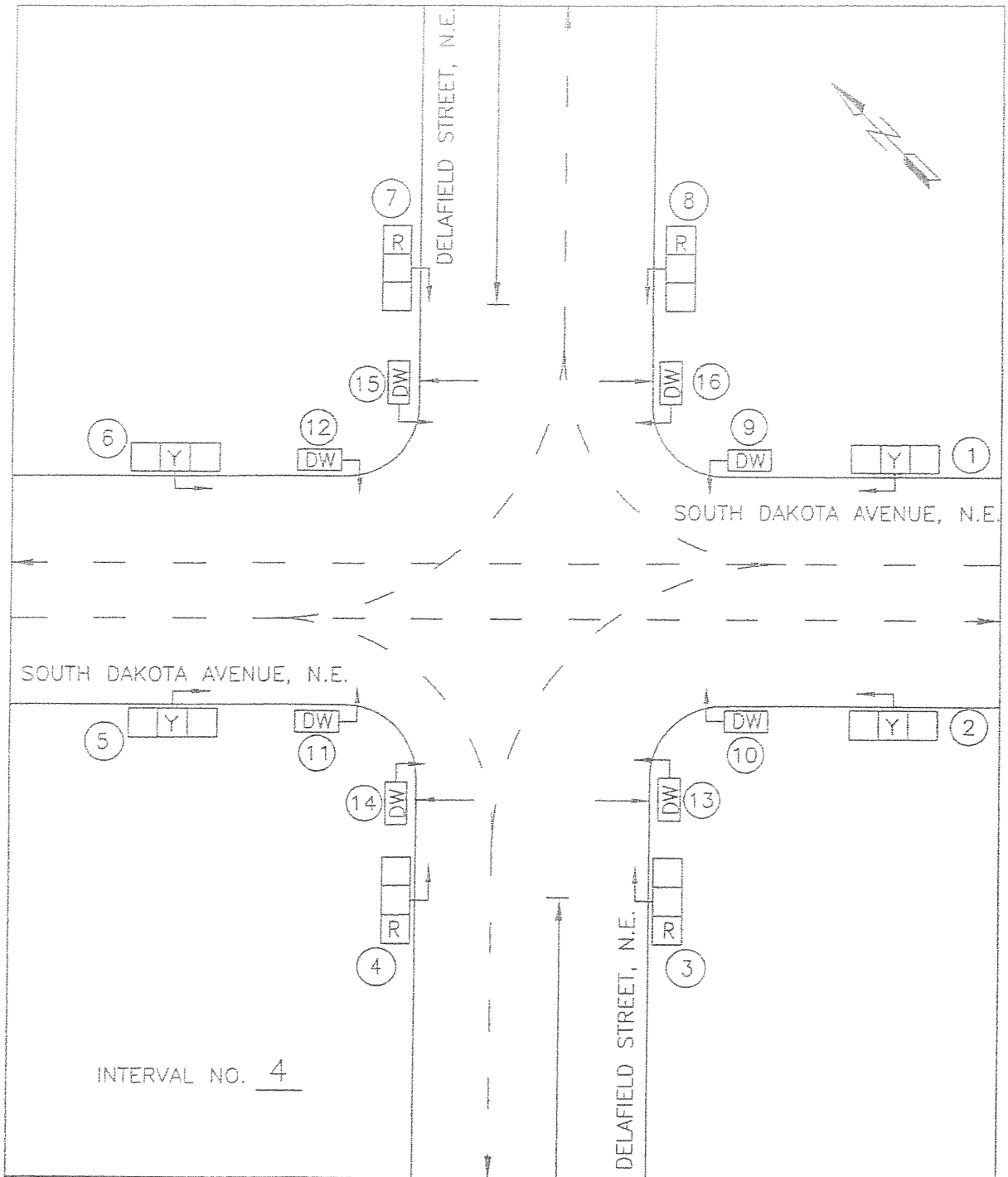
TRAFFIC SIGNAL OPERATION		
SOUTH DAKOTA AVENUE AND DELAFIELD STREET, N.E.		
D.C. DEPARTMENT OF TRANSPORTATION TRAFFIC SERVICES ADMINISTRATION TRAFFIC SIGNAL SYSTEM DIVISION	DESIGNED BY: <u>PSI</u>	T.S.
	SUBMITTED BY: <u>CHIEF, SIGNAL DESIGN BRANCH</u>	1240-A
	APPROVED BY: <u>William W. McQuinn 1/16/08</u>	SHEET
	DIVISION CHIEF	1 OF 10
CHECK BY:		
DRAWN BY: <u>XW</u>		
IN SERVICE:		



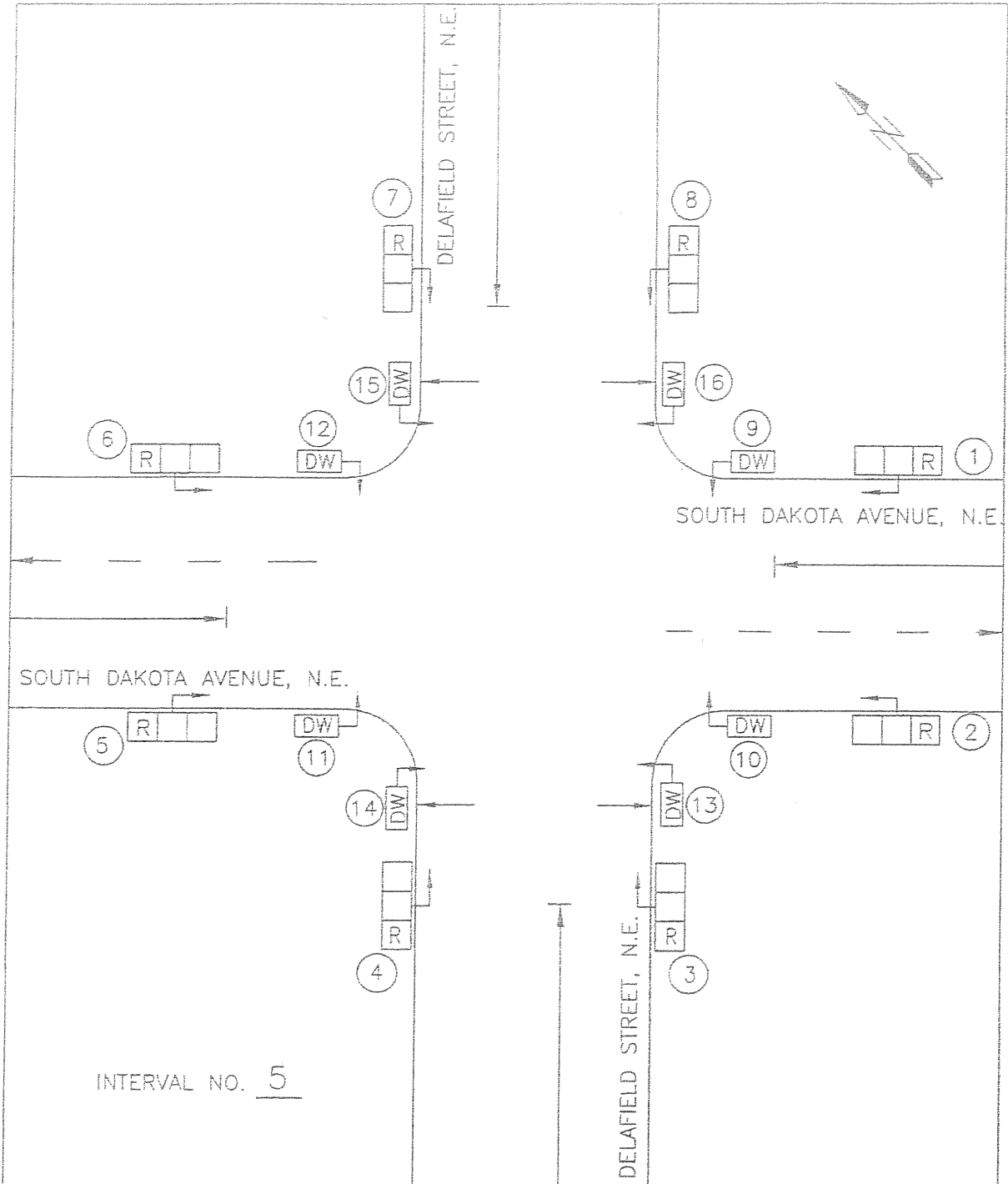
TRAFFIC SIGNAL OPERATION		
SOUTH DAKOTA AVENUE AND DELAFIELD STREET, N.E.		
D.C. DEPARTMENT OF TRANSPORTATION TRAFFIC SERVICES ADMINISTRATION TRAFFIC SIGNAL SYSTEM DIVISION		DESIGNED BY: _____
CHECK BY: _____		SUBMITTED BY: CHIEF, SIGNAL DESIGN BRANCH
DRAWN BY: XW		APPROVED BY: DIVISION CHIEF
IN SERVICE: _____		T.S. 1240-A SHEET 2 OF 10



TRAFFIC SIGNAL OPERATION		
SOUTH DAKOTA AVENUE AND DELAFIELD STREET, N.E.		
D.C. DEPARTMENT OF TRANSPORTATION TRAFFIC SERVICES ADMINISTRATION TRAFFIC SIGNAL SYSTEM DIVISION	DESIGNED BY: _____	T.S.
	SUBMITTED BY: CHIEF, SIGNAL DESIGN BRANCH	1240-A
	APPROVED BY: DIVISION CHIEF	SHEET
		3 OF 10
CHECK BY: _____		
DRAWN BY: XW		
IN SERVICE: _____		

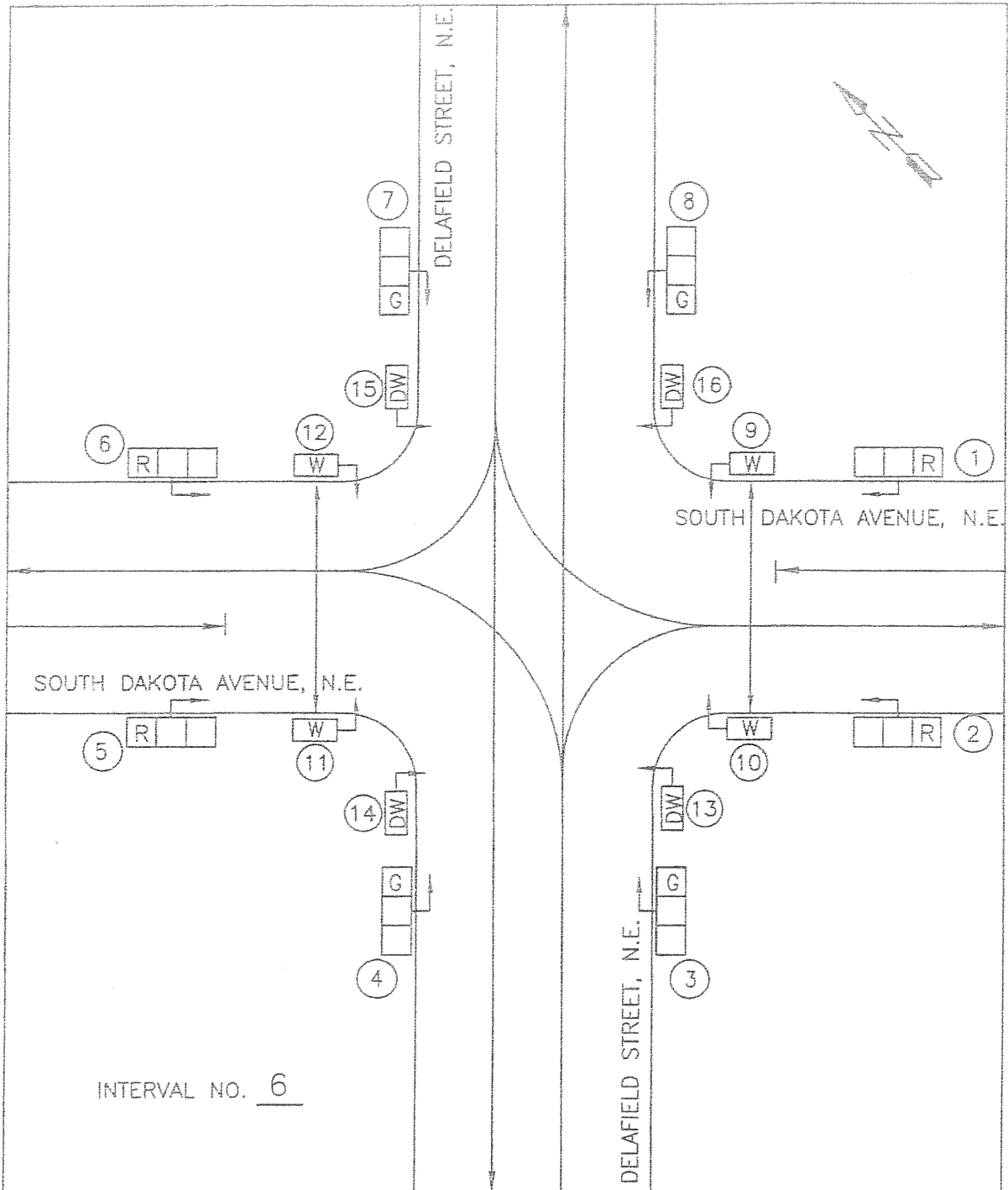


<p>TRAFFIC SIGNAL OPERATION</p> <p><b>SOUTH DAKOTA AVENUE AND DELAFIELD STREET, N.E.</b></p>		
<p>D.C. DEPARTMENT OF TRANSPORTATION</p> <p>TRAFFIC SERVICES ADMINISTRATION</p> <p>TRAFFIC SIGNAL SYSTEM DIVISION</p>	DESIGNED BY: _____	T.S.
	SUBMITTED BY: _____	1240-A
	APPROVED BY: _____	SHEET
	4 OF 10	
<p>CHECK BY: _____</p> <p>DRAWN BY: XW</p> <p>IN SERVICE: _____</p>	<p>CHIEF, SIGNAL DESIGN BRANCH</p> <p>DIVISION CHIEF</p>	

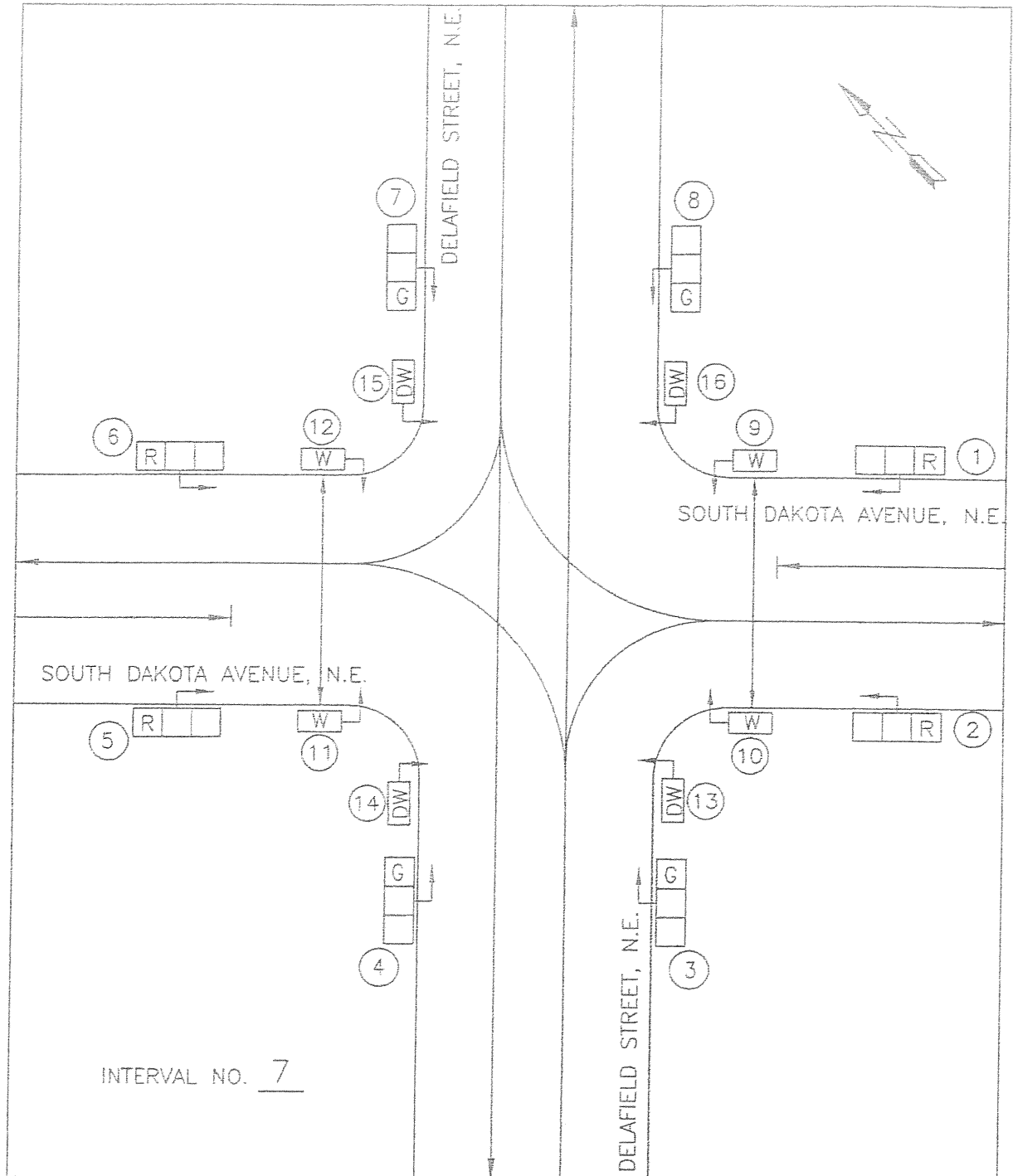


TRAFFIC SIGNAL OPERATION		
SOUTH DAKOTA AVENUE AND DELAFIELD STREET, N.E.		
D.C. DEPARTMENT OF TRANSPORTATION TRAFFIC SERVICES ADMINISTRATION TRAFFIC SIGNAL SYSTEM DIVISION	DESIGNED BY: _____	T.S.
	SUBMITTED BY: CHIEF, SIGNAL DESIGN BRANCH	1240-A
	APPROVED BY: DIVISION CHIEF	SHEET
		5 OF 10
CHECK BY: _____		
DRAWN BY: XW		
IN SERVICE:		

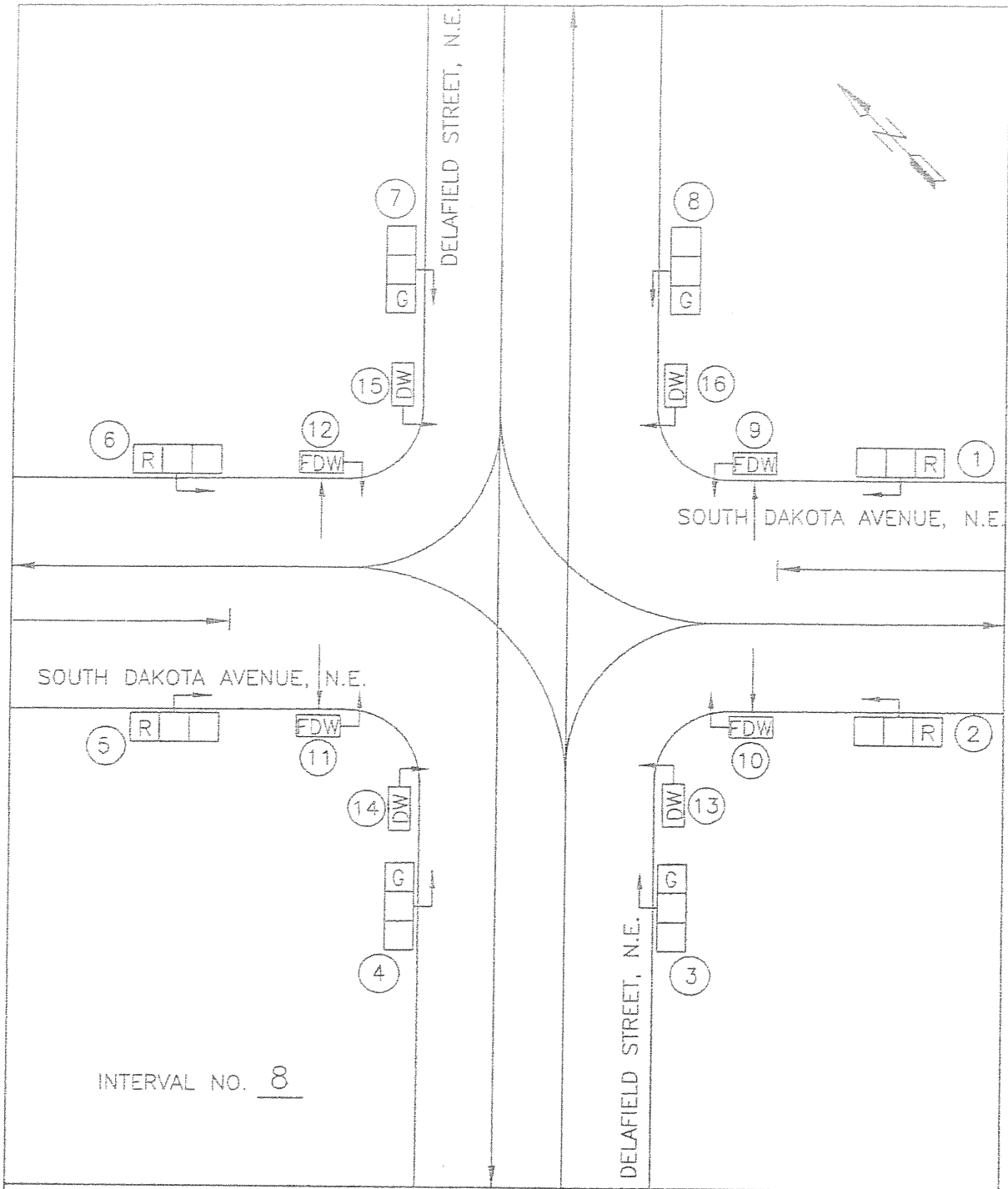




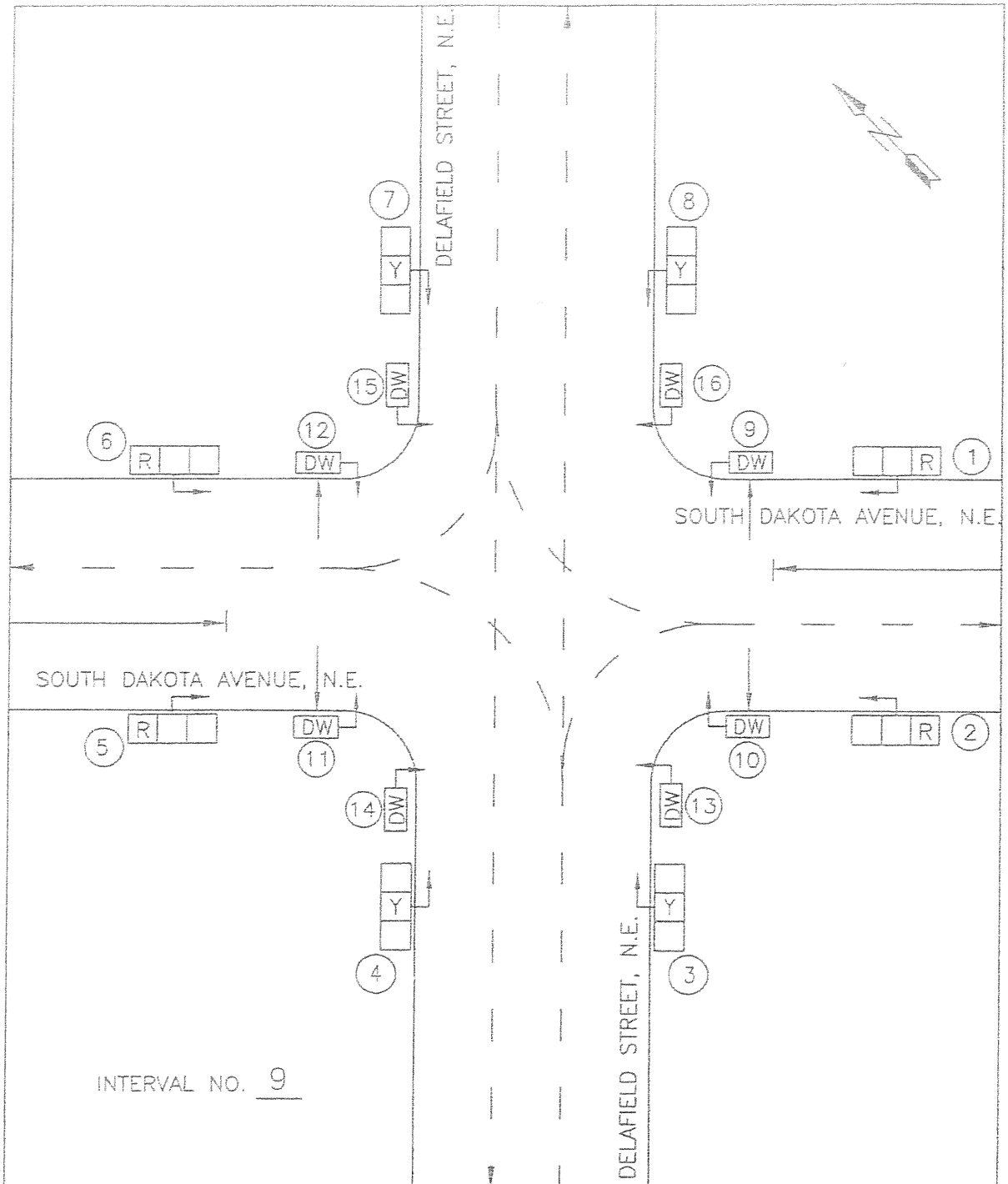
TRAFFIC SIGNAL OPERATION		
SOUTH DAKOTA AVENUE AND DELAFIELD STREET, N.E.		
D.C. DEPARTMENT OF TRANSPORTATION TRAFFIC SERVICES ADMINISTRATION TRAFFIC SIGNAL SYSTEM DIVISION	DESIGNED BY: _____	T.S.
	SUBMITTED BY: _____	1240-A
	APPROVED BY: _____	SHEET
	IN SERVICE:	6 OF 10



TRAFFIC SIGNAL OPERATION		
SOUTH DAKOTA AVENUE AND DELAFIELD STREET, N.E.		
D.C. DEPARTMENT OF TRANSPORTATION TRAFFIC SERVICES ADMINISTRATION TRAFFIC SIGNAL SYSTEM DIVISION		DESIGNED BY: _____
CHECK BY: _____		SUBMITTED BY: CHIEF, SIGNAL DESIGN BRANCH
DRAWN BY: XW		APPROVED BY: DIVISION CHIEF
IN SERVICE: _____		T.S. 1240-A SHEET 7 OF 10



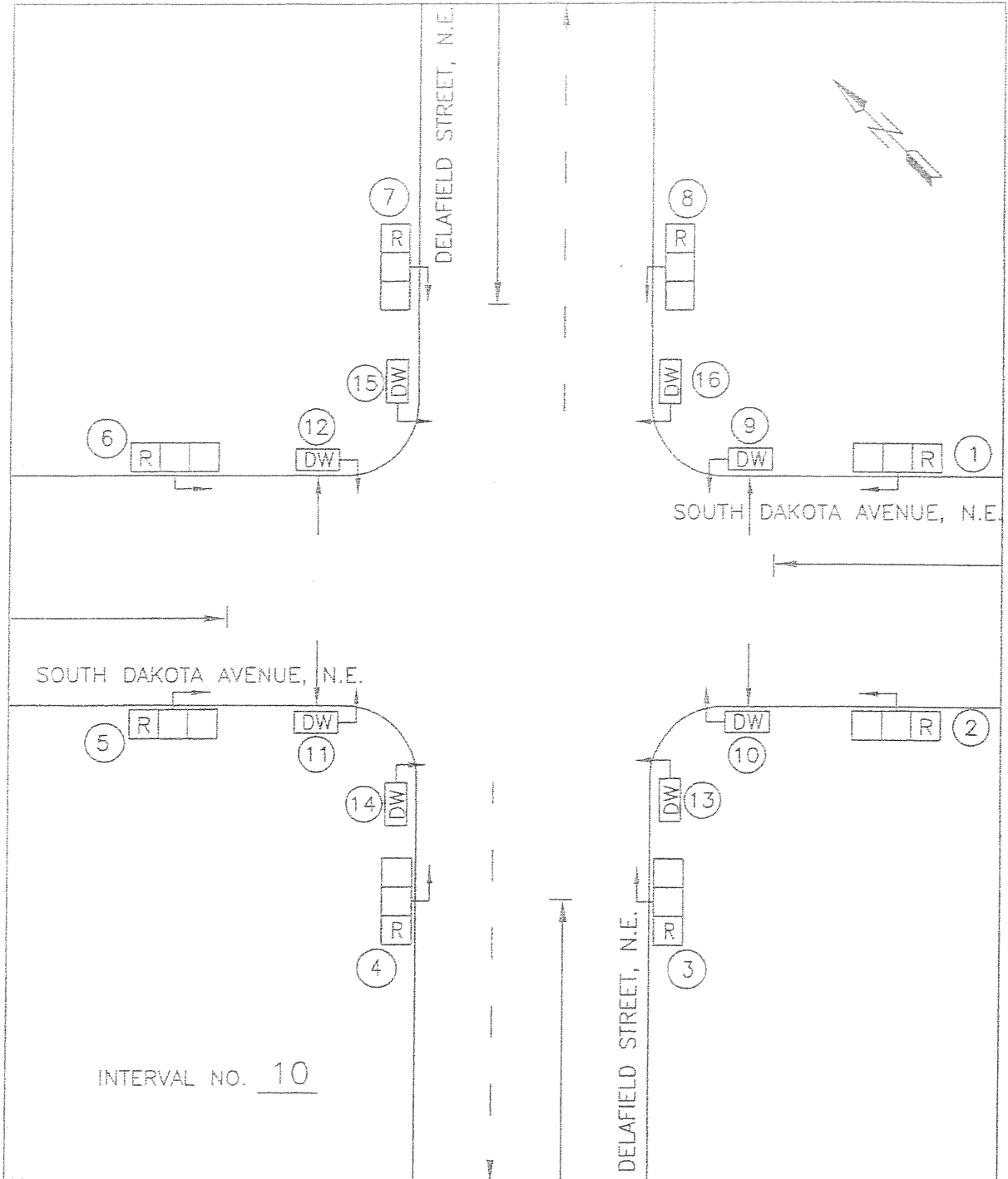
TRAFFIC SIGNAL OPERATION		
SOUTH DAKOTA AVENUE AND DELAFIELD STREET, N.E.		
D.C. DEPARTMENT OF TRANSPORTATION TRAFFIC SERVICES ADMINISTRATION TRAFFIC SIGNAL SYSTEM DIVISION	DESIGNED BY: _____	T.S.
	SUBMITTED BY: CHIEF, SIGNAL DESIGN BRANCH	1240-A
	APPROVED BY: DIVISION CHIEF	SHEET
		8 OF 10
CHECK BY: _____		
DRAWN BY: XW		
IN SERVICE:		



INTERVAL NO. 9

TRAFFIC SIGNAL OPERATION  
SOUTH DAKOTA AVENUE AND DELAFIELD STREET, N.E.

D.C. DEPARTMENT OF TRANSPORTATION TRAFFIC SERVICES ADMINISTRATION TRAFFIC SIGNAL SYSTEM DIVISION		DESIGNED BY: _____	T.S.
CHECK BY: _____		SUBMITTED BY: CHIEF, SIGNAL DESIGN BRANCH	1240-A
DRAWN BY: XW		APPROVED BY: DIVISION CHIEF	SHEET
IN SERVICE: _____			9 OF 10



TRAFFIC SIGNAL OPERATION  
SOUTH DAKOTA AVENUE AND DELAFIELD STREET, N.E.

D.C. DEPARTMENT OF TRANSPORTATION  
TRAFFIC SERVICES ADMINISTRATION  
TRAFFIC SIGNAL SYSTEM DIVISION

CHECK BY: \_\_\_\_\_

DRAWN BY: XW

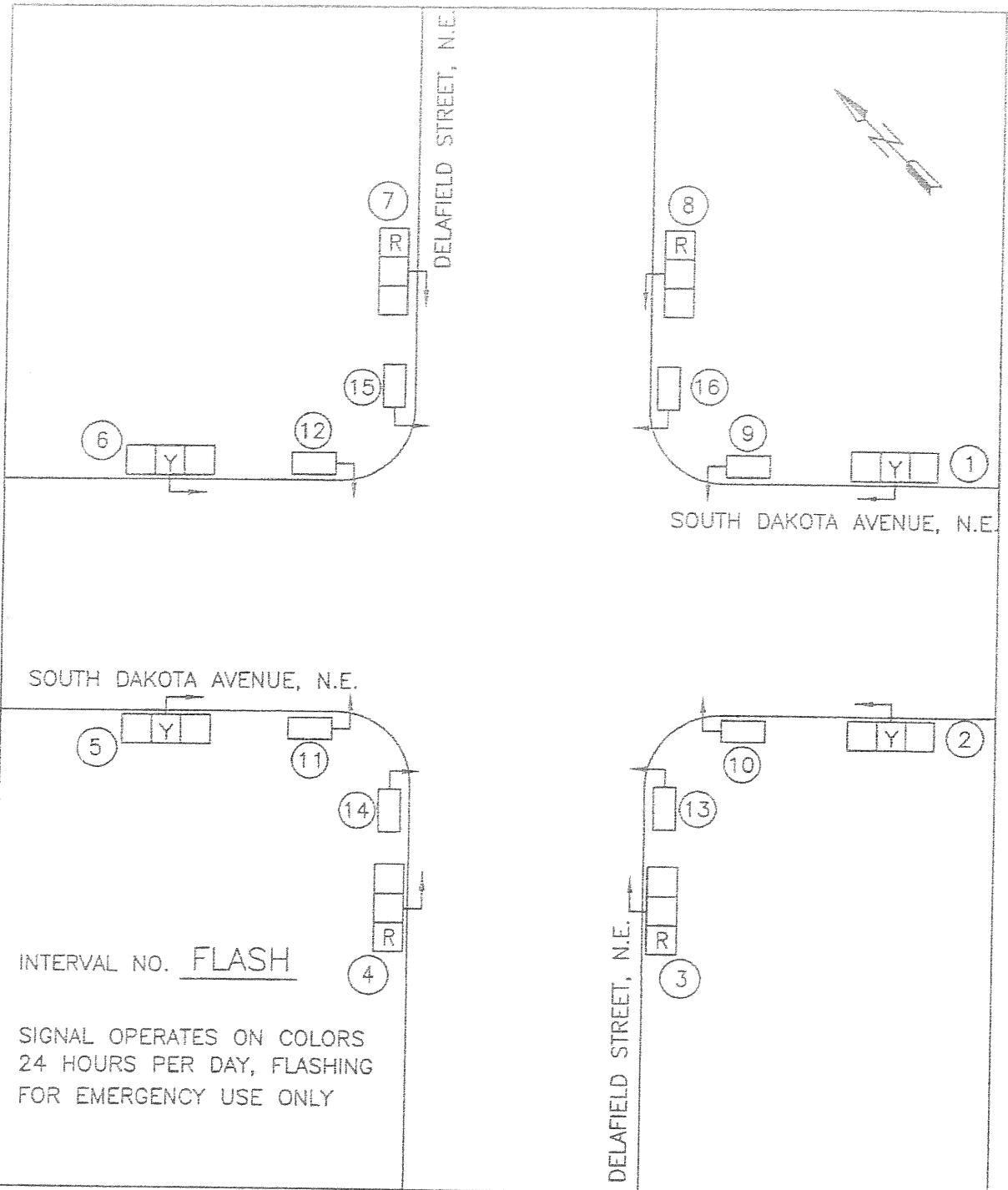
IN SERVICE: \_\_\_\_\_

DESIGNED BY: \_\_\_\_\_

SUBMITTED BY: CHIEF, SIGNAL DESIGN BRANCH

APPROVED BY: DIVISION CHIEF

T.S.  
1240-A  
SHEET  
10 OF 10



TRAFFIC SIGNAL OPERATION		
SOUTH DAKOTA AVENUE AND DELAFIELD STREET, N.E.		
D.C. DEPARTMENT OF TRANSPORTATION TRAFFIC SERVICES ADMINISTRATION TRAFFIC SIGNAL SYSTEM DIVISION	DESIGNED BY: _____	T.S.
	SUBMITTED BY: _____	1240-A
	APPROVED BY: _____	SHEET
	IN SERVICE: _____	FLASH
CHECK BY: _____	CHIEF, SIGNAL DESIGN BRANCH	OF
DRAWN BY: XW	DIVISION CHIEF	

## TIMING PLAN SCHEDULE

**S-DRAWING NO.:**

1000

WASHINGTON, D.C.

BUREAU OF TRAFFIC SERVICES

CONTROL 170

MINIMUM

ACISA

DESCRIPTION

**TIMING PLAN NUMBER**

[illegible]

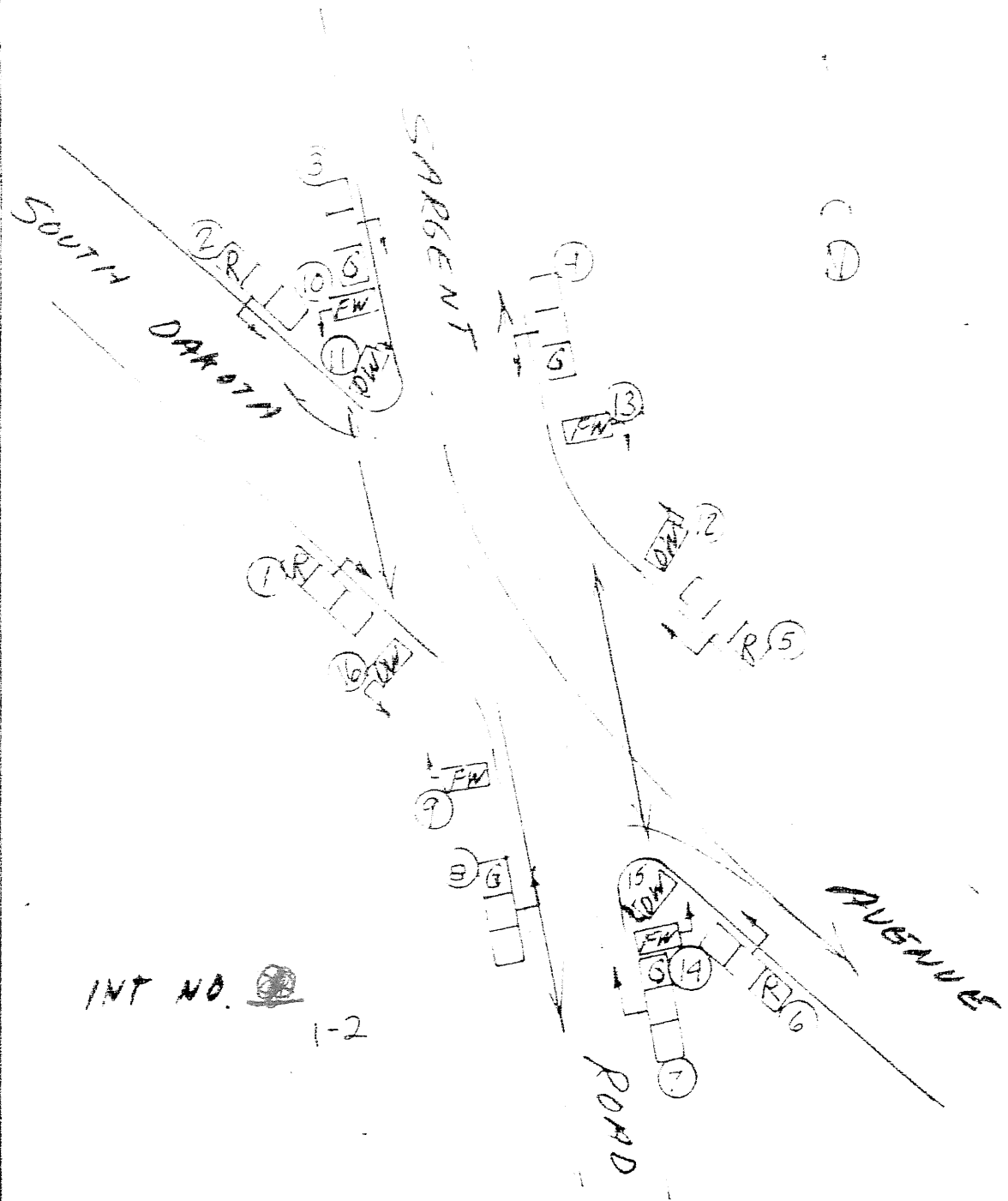
splices=5

C=Cumulative secs

fixed in 10% formalin.

$$V = \text{Variable}/\lambda$$

**Figure 1**

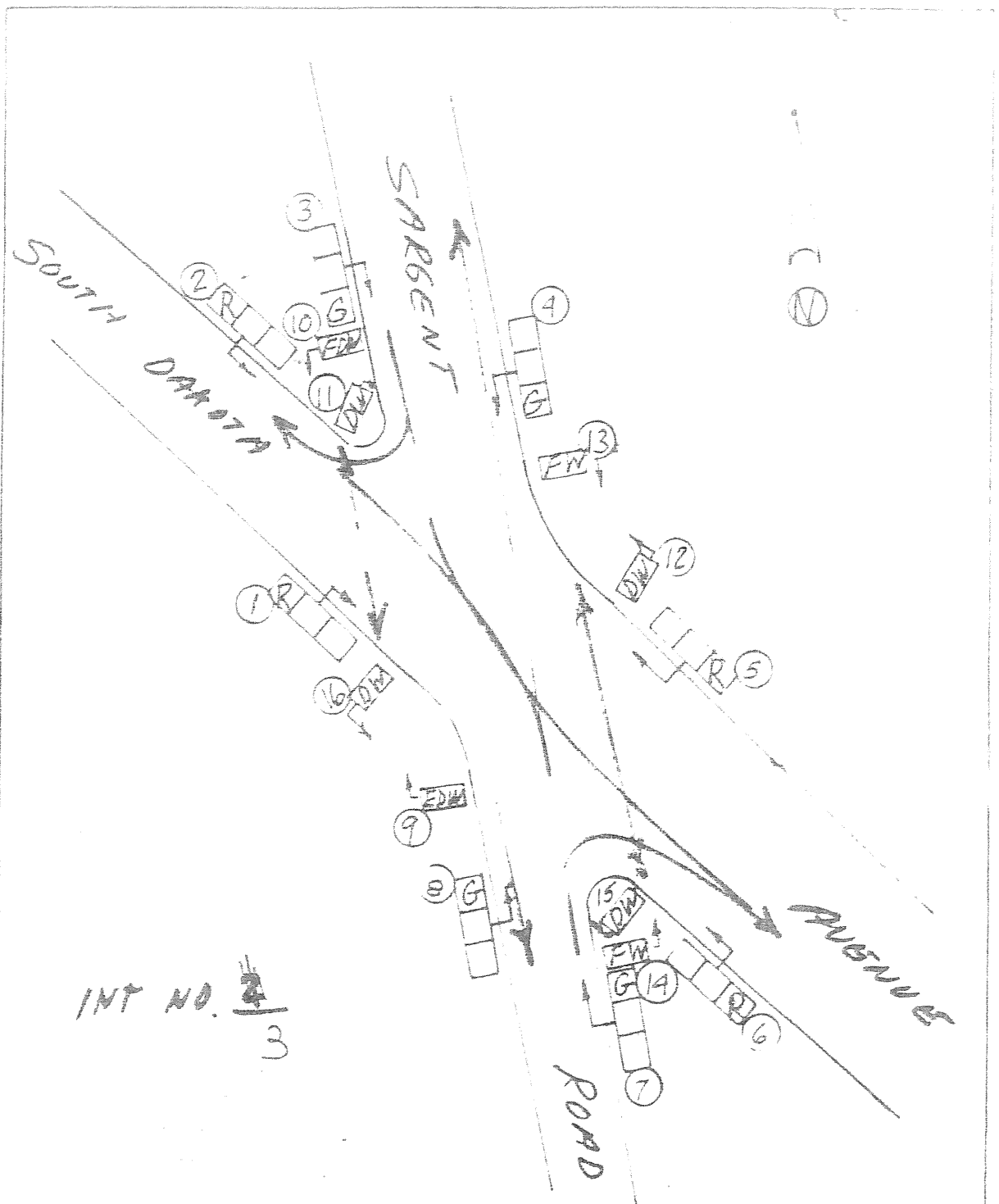


# SIGNAL OPERATION

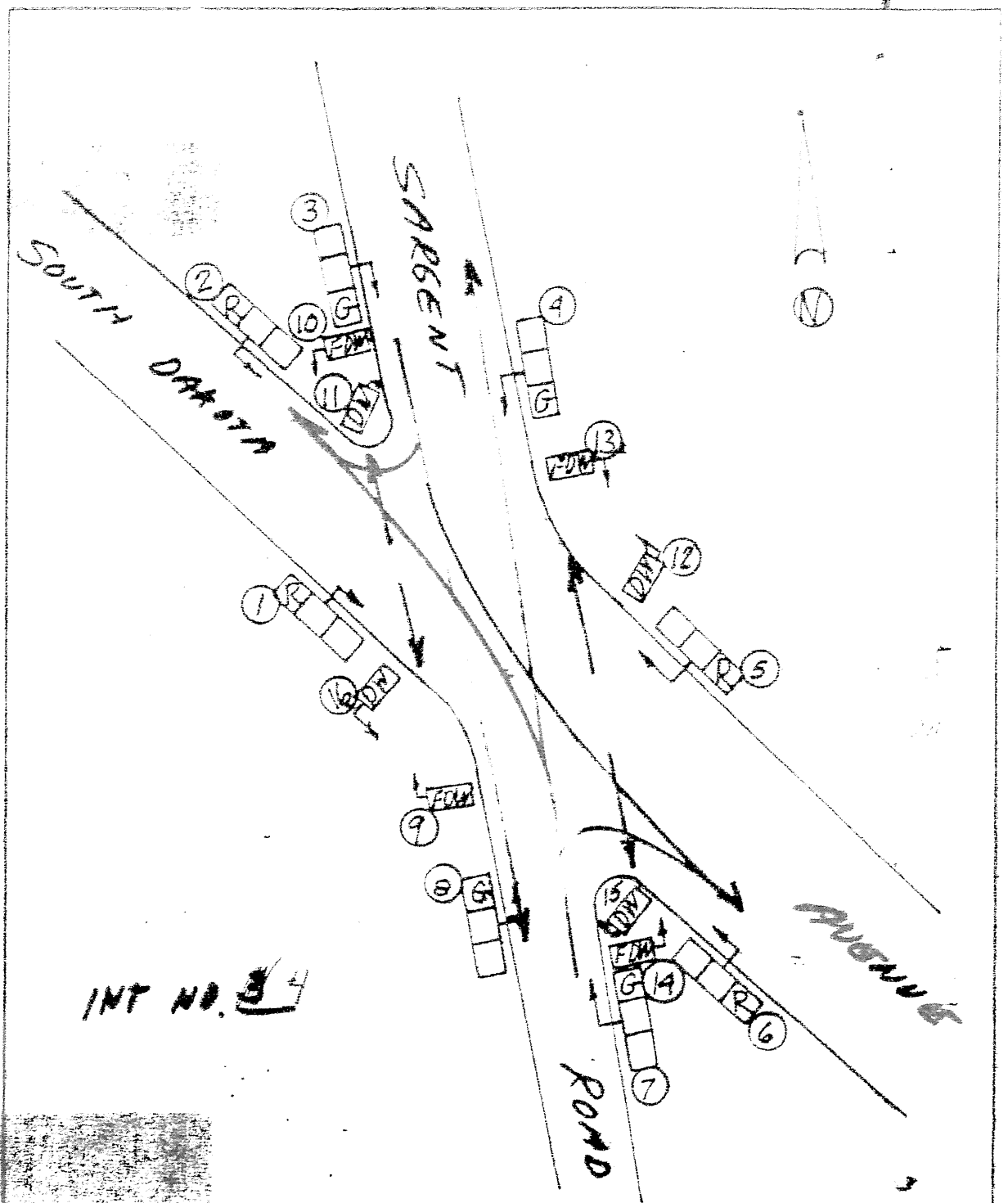
SARGENT ROAD & SOUTH DAKOTA AVENUE, N.E.

DEPARTMENT OF HIGHWAYS & TRAFFIC, D.C. BUREAU OF TRAFFIC ENGINEERING & OPERATIONS		Submitted Head, Signals, Signs & Markings	T. S.
Designed by		Recommended	1092-B
Checked by <u>NR</u>	Date <u>3-23-78</u>	Ass't Traffic Operations Engineer	
Drawn by <u>R. A. Hunt</u>	Date <u>3-17-78</u>	Approved	Sheet 1 of 10
In Service <u>4-12-79</u>	Scale NONE	Traffic Operations Engineer	





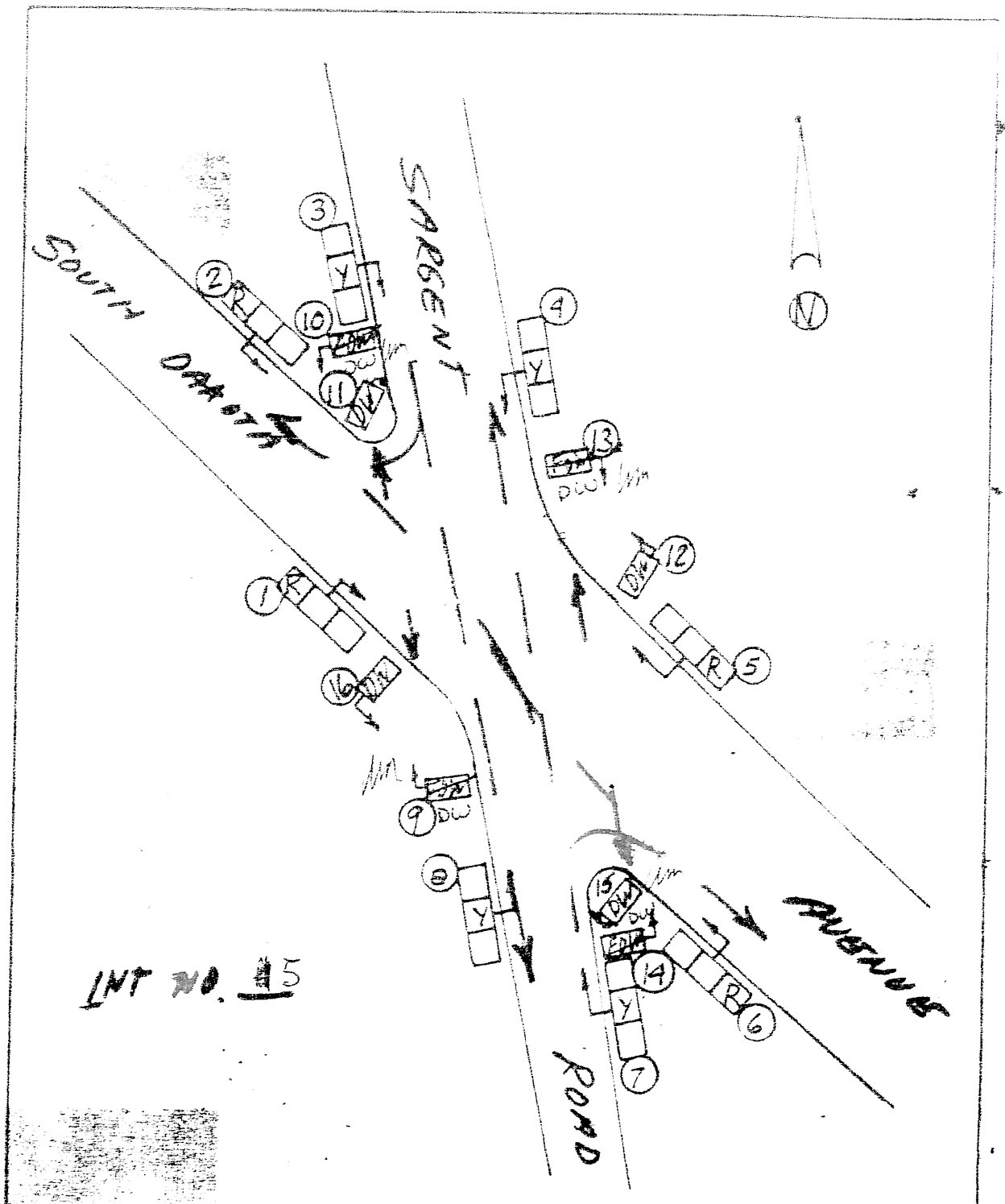
SIGNAL OPERATION			
SARGENT ROAD & SOUTH OAKOTA AVENUE, N.E.			
DEPARTMENT OF HIGHWAYS & TRAFFIC, D. C.		Submitted	T. S.
BUREAU OF TRAFFIC ENGINEERING & OPERATIONS		Head, Signals, Signs & Markings	
Designed by		Recommended	1092-B
Checked by	Date	Asst. Traffic Operations Engineer	
Drawn by	Date	Approved	Sheet
In Service	Scale	Traffic Operations Engineer	2 of 10



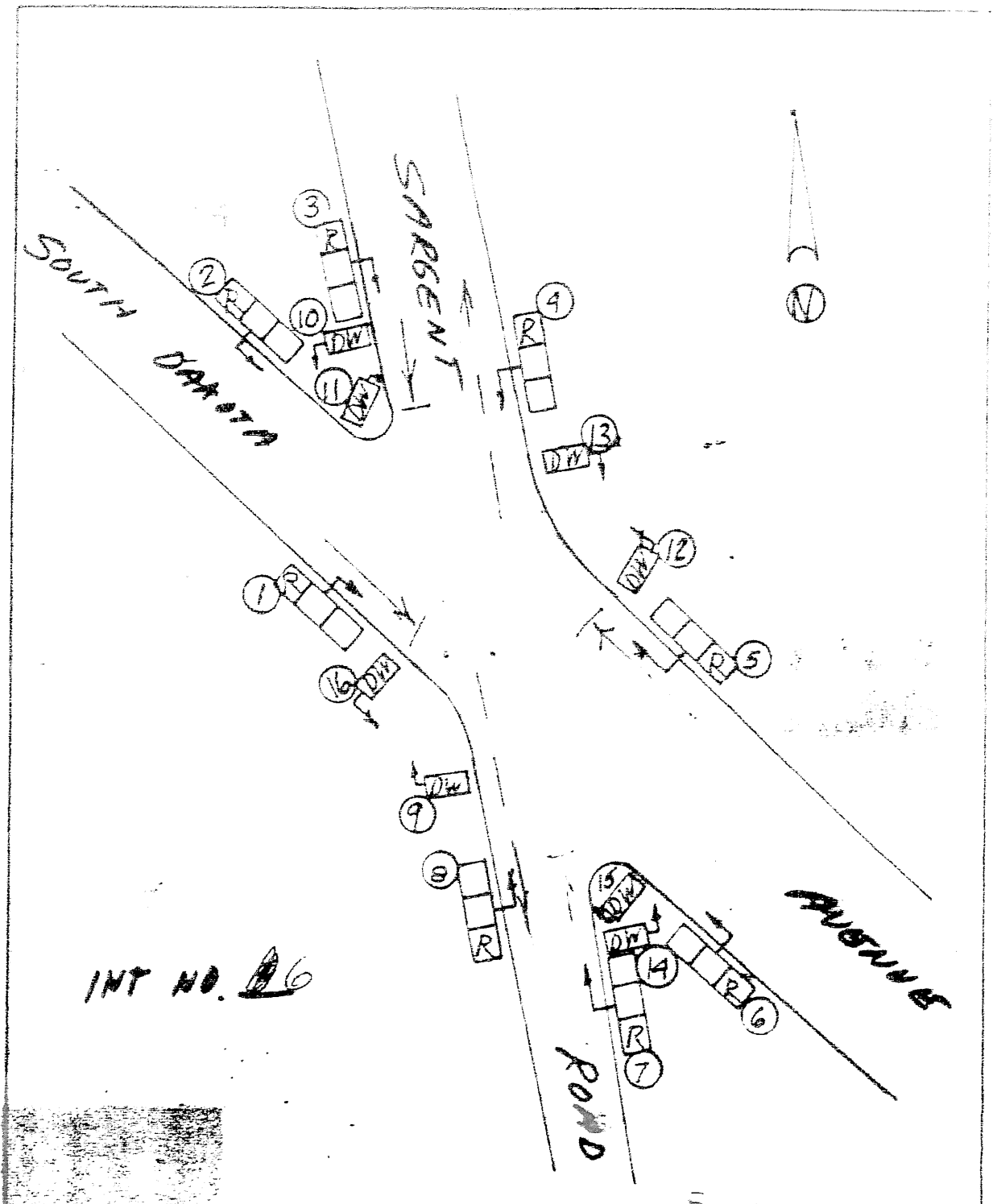
# SIGNAL OPERATION

SARGENT ROAD & SOUTH DAKOTA AVENUE, N.E.

DEPARTMENT OF HIGHWAYS & TRAFFIC, D. C. BUREAU OF TRAFFIC ENGINEERING & OPERATIONS		Submitted Head, Signals, Signs & Markings .....	T. S.
Designed by .....		Recommended .....	1097-B
Checked by .....	Date .....	Asst. Traffic Operations Engineer .....	
Drawn by .....	Date .....	Approved .....	Sheet
In Service .....	Scale NONE	Traffic Operations Engineer .....	3 of 10



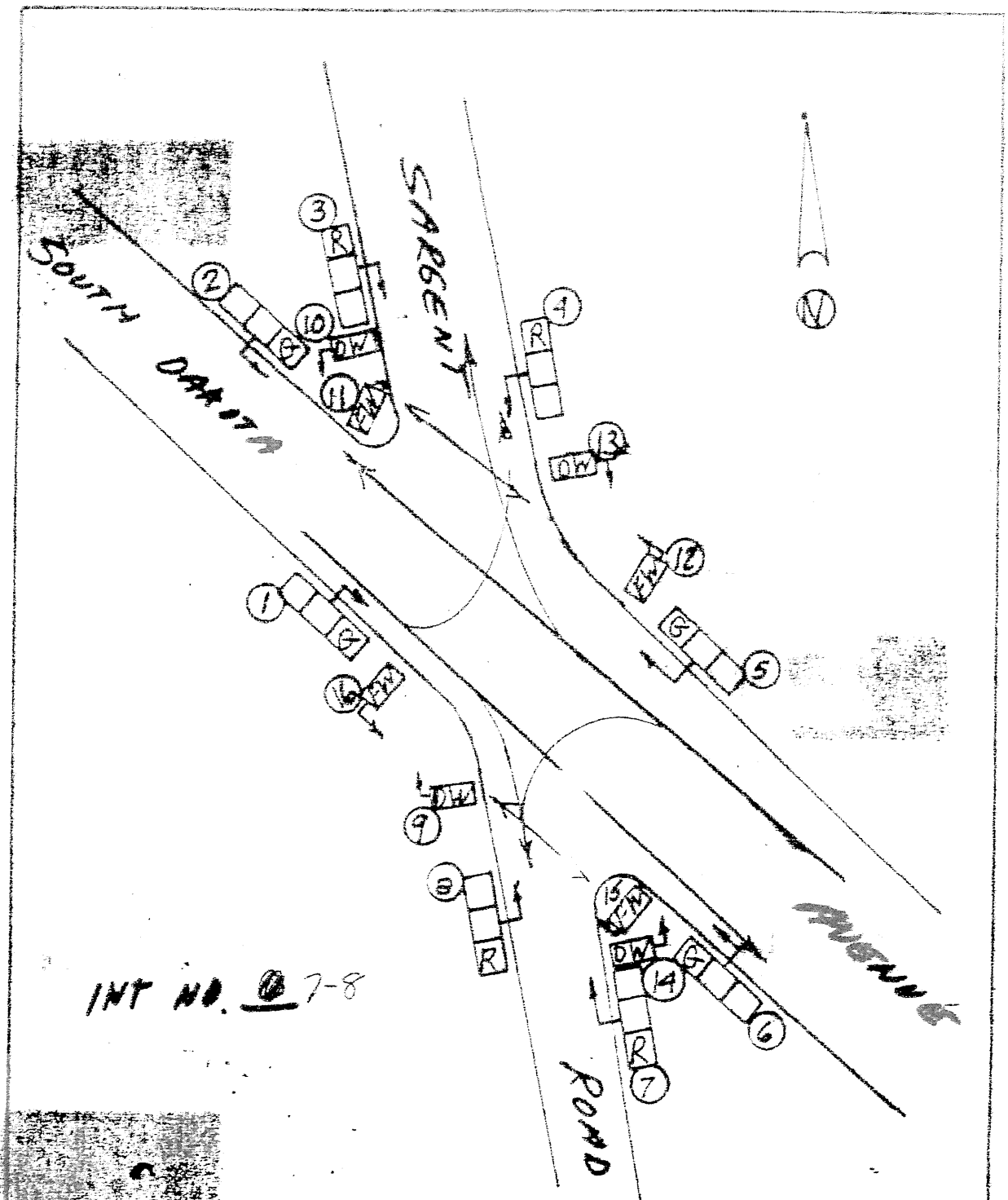
SIGNAL OPERATION			
SARGENT ROAD & SOUTH DAKOTA AVENUE, N.E.			
DEPARTMENT OF HIGHWAYS & TRAFFIC, D. C.		Submitted	T. S.
BUREAU OF TRAFFIC ENGINEERING & OPERATIONS		Head, Signals, Signs & Markings	
Designed by		Recommended	1097B
Checked by		Asst. Traffic Operations Engineer	
Drawn by		Approved	Sheet 4 of 10
In Service		Traffic Operations Engineer	
Scale		NONE	



# SIGNAL OPERATION

SARGENT ROAD & SOUTH DAKOTA AVENUE, N.E.

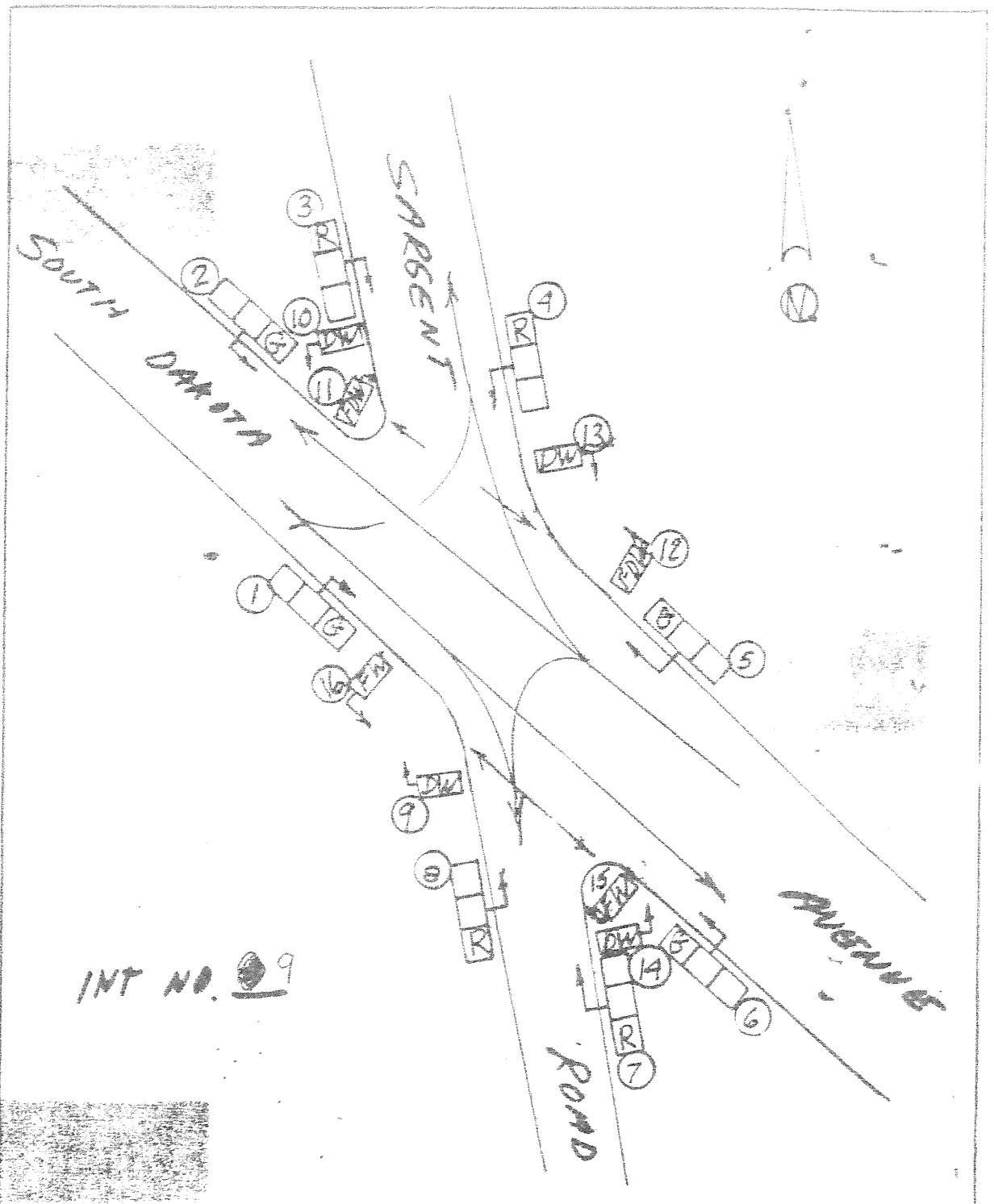
DEPARTMENT OF HIGHWAYS & TRAFFIC, D. C. BUREAU OF TRAFFIC ENGINEERING & OPERATIONS		Submitted Head, Signals, Signs & Markings.....	T. S.
Designed by.....		Recommended Asst. Traffic Operations Engineer.....	1097-B
Checked by.....	Date.....	Approved Traffic Operations Engineer.....	Sheet 5 of 10
Drawn by.....	Date.....		
In Service	Scale		



# SIGNAL OPERATION

SARGENT ROAD & SOUTH DAKOTA AVENUE, N.E.

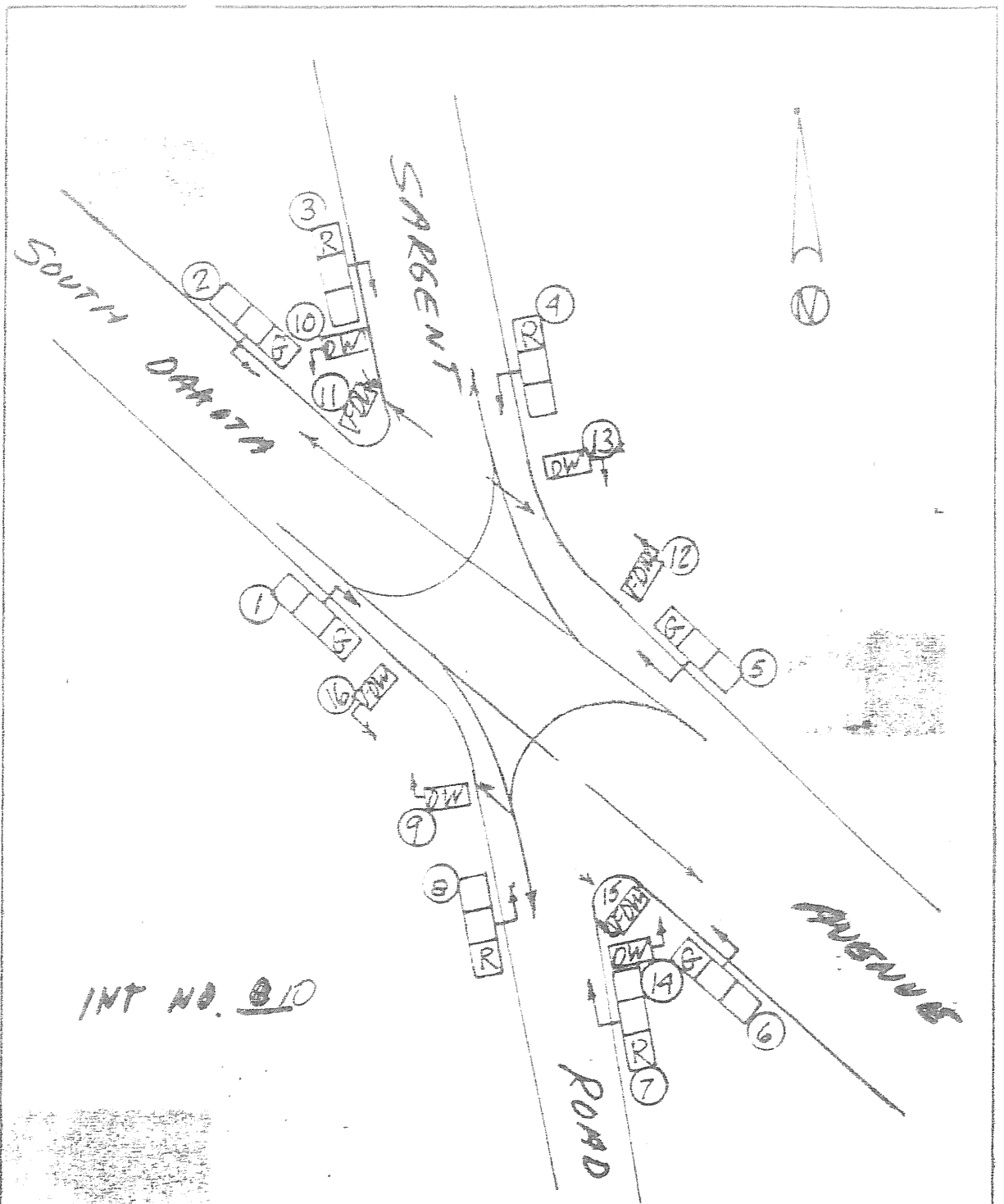
DEPARTMENT OF HIGHWAYS & TRAFFIC, D. C.		Submitted	T. &
BUREAU OF TRAFFIC ENGINEERING & OPERATIONS		Head, Signals, Signs & Markings	
Designed by		Recommended	1097-B
Checked by	Date	Asst. Traffic Operations Engineer	
Drawn by	Date	Approved	Sheet 6 of 10
In Service	Scale NONE	Traffic Operations Engineer	



# SIGNAL OPERATION

SARGENT ROAD & SOUTH DAKOTA AVENUE, N.E.

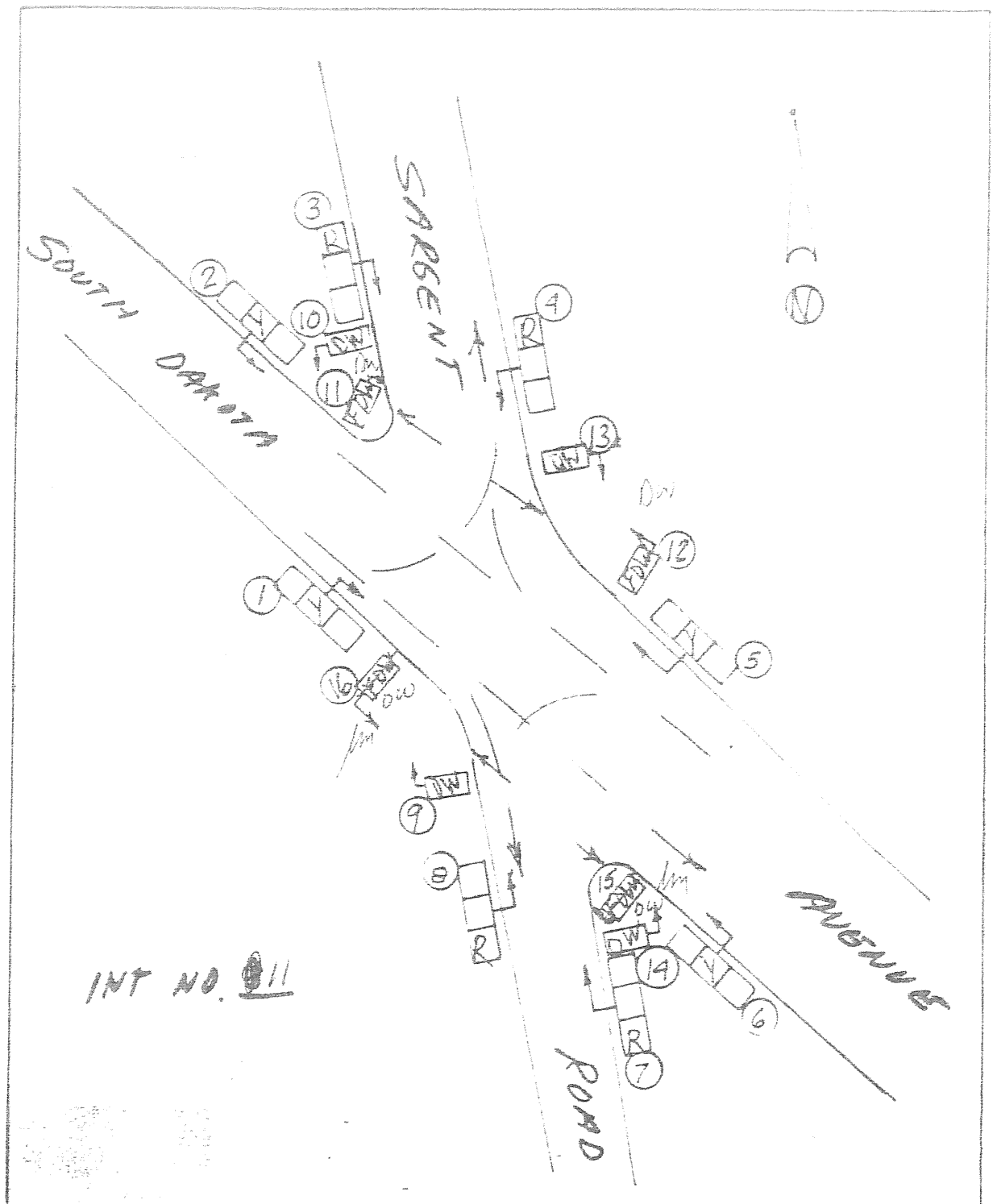
DEPARTMENT OF HIGHWAYS & TRAFFIC, D. C.		Submitted	T. S.
BUREAU OF TRAFFIC ENGINEERING & OPERATIONS		Head, Signals, Signs & Markings	
Designed by		Recommended	1097-B
Checked by	Date	Asst. Traffic Operations Engineer	
Drawn by	Date	Approved	Sheet 7 of 10
In Service	Scale NONE	Traffic Operations Engineer	



# SIGNAL OPERATION

SARGENT ROAD & SOUTH OAKOTA AVENUE, N.E.

DEPARTMENT OF HIGHWAYS & TRAFFIC, D. C. BUREAU OF TRAFFIC ENGINEERING & OPERATIONS		Submitted Head, Signals, Signs & Markings.....	T. S.  1097-B
Designed by		Recommended Asst. Traffic Operations Engineer.....	
Checked by	Date	Approved	Sheet 3 of 10
Drawn by	Date	Traffic Operations Engineer.....	
In Service	Scale	NONE	

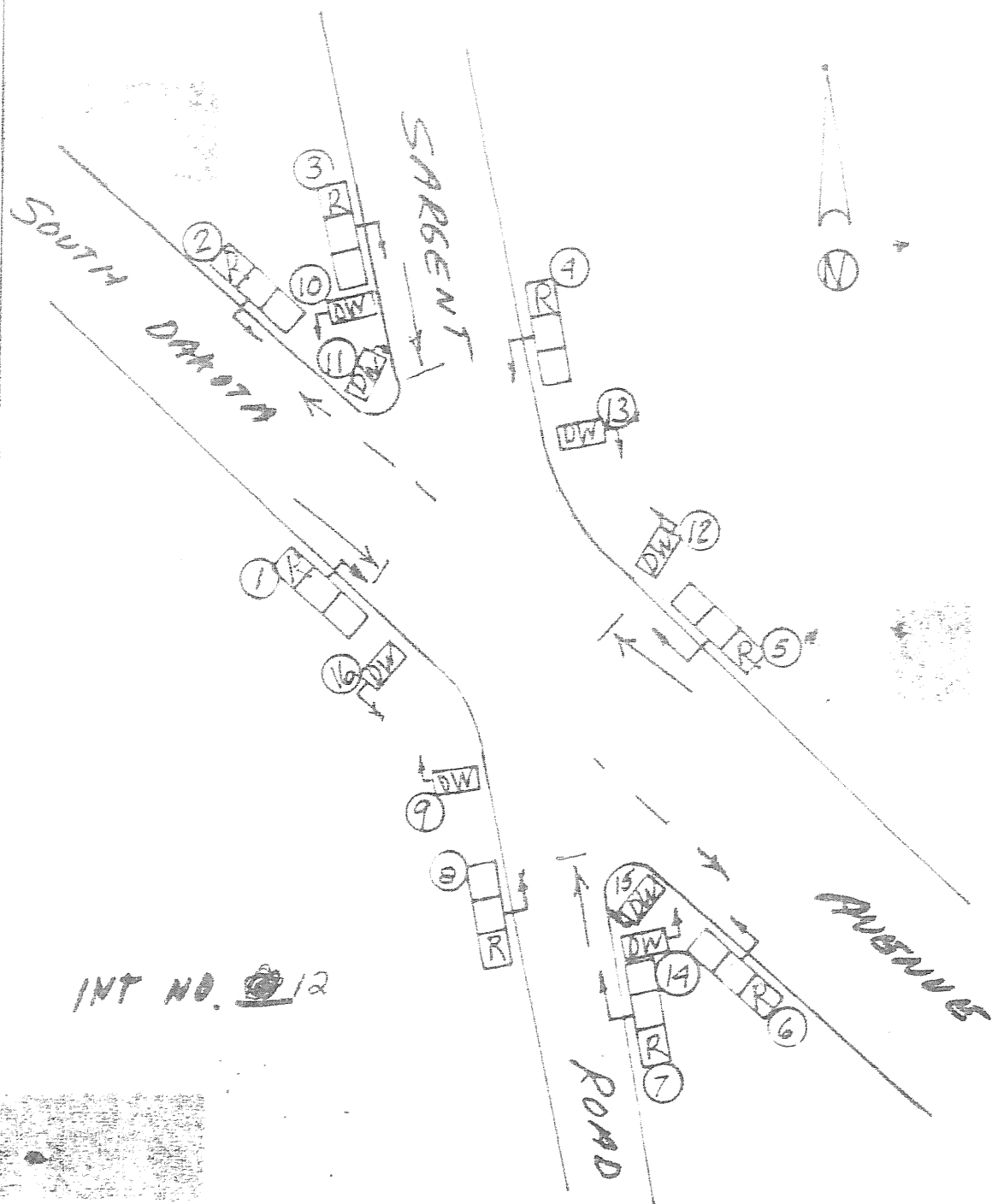


# SIGNAL OPERATION

SARGENT ROAD & SOUTH DAKOTA AVENUE, N.E.

DEPARTMENT OF HIGHWAYS & TRAFFIC, D. C. BUREAU OF TRAFFIC ENGINEERING & OPERATIONS		Submitted Head, Signals, Signs & Markings .....	T. S.  1097-B
Designed by .....		Recommended Asst. Traffic Operations Engineer .....	
Checked by .....	Date .....	Approved .....	Sheet 9 of 10
Drawn by .....	Date .....	Traffic Operations Engineer .....	
In Service .....	Scale NONE		





# SIGNAL OPERATION

SARGENT ROAD & SOUTH DAKOTA AVENUE, N.E.

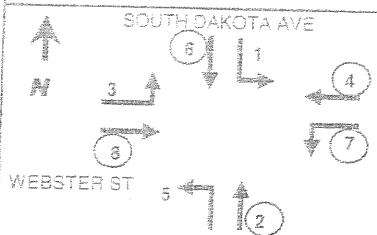
DEPARTMENT OF HIGHWAYS & TRAFFIC, D. C. BUREAU OF TRAFFIC ENGINEERING & OPERATIONS		Submitted Head, Signals, Signs & Markings .....	T. S.  1097-B
Designed by .....		Recommended Asst. Traffic Operations Engineer .....	
Checked by .....	Date .....	Approved .....	Sheet
Drawn by .....	Date .....	Traffic Operations Engineer .....	10 of 10
In Service	Scale	NONE	



# 14TH STREET, SOUTH DAKOTA AVENUE, AND WEBSTER STREET, N.E.

SHEET NO. 6

LOCATION



ISNUM: 673

DEPARTMENT OF TRANSPORTATION

WASHINGTON, D.C.

TRAFFIC SERVICES ADMINISTRATION

TRAFFIC SIGNAL SYSTEM DIVISION

F+COLOR CODE+OVERLAP

Green 9 Yellow C Red D

OVERLAP

A			
B			
C			
D			

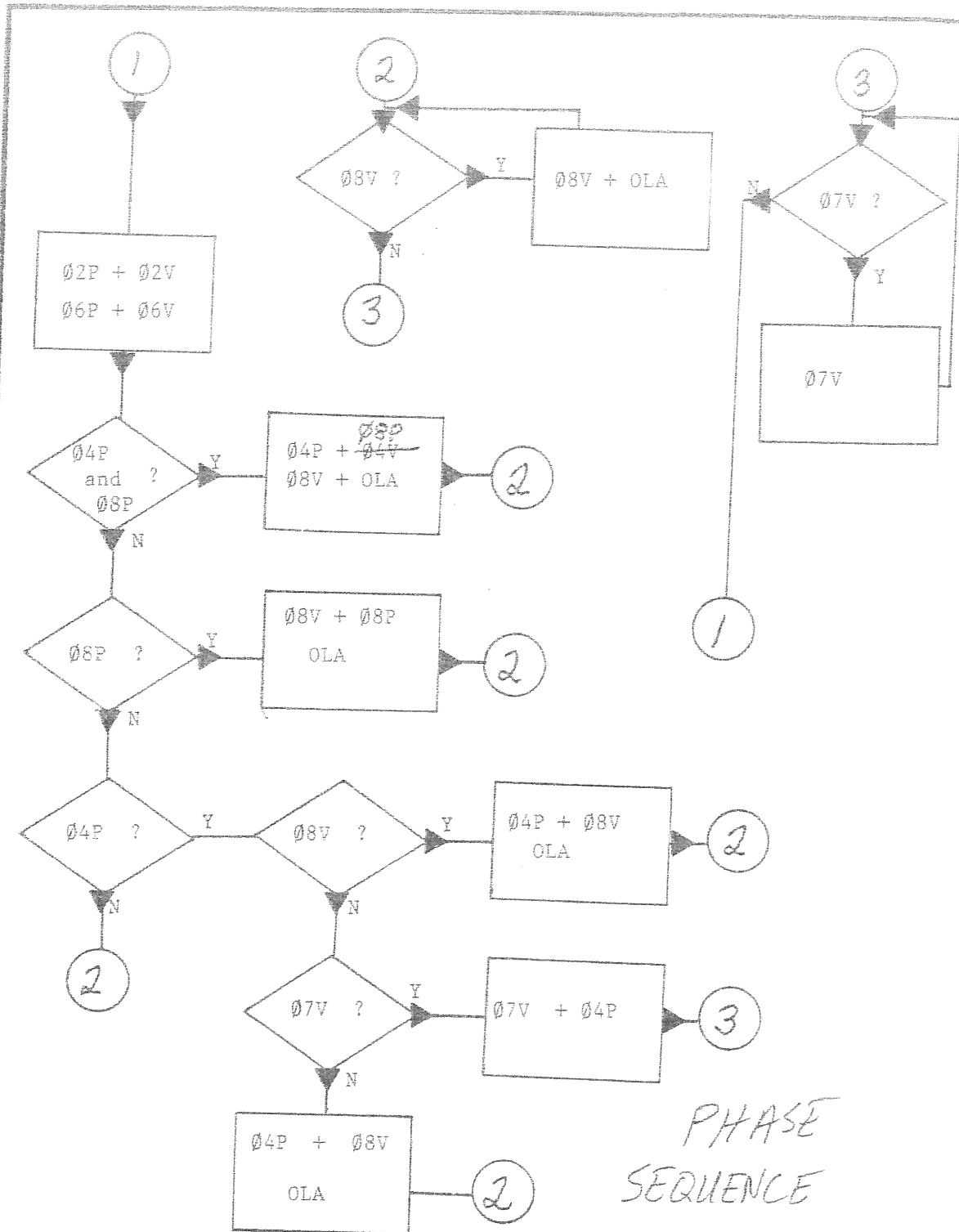
TS- 1241-B	DRAWING NO. S-1350-B	ACISA NO. 3201	C + F - FUNCTION	PHASE
F + PHASE + <INTVL>	PHASE	PREEMPT	FUNCTION	1 2 3 4 5 6 7 8
INTERVAL	1 2 3 4 5 6 7 8	F + 0 + <INTERVAL>	LAG	0 X X X X
WALK	0 6 6 6 6 6 6 6	RR1 DELAY	COOR PED RECALL	9 X X X
FLASH D/W	1 11 11 3 2	RR1 CLEAR	COOR MAX RECALL	A X X
MIN GREEN	2 6 7 6 7 7	EVA DELAY	COOR LAG RECALL	B X X
MAX INIT	3 6 7 6 7 7	EVA CLEAR	SYNC PHASES	C X X
ADD PER VEH	4	EVB DELAY	E - S - FEATURE	PHASE
VEH EXTEN	5 1 3 1 3 3	EVB CLEAR	FEATURE	1 2 3 4 5 6 7 8
MAX GAP	6 1 3 1 3 3	EVC DELAY	RR1 GRN CLR	1 X
MIN GAP	7 1 3 1 3 3	EVC CLEAR	RR2 GRN CLR	2 X
MAXIMUM	8 18 18 18 9 18	EVD DELAY	RR2 LTD	3 X
MAXIMUM 2	9 5 42 5 42 42	EVD CLEAR	OVERLAP PL YEL	9 X
(Reserved)	A	RR2 DELAY	EM VEH A	A X
RED REVERT	B	RR2 CLEAR	EM VEH B	B X
REDUCE BY	C	EV CLR TMR	EM VEH C	C X
REDUCE EVERY	D	EV DLY TMR	EM VEH D	D X
YELLOW	E 4 4 4 4 4	RR CLR TMR	EXTRA	E X
RED CLEAR	F 2 3 2 1 3	RR DLY TMR	IC SELECT	F X

C + PLAN + PHASE	CYCLE	KEYSTROKES: C + PLAN + PHASE	OFFSET
PLAN	0	1 2 3 4 5 6 7 8	A
OFF PEAK - 70	1 70	0 37 8 49 34	32
AM PEAK - 80	2 80	0 37 8 49 34	25
PM PEAK - 80	3 80	0 37 8 49 34	65
OFF PEAK - 100	4 100	0 37 8 49 34	68
AM PEAK - 100	5 100	0 37 8 49 34	10
PM PEAK - 100	6 100	0 37 8 49 34	51
AM PEAK - 120	7 120	0 37 8 49 34	17
PM PEAK - 120	8 120	0 37 8 49 34	82

C + PLAN + D	PERMISSIVE	F + C + O	ALL RED
PERMIT	0 X X X X X	CALL ALL	C X X X X X
RED LOCK	1 X X X X X	FIRST CALL	D X X X X X
YELLOW LOCK	2 X X X X X	STARTUP	E X X X X X
VEH RECALL	3 X X X X X	FIRST PHASES	F X X X X X
PED RECALL	4 X X X X X		
PEDESTRIAN	5 X X X X X		
REST IN WALK	6 X X X X X		
RED REST	7 X X X X X		
DOUBLE ENTRY	8 X X X X X		
MAX RECALL	9 X X X X X		
FLASH WALK	A X X X X X		
RESERVED	B X X X X X		

PREPARED BY: *William W. McQuinn*  
 WORK/SHOP ORDER NO: *507-0510*  
 APPROVED BY: *William W. McQuinn* 7/27/05  
 DATE INSTALLED: *7/27/05*  
 INSTALLED BY: *William W. McQuinn*





### TRAFFIC SIGNAL OPERATION

14<sup>th</sup> Street, South Dakota Avenue and Webster Street, N.E

DEPARTMENT OF PUBLIC WORKS, D.C.  
BUREAU OF TRAFFIC SERVICES  
TRAFFIC SIGNAL SYSTEM DIVISION

CHECKED BY	DATE
DRAWN BY	DATE
IN SERVICE	SCALE NONE

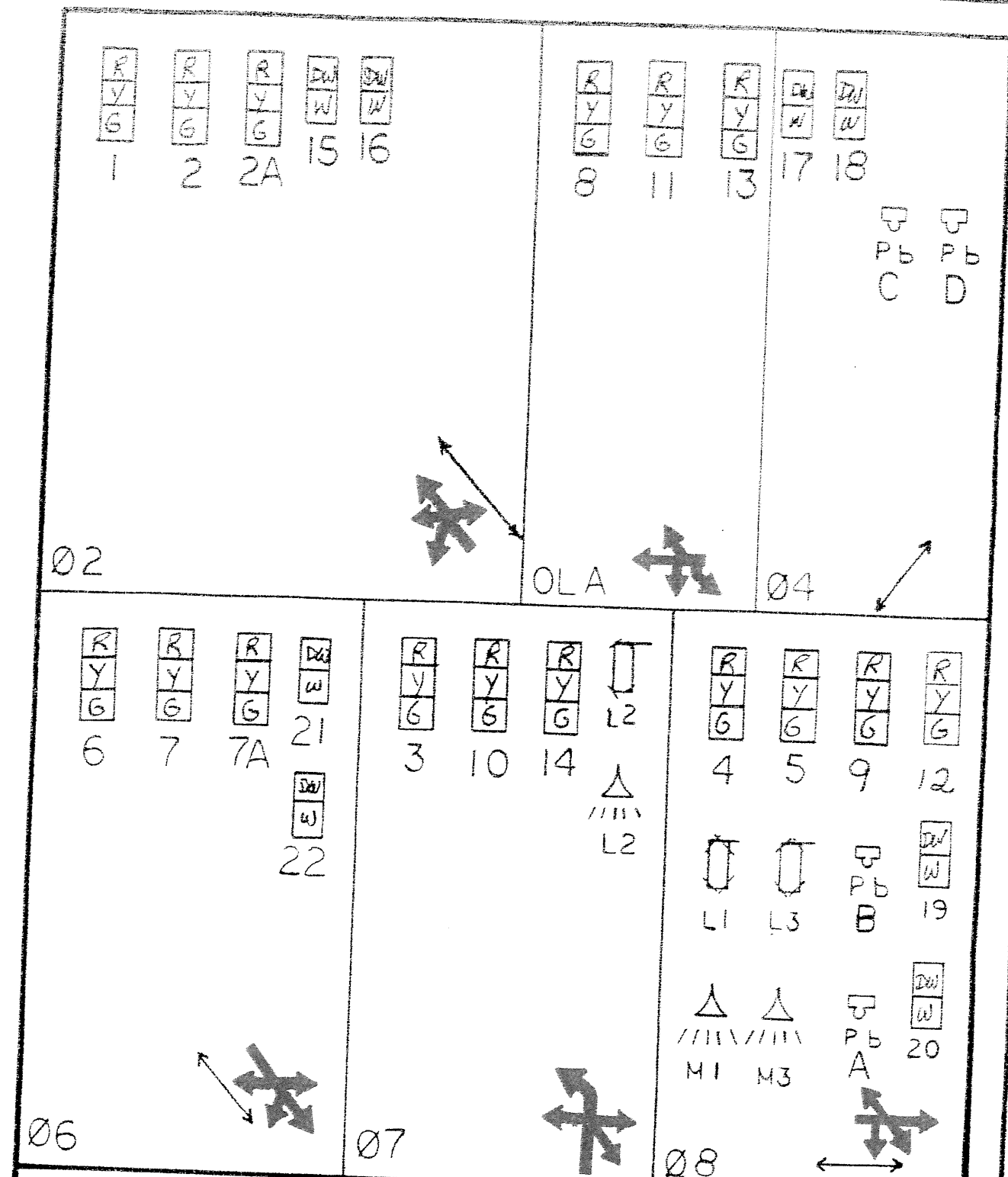
DESIGNED BY:	
SUBMITTED BY:	CHIEF, SIGNAL DESIGN BRANCH
APPROVED BY:	DIVISION CHIEF

T.S.

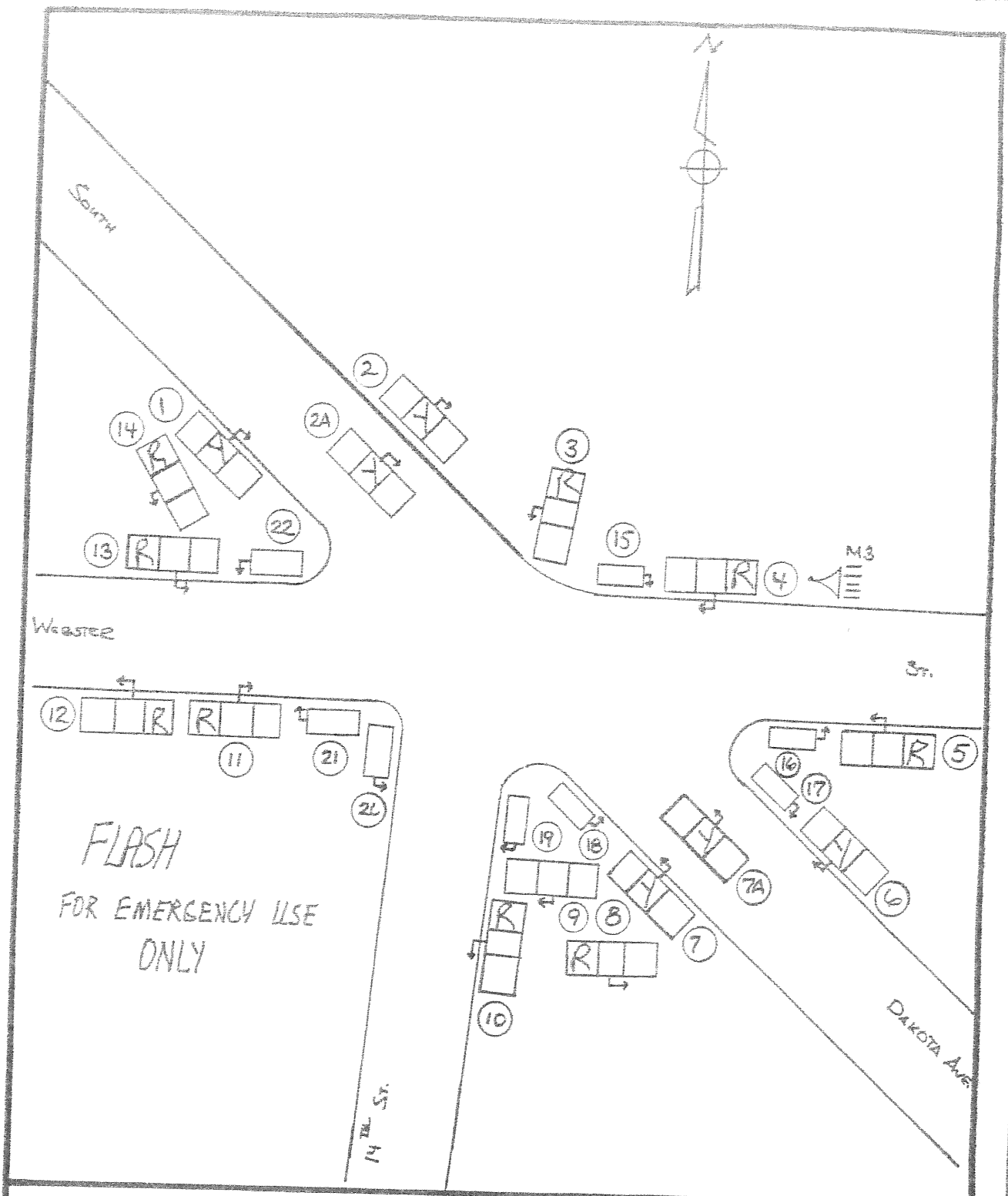
1241-B

SHEET

2 OF 4



<p align="center"><b>TRAFFIC SIGNAL OPERATION</b></p> <p align="center">14<sup>th</sup> STREET, SOUTH DAKOTA AVENUE, &amp; WEBSTER STREET, N.E</p>			
<p>DEPARTMENT OF PUBLIC WORKS, D.C.</p> <p>BUREAU OF TRAFFIC SERVICES</p> <p>TRAFFIC SIGNAL SYSTEM DIVISION</p>		<p>DESIGNED BY: _____</p> <p>SUBMITTED BY: _____</p> <p>CHIEF, SIGNAL DESIGN BRANCH</p> <p>APPROVED BY: _____</p> <p>DIVISION CHIEF</p>	<p>T.S.</p> <p>1247B</p> <p>SHEET</p> <p>3 of 4</p>
CHECKED BY	DATE		
DRAWN BY	DATE		
IN SERVICE	SCALE NONE		



# TRAFFIC SIGNAL OPERATION 14TH STREET, SOUTH DAKOTA AVENUE & WEBSTER STREET N.E.

DEPARTMENT OF PUBLIC WORKS, D.C.  
BUREAU OF TRAFFIC SERVICES  
TRAFFIC SIGNAL SYSTEM DIVISION

CHECKED BY	DATE
DRAWN BY	DATE
IN SERVICE	SCALE NONE

DESIGNED BY:

SUBMITTED BY:

CHIEF, SIGNAL DESIGN BRANCH

APPROVED BY:

DIVISION CHIEF

T.S.

1241-B

SHEET

4 OF 4



# 12TH STREET AND SOUTH DAKOTA AVENUE, NE

SHEET NO. 2

LOCATION

DEPARTMENT OF PUBLIC WORKS

WASHINGTON, D.C.

BUREAU OF TRAFFIC SERVICES

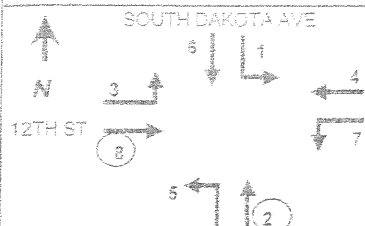
TRAFFIC SIGNAL SYSTEM DIVISION

F+COLOR CODE+OVERLAP

Green 9 Yellow C Red D

OVERLAP

A			
B			
C			
D			



ISNUM: 672

TS- 719	DRAWING NO.	ACISA NO. 3200	C + F + FUNCTION	PHASE
F + PHASE + <INTVL>	PHASE	PREEMPT	FUNCTION	1 2 3 4 5 6 7 8
INTERVAL	1 2 3 4 5 6 7 8	F + 0 + <INTERVAL>	LAG	0 X X X X
WALK	0 0	7 RR1 DELAY	COOR PED RECALL	9 X
FLASH D/W	1 0	10 RR1 CLEAR	COOR MAX RECALL	A X
MIN GREEN	2 27	7 EVA DELAY	COOR LAG RECALL	B X
MAX INIT	3 27	7 EVA CLEAR	SYNO PHASES	C X
ADD PER VEH	4 0	0 EVB DELAY	E + E + FEATURE	PHASE
VEH EXTEN	5 1	3 EVB CLEAR	FEATURE	1 2 3 4 5 6 7 8
MAX GAP	6 1	3 EVC DELAY	RR1 GRN CLR	1
MIN GAP	7 1	3 EVC CLEAR	RR2 GRN CLR	2
MAXIMUM	8 51	21 EVD DELAY	RR2 LTD	3
MAXIMUM 2	9 26	45 EVD CLEAR	OVERLAP FL YEL	9
(Reserved)	A	RR2 DELAY	EM VEH A	A
RED REVERT	B 5	5 RR2 CLEAR	EM VEH B	B
REDUCE BY	C 0	0 EV CLR TMR	EM VEH C	C
REDUCE EVERY	D 0	0 EV DLY TMR	EM VEH D	D
YELLOW	E 4	4 RR CLR TMR	EXTRA	E
RED CLEAR	F 0	0 RR DLY TMR	IC SELECT	F
C + PLAN + PHASE	CYCLE	KEYSTROKES: C + PLAN + PHASE	OFFSET	E + F + FEATURE
PLAN	0	1 2 3 4 5 6 7 8	A	PHASE
OFF PEAK - 70	1 70	25 21	OVERLAP A	A
AM PEAK - 80	2 80	25 7	OVERLAP B	B
PM PEAK - 80	3 80	25 13	OVERLAP C	C
OFF PEAK - 100	4 100	25 54	OVERLAP D	D
AM PEAK - 100	5 100	25 24	SP FUNCT FLASH	E
PM PEAK - 100	6 100	25 71	SP FUNCT MASK	F
AM PEAK - 120	7 120	25 107	F + F + FEATURE	PHASE
PM PEAK - 120	8 120	25 92	FEATURE	1 2 3 4 5 6 7 8
			PERMIT	0 X X
			RED LOCK	1 X
			YELLOW LOCK	2 X X
			VEH RECALL	3 X
			PED RECALL	4 X
			PEDESTRIAN	5
			PED IN WALK	6
			RED REST	7
			DOUBLE ENTRY	8
			MAX RECALL	9 X
			FLASH WALK	A
			RESERVED	B
			CALL ALL	C X X
			FIRST CALL	D X X
			STARTUP	E
			FIRST PHASES	F X

NOTES: PEDESTRIAN ACTUATION EXTENDS GREEN MODIFIED FOR PEDESTRIAN RECALL 1/6/89. CORRECTED 08 1/6/89

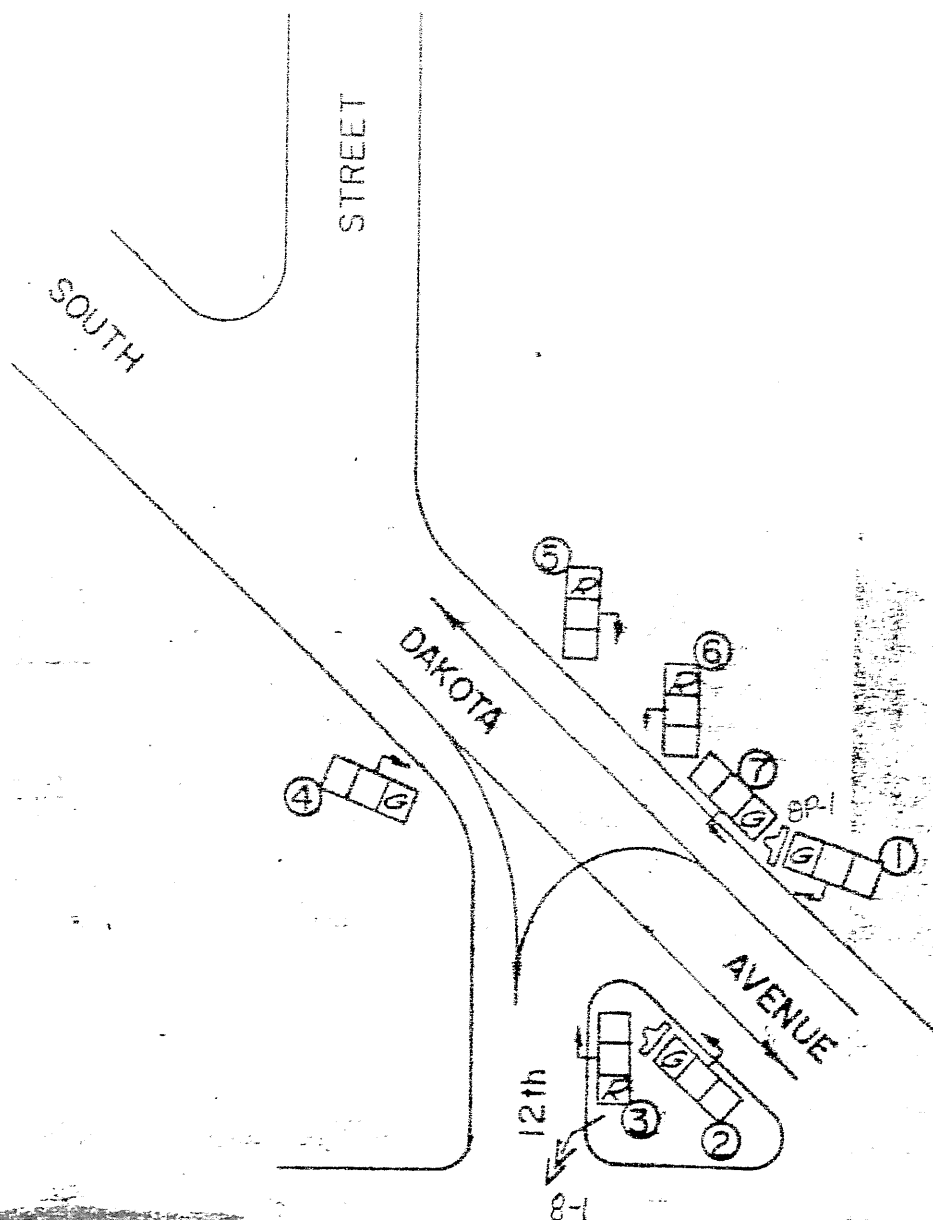
C+PLAN=0 PERMISSIVE

F+C+O ALL RED

4

PREPARED BY: Navin Jain  
 WORKSHOP ORDER NO: 501-04-01  
 APPROVED BY: William W. McQuinn 1/31/04  
 DATE INSTALLED: 2/23/04  
 INSTALLED BY: Bo Yuan





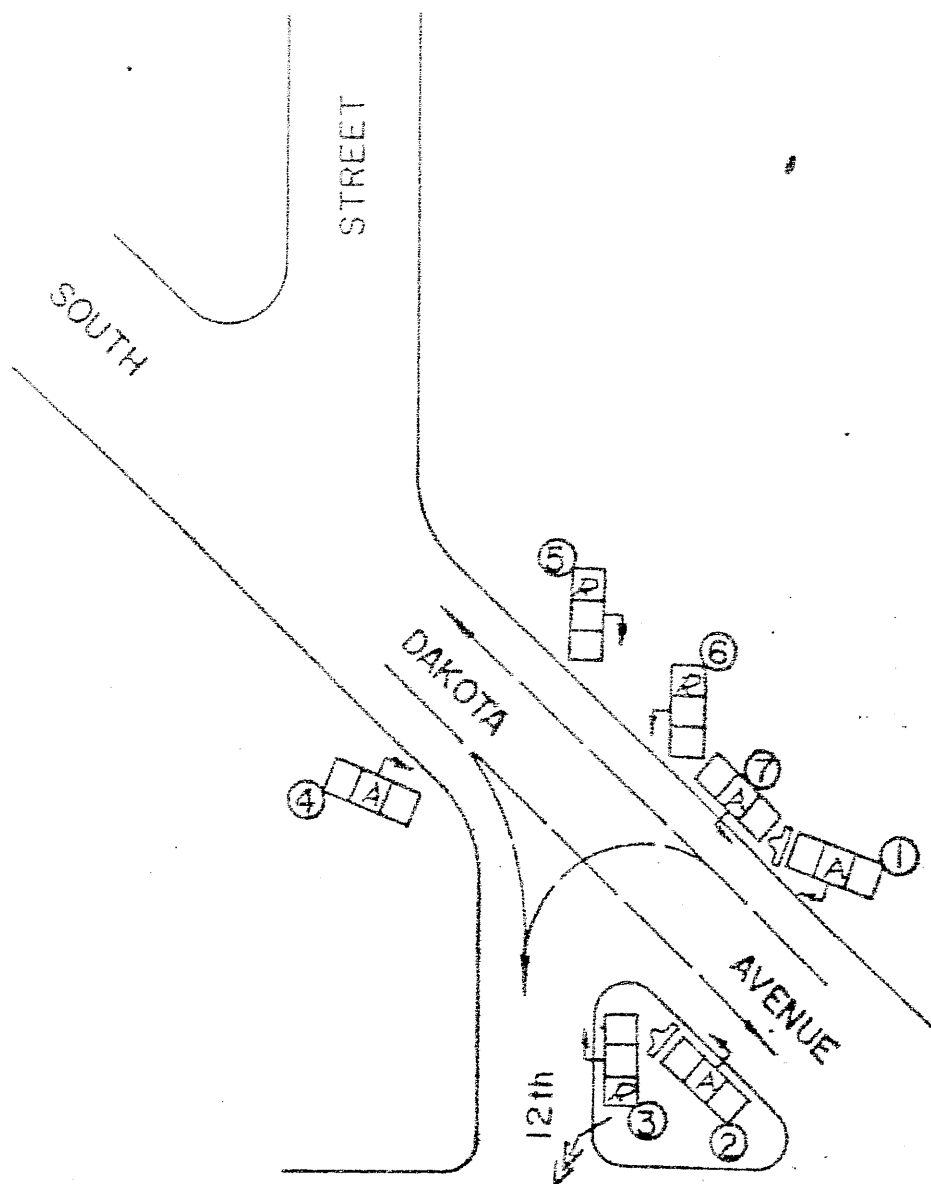
PHASE NO. A

ULTRA-SONIC MOTION DETECTOR

SIGNAL OPERATION

12th ST. & SOUTH DAKOTA AVE., N.E.

DEPARTMENT OF HIGHWAYS & TRAFFIC, D. C. BUREAU OF TRAFFIC ENGINEERING & OPERATIONS		Submitted Traffic Signal Engineer	T. S.
Designed by <i>Stockfish</i>	Date <i>9/3/65</i>	Recommended Traffic Control Engineer	719
Checked by <i>Stockfish</i>	Date	Approved	
In Service <i>4-27-66</i>	Scale <i>NONE</i>	Traffic Operations Engineer	7134



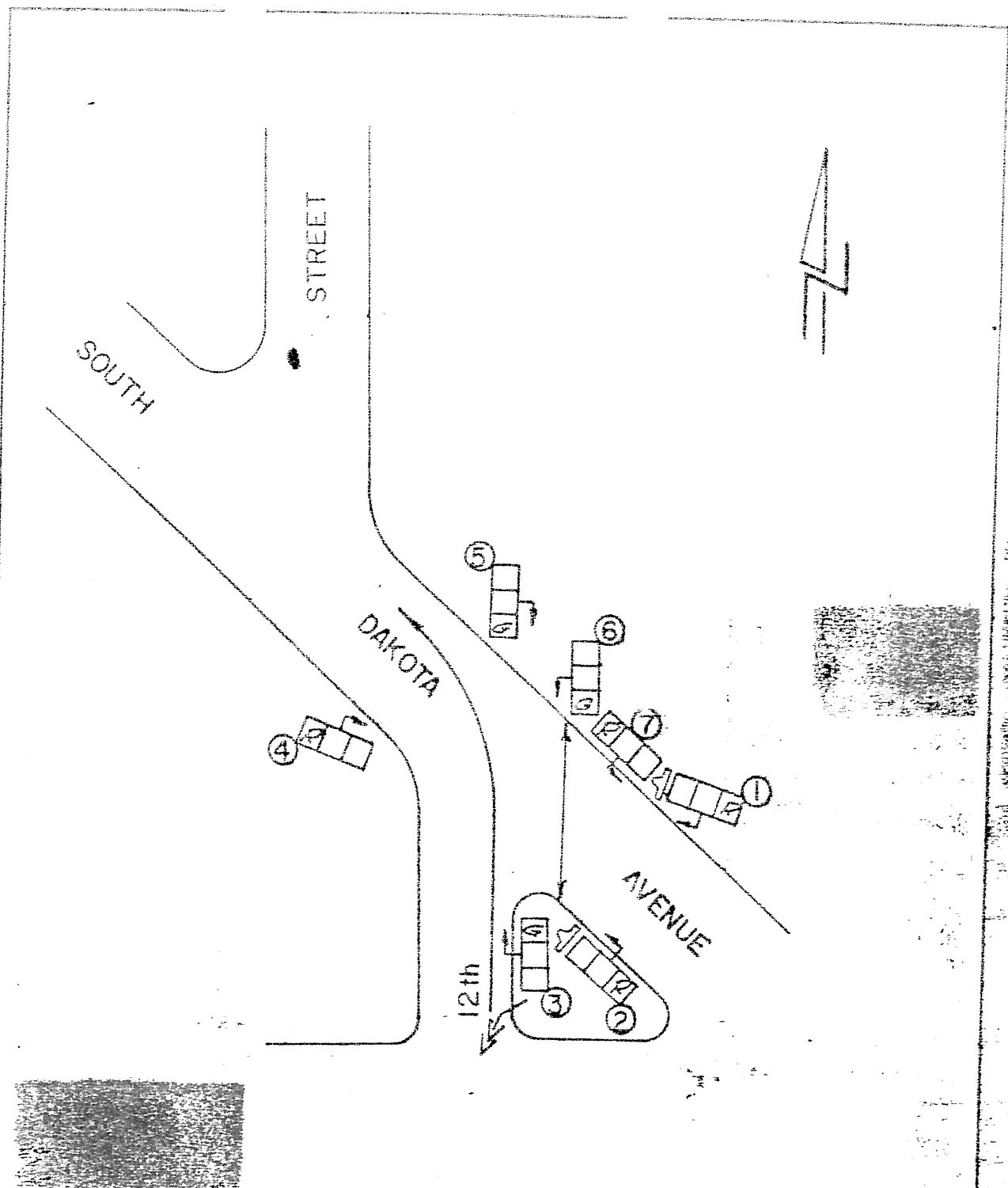
PHASE NO. A (CLEARANCE)

SIGNAL OPERATION  
12th ST. & SOUTH DAKOTA AVE., N.E.

DEPARTMENT OF HIGHWAYS & TRAFFIC, D. C.		Submitted	T. S.
BUREAU OF TRAFFIC ENGINEERING & OPERATIONS		Traffic Signal Engineer	
Designed by		Recommended	719-
Checked by	Date	Traffic Control Engineer	
Drawn by	Date	Approved	Sheet
In Service	Scale	Traffic Operations Engineer	

NONE

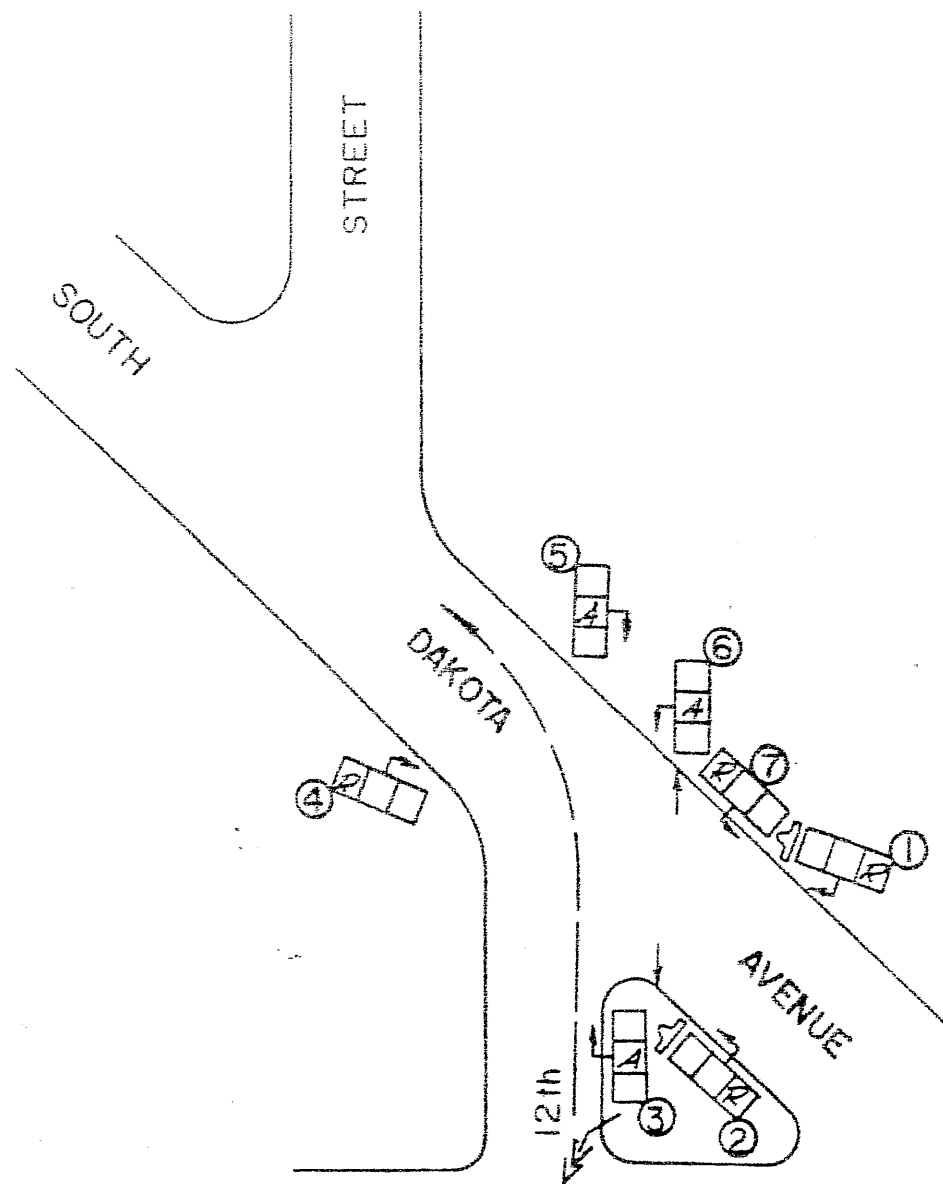
2 of 4



PHASE NO. B

SIGNAL OPERATION  
12th ST. & SOUTH DAKOTA AVE., N.E.

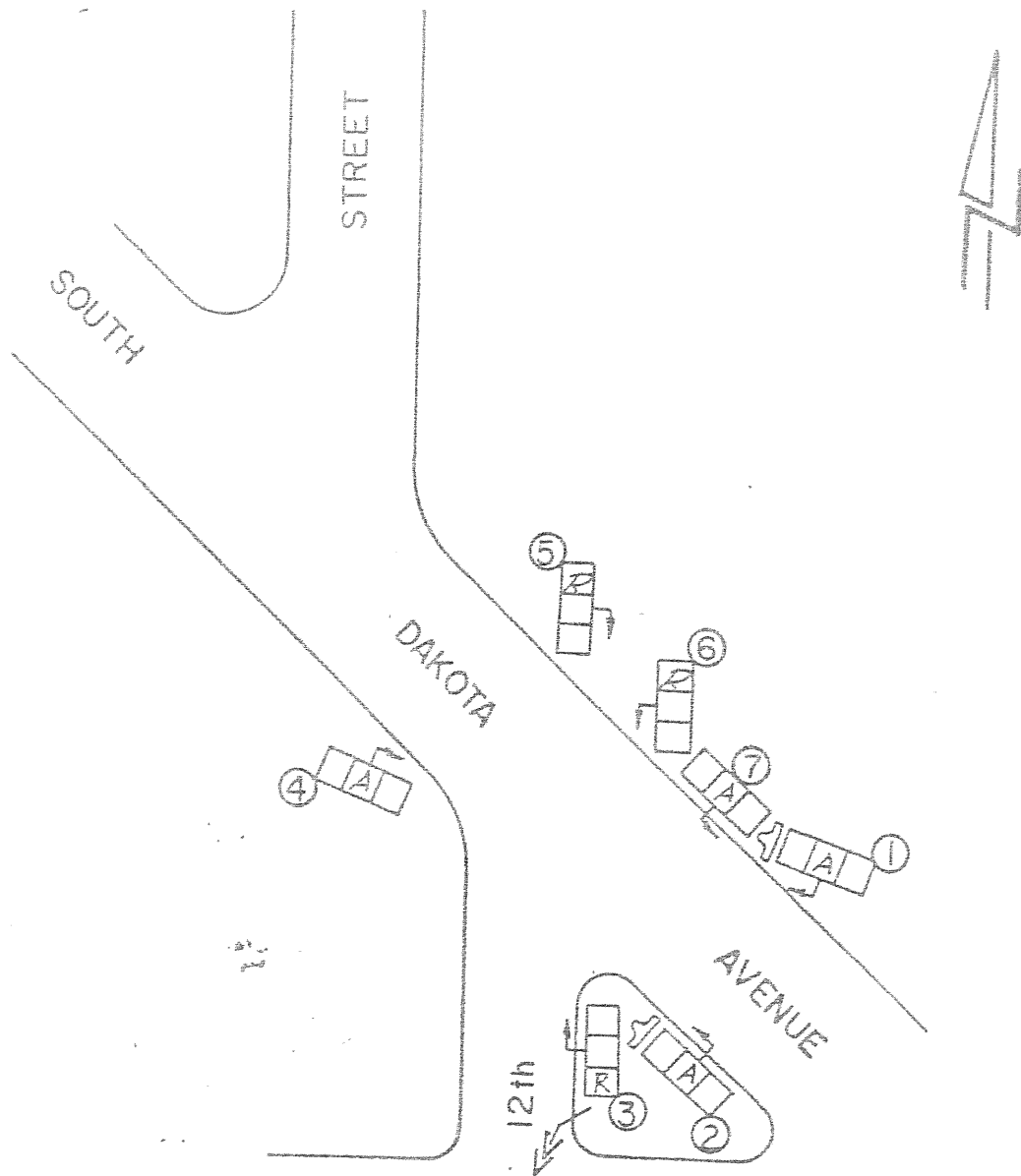
DEPARTMENT OF HIGHWAYS & TRAFFIC, D. C. BUREAU OF TRAFFIC ENGINEERING & OPERATIONS		Submitted Traffic Signal Engineer	T. S.
Designed by		Recommended	719
Checked by	Date	Traffic Control Engineer	
Drawn by	Date	Approved	Sheet 5 of 4
In Service	Scale NONE	Traffic Operations Engineer	



PHASE NO. *B* (CLEARANCE)

SIGNAL OPERATION  
12th ST. & SOUTH DAKOTA AVE., N.E.

DEPARTMENT OF HIGHWAYS & TRAFFIC, D. C. BUREAU OF TRAFFIC ENGINEERING & OPERATIONS		Submitted Traffic Signal Engineer	T. S.
Designed by		Recommended	719
Checked by	Date	Traffic Control Engineer	
Drawn by	Date	Approved	Sheet
In Service	Scale	Traffic Operations Engineer	4 of 4



Revised for  
LAMPS marked "R" and "Y"  
flash during hours signals are  
not operated on colors. 19  
1955

As of: 3-27-80 during  
operation of color.

12th ST. & SOUTH DAKOTA AVE., N.E.

DEPARTMENT OF HIGHWAYS & TRAFFIC, D. C.		Submitted	T. S.  719- FLASH Sheet of
BUREAU OF TRAFFIC ENGINEERING & OPERATIONS		Traffic Signal Engineer	
Designed by <i>Thompson</i>		Recommended	
Checked by <i>Thompson</i>	Date <i>9/3/65</i>	Traffic Control Engineer	
Drawn by <i>Thompson</i>	Date	Approved	
In Service	Scale NONE	Traffic Operations Engineer	

## **APPENDIX D**

### **CAPACITY CALCULATIONS - EXISTING CONDITIONS**

SHORT REPORT												
General Information						Site Information						
Analyst Agency or Co. <i>Street Traffic Studies</i> Date Performed <i>10/5/2009</i> Time Period <i>Morning Peak Hour</i>						Intersection Area Type <i>All other areas</i> Jurisdiction <i>District of Columbia</i> Analysis Year <i>Existing Traffic Volumes</i>						
Volume and Timing Input												
	EB			WB			NB			SB		
	LT	TH	RT	LT	TH	RT	LT	TH	RT	LT	TH	RT
Number of Lanes	1	1	0	0		0		2	0	0	2	
Lane Group	L	TR			LR			TR			LT	
Volume (vph)	54	18	33	56		26		742	12	5	723	
% Heavy Vehicles	0	0	0	0		0		0	0	0	0	
PHF	0.98	0.98	0.98	0.98		0.98		0.98	0.98	0.98	0.98	
Pretimed/Actuated (P/A)	P	P	P	P		P		P	P	P	P	
Startup Lost Time	2.0	2.0			2.0			2.0			2.0	
Extension of Effective Green	2.0	2.0			2.0			2.0			2.0	
Arrival Type	3	3			3			3			3	
Unit Extension	3.0	3.0			3.0			3.0			3.0	
Ped/Bike/RTOR Volume	9	0	0	5	0	0	16	0	0	0	0	
Lane Width	16.0	16.0			16.0			11.0			11.0	
Parking/Grade/Parking	N	0	N	N	0	N	N	0	N	N	0	N
Parking/Hour												
Bus Stops/Hour	0	0			0			0			0	
Minimum Pedestrian Time		12.3			12.2			14.3			3.2	
Phasing	EW Perm	02	03	04	NS Perm	06	07	08				
Timing	G = 24.0	G =	G =	G =	G = 84.0	G =	G =	G =				
	Y = 6	Y =	Y =	Y =	Y = 6	Y =	Y =	Y =				
Duration of Analysis (hrs) = 0.25						Cycle Length C = 120.0						
Lane Group Capacity, Control Delay, and LOS Determination												
	EB			WB			NB			SB		
Adjusted Flow Rate	55	52			84			769			743	
Lane Group Capacity	409	388			315			2442			2327	
v/c Ratio	0.13	0.13			0.27			0.31			0.32	
Green Ratio	0.20	0.20			0.20			0.70			0.70	
Uniform Delay $d_1$	39.5	39.5			40.6			6.9			7.0	
Delay Factor k	0.50	0.50			0.50			0.50			0.50	
Incremental Delay $d_2$	0.7	0.7			2.1			0.3			0.4	
PF Factor	1.000	1.000			1.000			1.000			1.000	
Control Delay	40.1	40.2			42.6			7.3			7.3	
Lane Group LOS	D	D			D			A			A	
Approach Delay	40.2			42.6			7.3			7.3		
Approach LOS	D			D			A			A		
Intersection Delay	11.1			Intersection LOS						B		

SHORT REPORT												
General Information							Site Information					
Analyst Agency or Co. <i>Street Traffic Studies</i> Date Performed <i>10/5/2009</i> Time Period <i>Evening Peak Hour</i>							Intersection Area Type <i>All other areas</i> Jurisdiction <i>District of Columbia</i> Analysis Year <i>Existing Traffic Volumes</i>					
Volume and Timing Input												
	EB			WB			NB			SB		
	LT	TH	RT	LT	TH	RT	LT	TH	RT	LT	TH	RT
Number of Lanes	1	1	0	0		0		2	0	0	2	
Lane Group	L	TR			LR			TR			LT	
Volume (vph)	34	42	31	26		26		697	22	8	737	
% Heavy Vehicles	0	0	0	0		0		0	0	0	0	
PHF	0.95	0.95	0.95	0.95		0.95		0.95	0.95	0.95	0.95	
Pretimed/Actuated (P/A)	P	P	P	P		P		P	P	P	P	
Startup Lost Time	2.0	2.0			2.0			2.0			2.0	
Extension of Effective Green	2.0	2.0			2.0			2.0			2.0	
Arrival Type	3	3			3			3			3	
Unit Extension	3.0	3.0			3.0			3.0			3.0	
Ped/Bike/RTOR Volume	4	0	0	38	0	0	14	0	0	0	0	
Lane Width	16.0	16.0			16.0			11.0			11.0	
Parking/Grade/Parking	N	0	N	N	0	N	N	0	N	N	0	N
Parking/Hour												
Bus Stops/Hour	0	0			0			0			0	
Minimum Pedestrian Time		12.2			12.5			14.3			3.2	
Phasing	EW Perm	02	03	04	NS Perm	06	07	08				
Timing	G = 24.0	G =	G =	G =	G = 84.0	G =	G =	G =				
	Y = 6	Y =	Y =	Y =	Y = 6	Y =	Y =	Y =				
Duration of Analysis (hrs) = 0.25						Cycle Length C = 120.0						
Lane Group Capacity, Control Delay, and LOS Determination												
	EB			WB			NB			SB		
Adjusted Flow Rate	36	77			54			757			784	
Lane Group Capacity	409	403			337			2437			2318	
v/c Ratio	0.09	0.19			0.16			0.31			0.34	
Green Ratio	0.20	0.20			0.20			0.70			0.70	
Uniform Delay $d_1$	39.1	39.9			39.7			6.9			7.1	
Delay Factor k	0.50	0.50			0.50			0.50			0.50	
Incremental Delay $d_2$	0.4	1.1			1.0			0.3			0.4	
PF Factor	1.000	1.000			1.000			1.000			1.000	
Control Delay	39.5	41.0			40.7			7.2			7.5	
Lane Group LOS	D	D			D			A			A	
Approach Delay	40.5			40.7			7.2			7.5		
Approach LOS	D			D			A			A		
Intersection Delay	10.6			Intersection LOS						B		



## **APPENDIX E**

### **CAPACITY CALCULATIONS - TOTAL CONDITIONS**

SHORT REPORT												
General Information						Site Information						
Analyst <i>MN</i>						Intersection <i>S Dakota Ave @ Emerson St</i>						
Agency or Co. <i>Street Traffic Studies</i>						Area Type <i>All other areas</i>						
Date Performed <i>10/5/2009</i>						Jurisdiction <i>District of Columbia</i>						
Time Period <i>Morning Peak Hour</i>						Analysis Year <i>Total Traffic Volumes</i>						
Volume and Timing Input												
	EB			WB			NB			SB		
	LT	TH	RT	LT	TH	RT	LT	TH	RT	LT	TH	RT
Number of Lanes	1	1	0	0		0		2	0	0	2	
Lane Group	L	TR			LR			TR			LT	
Volume (vph)	54	18	33	69		39		742	12	18	723	
% Heavy Vehicles	0	0	0	0		0		0	0	0	0	
PHF	0.98	0.98	0.98	0.98		0.98		0.98	0.98	0.98	0.98	
Pretimed/Actuated (P/A)	P	P	P	P		P		P	P	P	P	
Startup Lost Time	2.0	2.0			2.0			2.0			2.0	
Extension of Effective Green	2.0	2.0			2.0			2.0			2.0	
Arrival Type	3	3			3			3			3	
Unit Extension	3.0	3.0			3.0			3.0			3.0	
Ped/Bike/RTOR Volume	9	0	0	5	0	0	16	0	0	0	0	
Lane Width	16.0	16.0			16.0			11.0			11.0	
Parking/Grade/Parking	N	0	N	N	0	N	N	0	N	N	0	N
Parking/Hour												
Bus Stops/Hour	0	0			0			0			0	
Minimum Pedestrian Time		12.3			12.2			14.3			3.2	
Phasing	EW Perm	02	03	04	NS Perm	06	07	08				
Timing	G = 24.0	G =	G =	G =	G = 84.0	G =	G =	G =				
	Y = 6	Y =	Y =	Y =	Y = 6	Y =	Y =	Y =				
Duration of Analysis (hrs) = 0.25						Cycle Length C = 120.0						
Lane Group Capacity, Control Delay, and LOS Determination												
	EB			WB			NB			SB		
Adjusted Flow Rate	55	52			110			769			756	
Lane Group Capacity	409	388			318			2442			2267	
v/c Ratio	0.13	0.13			0.35			0.31			0.33	
Green Ratio	0.20	0.20			0.20			0.70			0.70	
Uniform Delay $d_1$	39.5	39.5			41.3			6.9			7.0	
Delay Factor k	0.50	0.50			0.50			0.50			0.50	
Incremental Delay $d_2$	0.7	0.7			3.0			0.3			0.4	
PF Factor	1.000	1.000			1.000			1.000			1.000	
Control Delay	40.1	40.2			44.2			7.3			7.4	
Lane Group LOS	D	D			D			A			A	
Approach Delay	40.2			44.2			7.3			7.4		
Approach LOS	D			D			A			A		
Intersection Delay	11.7			Intersection LOS						B		

## SHORT REPORT

## General Information

Analyst *MN*  
 Agency or Co. *Street Traffic Studies*  
 Date Performed *10/5/2009*  
 Time Period *Evening Peak Hour*

## Site Information

Intersection *S Dakota Ave @ Emerson St*  
 Area Type *All other areas*  
 Jurisdiction *District of Columbia*  
 Analysis Year *Total Traffic Volumes*

## Volume and Timing Input

	EB			WB			NB			SB		
	LT	TH	RT	LT	TH	RT	LT	TH	RT	LT	TH	RT
Number of Lanes	1	1	0	0		0		2	0	0	2	
Lane Group	L	TR			LR			TR			LT	
Volume (vph)	34	44	31	43		44		697	22	23	737	
% Heavy Vehicles	0	0	0	0		0		0	0	0	0	
PHF	0.95	0.95	0.95	0.95		0.95		0.95	0.95	0.95	0.95	
Pretimed/Actuated (P/A)	P	P	P	P		P		P	P	P	P	
Startup Lost Time	2.0	2.0			2.0			2.0			2.0	
Extension of Effective Green	2.0	2.0			2.0			2.0			2.0	
Arrival Type	3	3			3			3			3	
Unit Extension	3.0	3.0			3.0			3.0			3.0	
Ped/Bike/RTOR Volume	4	0	0	38	0	0	14	0	0	0	0	
Lane Width	16.0	16.0			16.0			11.0			11.0	
Parking/Grade/Parking	N	0	N	N	0	N	N	0	N	N	0	N
Parking/Hour												
Bus Stops/Hour	0	0			0			0			0	
Minimum Pedestrian Time		12.2			12.5			14.3			3.2	
Phasing	EW Perm	02	03	04	NS Perm	06	07	08				
Timing	G = 24.0	G =	G =	G =	G = 84.0	G =	G =	G =				
	Y = 6	Y =	Y =	Y =	Y = 6	Y =	Y =	Y =				
Duration of Analysis (hrs) = 0.25				Cycle Length C = 120.0								

## Lane Group Capacity, Control Delay, and LOS Determination

	EB			WB			NB			SB		
Adjusted Flow Rate	36	79			91			757			800	
Lane Group Capacity	409	404			316			2437			2238	
v/c Ratio	0.09	0.20			0.29			0.31			0.36	
Green Ratio	0.20	0.20			0.20			0.70			0.70	
Uniform Delay $d_1$	39.1	40.0			40.7			6.9			7.2	
Delay Factor k	0.50	0.50			0.50			0.50			0.50	
Incremental Delay $d_2$	0.4	1.1			2.3			0.3			0.4	
PF Factor	1.000	1.000			1.000			1.000			1.000	
Control Delay	39.5	41.0			43.0			7.2			7.6	
Lane Group LOS	D	D			D			A			A	
Approach Delay	40.6			43.0			7.2			7.6		
Approach LOS	D			D			A			A		
Intersection Delay	11.4			Intersection LOS						B		

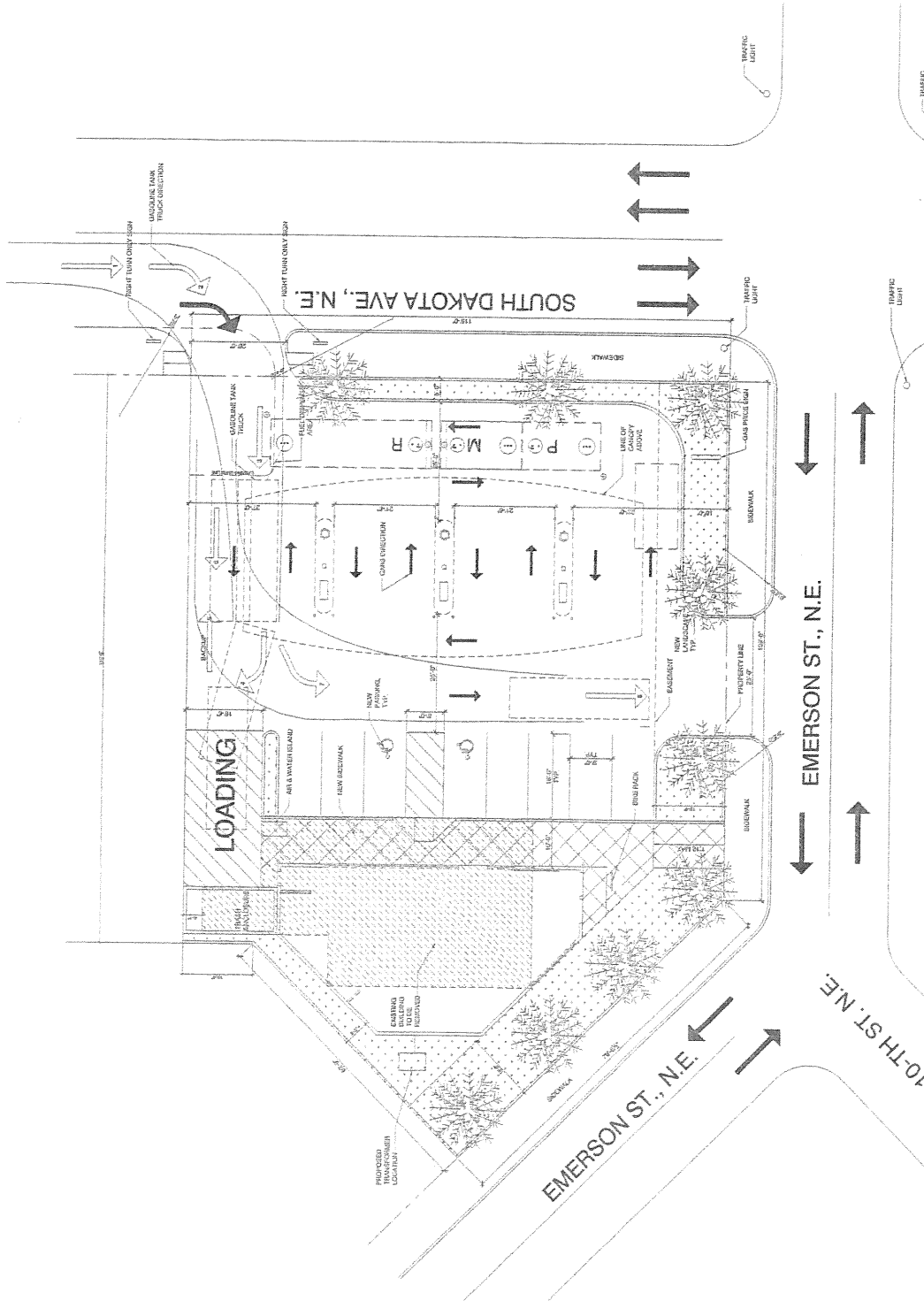
TWO-WAY STOP CONTROL SUMMARY								
<b>General Information</b>				<b>Site Information</b>				
Analyst	MN			Intersection	Emerson St @ Site Access			
Agency/Co.	Street Traffic Studies			Jurisdiction				
Date Performed	10/5/2009			Analysis Year	Total Traffic Volumes			
Analysis Time Period	Morning Peak Hour							
Project Description 4975 S Dakota Avenue								
East/West Street: Emerson Street				North/South Street: Site Access				
Intersection Orientation: East-West				Study Period (hrs): 0.25				
<b>Vehicle Volumes and Adjustments</b>								
<b>Major Street</b>	<b>Eastbound</b>			<b>Westbound</b>				
Movement	1	2	3	4	5	6		
	L	T	R	L	T	R		
Volume (veh/h)		35	13	4	82			
Peak-Hour Factor, PHF	1.00	0.98	0.98	0.98	0.98	1.00		
Hourly Flow Rate, HFR (veh/h)	0	35	13	4	83	0		
Percent Heavy Vehicles	0	—	—	0	—	—		
Median Type	Undivided							
RT Channelized			0			0		
Lanes	0	1	0	0	1	0		
Configuration			TR	LT				
Upstream Signal		0			0			
<b>Minor Street</b>	<b>Northbound</b>			<b>Southbound</b>				
Movement	7	8	9	10	11	12		
	L	T	R	L	T	R		
Volume (veh/h)	26		4					
Peak-Hour Factor, PHF	0.98	1.00	0.98	1.00	1.00	1.00		
Hourly Flow Rate, HFR (veh/h)	26	0	4	0	0	0		
Percent Heavy Vehicles	0	0	0	0	0	0		
Percent Grade (%)		0			0			
Flared Approach		N			N			
Storage		0			0			
RT Channelized			0			0		
Lanes	0	0	0	0	0	0		
Configuration		LR						
<b>Delay, Queue Length, and Level of Service</b>								
<b>Approach</b>	<b>Eastbound</b>	<b>Westbound</b>	<b>Northbound</b>			<b>Southbound</b>		
Movement	1	4	7	8	9	10	11	12
Lane Configuration		LT		LR				
v (veh/h)		4		30				
C (m) (veh/h)		1572		883				
v/c		0.00		0.03				
95% queue length		0.01		0.11				
Control Delay (s/veh)		7.3		9.2				
LOS		A		A				
Approach Delay (s/veh)	—	—	9.2					
Approach LOS	—	—	A					

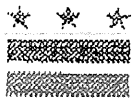
## TWO-WAY STOP CONTROL SUMMARY

General Information			Site Information				
Analyst	MN		Intersection	Emerson St @ Site Access			
Agency/Co.	Street Traffic Studies		Jurisdiction	District of Columbia			
Date Performed	10/5/2009		Analysis Year	Total Traffic Volumes			
Analysis Time Period	Evening Peak Hour						
Project Description 4975 S Dakota Avenue							
East/West Street: Emerson Street			North/South Street: Site Access				
Intersection Orientation: East-West			Study Period (hrs): 0.25				
<b>Vehicle Volumes and Adjustments</b>							
Major Street	Eastbound			Westbound			
Movement	1	2	3	4	5	6	
	L	T	R	L	T	R	
Volume (veh/h)		72	17	6	52		
Peak-Hour Factor, PHF	1.00	0.95	0.95	0.95	0.95	1.00	
Hourly Flow Rate, HFR (veh/h)	0	75	17	6	54	0	
Percent Heavy Vehicles	0	—	—	0	—	—	
Median Type	Undivided						
RT Channelized			0			0	
Lanes	0	1	0	0	1	0	
Configuration			TR	LT			
Upstream Signal		0			0		
Minor Street	Northbound			Southbound			
Movement	7	8	9	10	11	12	
	L	T	R	L	T	R	
Volume (veh/h)	35		5				
Peak-Hour Factor, PHF	0.95	1.00	0.95	1.00	1.00	1.00	
Hourly Flow Rate, HFR (veh/h)	36	0	5	0	0	0	
Percent Heavy Vehicles	0	0	0	0	0	0	
Percent Grade (%)		0			0		
Flared Approach		N			N		
Storage		0			0		
RT Channelized			0			0	
Lanes	0	0	0	0	0	0	
Configuration		LR					
<b>Delay, Queue Length, and Level of Service</b>							
Approach	Eastbound	Westbound	Northbound			Southbound	
Movement	1	4	7	8	9	10	11
Lane Configuration		LT		LR			
v (veh/h)		6		41			
C (m) (veh/h)		1515		859			
v/c		0.00		0.05			
95% queue length		0.01		0.15			
Control Delay (s/veh)		7.4		9.4			
LOS		A		A			
Approach Delay (s/veh)	—	—		9.4			
Approach LOS	—	—		A			

## APPENDIX F

### TRUCK TURNING PATH





# ADVISORY NEIGHBORHOOD COMMISSION 5A

SLOWE SCHOOL DEMOUNTABLE

1322 IRVING STREET, N.E. ▼ WASHINGTON, D.C. 20017

TELEPHONE: 202-635-6563 ▼ FAX: 202-635-6565 ▼ Email: ANC5ADC@aol.com

2009-10-9 PM 4:49  
OFFICE OF ZONING

## Officers

Angel Alston  
Chair

Janae Grant  
Vice-Chair

Sandi Washington  
Treasurer

Patricia Roberts  
Corresponding Secretary

John Feeley  
Recording Secretary

Robert King  
Parliamentary

Date: November 4, 2009

To: Chairman Marc D. Loud  
Board of Zoning Adjustment  
441 4th Street NW, Suite 220  
Washington, DC 20001

Ref: # 17963 Application of Euclid of Virginia 4975 South Dakota Avenue, N.E. (Square 3899, Lot 76).

Advisory Neighborhood Commission 5A held its monthly meeting on Wednesday, July 22, 2009 at 7:00pm and with a quorum unanimously voted to support the community in its efforts to oppose the applied for (Special Exceptions) variances pursuant to 11 DCMR § 3104.1, which allows for a special exception to place a new self-service gasoline station and convenience store under section 706, in the C-1 District at premises 4975 South Dakota Ave, NE, WDC 20017.

With proper notification, ANC Commissioner Angel Alston held a Single Member District meeting on June 23, 2009. With over 65 community members in attendance, the community voted against granting the special variances at 4975 South Dakota Avenue, N.E. Commissioner Alston also met with abutting SMD 5A04 and who voted to support the residents of SMD 5A03 with their opposition of this proposed gas station.

ANC 5A heard reports from Commissioner Alston regarding Application #17963 at ANC 5A meetings on Wednesday, June 24, 2009 and Wednesday, July 22, 2009. On Monday September 14, 2009, ANC 5A unanimously voted once again to support the residents of SMD 5A03 in their opposition of this proposed self-service gasoline station and convenience store.

At the hearing on September 22<sup>nd</sup>, Mr. Jeff Jennings, the DDOT representative gave testimony to the Zoning Board regarding its recommendations on conditional support of the Applicants request for a special exception. During his presentations, he expressed that he was only "vaguely familiar" with the intersection adjacent to the site of the proposed Lowest Price Gas Station but supported its request based on a traffic study conducted by DDOT. In addition, Mr. Jeff Jennings did not submit any written testimony showing the results of DDOT's traffic study and or its analysis. Subsequently, I requested a copy of the traffic study used, a complete email list of the dates in which DDOT meetings took place and a record of correspondences regarding the application at this property.

As a result, Chair Loud ordered that a copy of the DDOT Traffic Study which was utilized in support of this application and any other supporting documentation be submitted to the Zoning Board and a copy to the ANC by Friday, October 16<sup>th</sup>. This date was agreed upon by the applicant and the ANC to allow time for proper community review, ANC discussion and community input prior to the November 10<sup>th</sup> hearing. ANC 5a did not receive anything on the agreed date of October 16<sup>th</sup> and as of

Ft. Totten\* Lamond-Riggs\* North Michigan Park\* Michigan Park\* Brookland\* Queens Chapel\* Woodridge\* Gateway\* Fort Lincoln

BOARD OF ZONING ADJUSTMENT  
District of Columbia

CASE NO.

17963

EXHIBIT NO.

32



November 2nd, neither the Zoning Board nor the affected ANC has received a copy of the Traffic Study used to validate its findings.

On October 5th, the applicant met with the Commissioners of ANC5A and informed us that DDOT had agreed to do a new traffic study along South Dakota Ave., N.E. He expressed that this Study would better support DDOT's previous findings which was supported by a traffic study that DDOT conducted along Minnesota Ave., in N.E. DC. To date, ANC5A is still awaiting a copy of the initial supporting documentation and Traffic Study that was referenced at the Zoning Board Hearing on September 22<sup>nd</sup>.

The community stands behind the facts that's:

- The proposed location would be directly adjacent to an existing BP Gas Station and 3 blocks away from a new self-service gas station with 24 hour convenient store
- The current property (without the closing of the curb cut) can not accommodate a self service gas station without flooding our community with more traffic and creating more safety issues.
- The 8 proposed parking spaces will not be enough to accommodate workers and shoppers. This will result in temporary parking/idling on the street until space becomes available, causing a traffic congestion and road hazards.
- There is nothing in the plan to address the traffic and safety concerns in the event of an emergency.
- The inconsistency of information received regarding the proposed hours of operation, fuel delivery and delivery vehicles.
- The current location is surrounded by homes and a new gas station will create safety issues and drain our police resources as stated by Lt. Rodgers from the 4<sup>th</sup> district.
- The current owners do not maintain their property and the community currently has issues with loitering and trash as well congestion due to the McDonald's across the street and the existing adjacent gas station.
- The owners have not posted any visible signs or public notices of intent for the proposed use of the property (except on the day of the hearing of Sep 22<sup>nd</sup>).
- The current owners of the convenient store have been in our community for over 20 years and do not live in our neighborhood. They are also not active participants in our community events in 5A03 or ANC 5A.

In a 3 PAGE document submitted on Oct 16th by Karina Ricks Exhibit No 31, in which the ANC 5A never received a copy of, it states that the applicant "*will provide some means of safety to the transportation environment.*" This is not acceptable to our community because we have had vehicular tragedies and deaths due to speed and the lack of traffic calming measures. Also on page 2 it states that the "applicant provided DDOT a transportation study supporting redevelopment of the property located at 4975 S. Dakota Ave NE." At the hearing on Sep 22nd it was understood from Mr. Jennings testimony that this study was conducted by DDOT and not supplied by the applicant. This traffic study can bias and is in no way comparable and does address the ongoing traffic issues at the property location. In the 4<sup>th</sup> paragraph on pg 2 it states that "*the applicant reports, the neighborhood surrounding the S. Dakota intersection are well established and do not lend themselves to cut through commuter traffic*". That again, is incorrect and in fact since traffic has increased the residents are suffering even more because commuters are using the alleys and residential streets as through streets to eliminate the traffic lights. Hence the communities numerous existing requests, within the Office of Transportation, for speed hump and bump in both the streets and allies surrounding this location. Also, in the next sentence this document states that "the signal timing at the intersection and along S. Dakota in general, gives preference to moving traffic along S. Dakota Ave which in and of itself would discourage commuters from cutting through the neighbor. This is again as far from the truth as

possible. In fact, that signal timing at this light is so short that it does not allow for more than three cars to pass before turning red. If a traffic study was done it would have highlighted these concerns and saw that the closing of this curb cut to allow this special exception will have a dangerous impact on the existing community. We as a community have requested services from DDOT to address the sever traffic calming concerns at this location. We have numerous speed petitions request in for the streets, allies and major interceptions. The Officers of MPD PSA 405 have been asked to sit at this property address to catch speeders and write tickets on a daily bases. We are a community of pedestrians, seniors, students and have a lot of foot traffic that seems to compete with vehicular traffic.

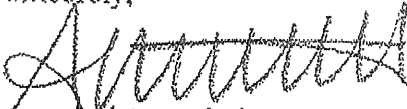
ANC 5A request that the Board of Zoning Adjustment supports the request of the community and the ANC and deny the request of the applicant. Because the Applicant and DDOT has failed to provide the agreed upon documentation for us to make an informed decision, the community cannot even consider supporting the application. As it stands we hold to our previous stance that this special exception will have an adverse affect on our community and be detrimental to the surrounding neighborhood and its constituents. The community is adamantly against this project because of the dangerous traffic concerns at this interception, and the posed safety issues.

Since the hearing held on September 22<sup>nd</sup> this application was again heard before ANC 5A on September 23<sup>rd</sup>, October 5<sup>th</sup>, and October 21<sup>st</sup> of 2009 and for the reasons listed above we as a commission unanimously vote against this application.

As the Chair, I do authorize Commissioner Alston to represent her SMD at the hearing scheduled for November 10, at 9:45a.m.

I can be reached on 202-556-5958 or via email at [angelalston@yahoo.com](mailto:angelalston@yahoo.com).

Sincerely,



Angel Alston, Chair 5A  
202-556-5958 cell  
202-315-6057 cell  
202-635-6565 fax  
[angel.alston@anc.dc.gov](mailto:angel.alston@anc.dc.gov)

# FAX

*Advisory Neighborhood Commission 5A*  
1322 Irving Street NE  
Washington, DC 20017

202-635-6563 office  
202-635-6565 fax

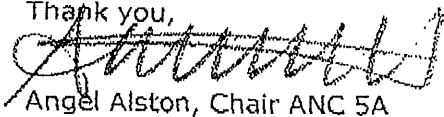
To: Board of Zoning Adjustment  
Fax: 202-727-6072  
Phone: 202-727-6311  
Re: App # 17963

From: Chair, Angel Alston  
Pages: 4 including fax cover  
Date: November 9, 2009  
cc:

☐ Urgent ☐ For Review ☐ Please Comment ☐ Please Reply ☐ Please Recycle

Comments: I am submitting this letter on behalf of ANC 5A regarding Application Number 17963 Application of Euclid of Virginia 4975 South Dakota Ave NE. Please file this in the folder for the hearing. A paper copy preceded this with an original signature. Feel free to contact me on my cell at 202-315-6057 or email me at [angel.alston@yahoo.com](mailto:angel.alston@yahoo.com)

Thank you,

  
Angel Alston, Chair ANC 5A

202-556-5958 cell

202-635-6563 office

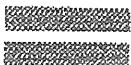
202-635-6563 fax

[Angel.alston@anc.dc.gov](mailto:Angel.alston@anc.dc.gov)

2009 NOV -9 PM 4:49

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GOVERNMENT OF THE DISTRICT OF COLUMBIA  
DISTRICT DEPARTMENT OF TRANSPORTATION

★ ★ ★

██████████

██████████

Transportation Policy and Planning Administration

## Memorandum

TO: Jamison L. Weinbaum, Director  
DC Office of Zoning

FROM: Karina Ricks *KR*  
District Department of Transportation

DATE: October 16, 2009

SUBJECT: BZA Application #17963 (Application of Euclid of Virginia)  
4975 South Dakota Avenue, N.E.

### APPLICATION

Euclid of Virginia (the "Applicant"), pursuant to 11 DCMR § 3104.1, seeks a special exception to allow a new self-service gasoline station and convenience store under section 706, in the C-1 District at premises 4975 South Dakota Avenue, N.E. (Square 3899, Lot 76).

### RECOMMENDATION IN BRIEF

The District Department of Transportation (DDOT) recommends conditional support of the Applicant's request for a special exception. DDOT believes the Applicant has provided the requested measures that will provide some means of safety to the transportation environment. The Applicant has agreed to use a smaller truck for fuel delivery to the fuel service station and deliver fuel during limited hours to avoid traffic congestion in the North Michigan Park neighborhood.

2000 14<sup>TH</sup> Street, N.W., 7<sup>th</sup> Floor, Washington, D.C. 20009

202 671-2730, Fax: 202 671-0617

BOARD OF ZONING ADJUSTMENT  
District of Columbia

CASE NO. 17963

EXHIBIT NO. 31

## BZA 17963 - Application of Euclid of Virginia

### DDOT ANALYSIS

The Applicant has provided DDOT a transportation study supporting redevelopment of the property located at 4975 S. Dakota Avenue, N.E. with a three pump gas station and convenience market.

The Applicant has agreed to deliver fuel to the proposed service station using a 30' long truck. DDOT believes this type of vehicle will have less of an impact on the infrastructure in comparison to a 55' semi tractor trailer. The Applicant's fuel delivery vehicle will enter a 20' wide curb cut along South Dakota Avenue. The 30' delivery vehicle has the turn radius, from South Dakota Avenue, to enter the curb cut. The lone South Dakota curb cut will be restricted to a right in vehicle turn movement. All vehicles exiting the property will use the curb cut along Emerson Street. The Emerson Street curb cut will serve as a two directional vehicle access point. The two proposed curb cuts will allow for better vehicle circulation through the site while allowing for safer multi modal transportation use on South Dakota Avenue.

The Applicant agrees to deliver fuel between the hours of 7PM and 7AM when there may not be as much vehicle traffic in the area of the site. DDOT believes the limited hours for fuel delivery may help avoid traffic congestion along South Dakota Avenue.

At DDOT's request, the Applicant reviewed the potential impact for cut through vehicle traffic on the North Michigan Park community and the response is credible. The Applicant reports, "The neighborhood surrounding the S. Dakota/Emerson/Delafield intersection are [sic] well established and do not lend themselves [sic] to cut through commuter traffic. The signal timing at the intersection and along S. Dakota in general, gives preference to moving traffic along S. Dakota which in and of itself would discourage commuters from cutting through the neighborhood. In addition, Delafield Street is restricted to one-way traffic flow approaching S. Dakota so even residents are discouraged from traveling from one side of the neighborhood to the other."

As a means to continue to follow-up, DDOT believes the Applicant should annually attend the Advisory Neighborhood Commission 5A meeting to understand if there may be community concerns associated with transportation. The Applicant should be responsible to report any news related to its proposed electric vehicle charging station to the ANC and DDOT as it continues to negotiate the matter with PEPCO. If the case is approved, the ANC meeting attendance should be part of the BZA order.

## BZA 17963 - Application of Euclid of Virginia

### SUMMARY AND RECOMMENDATION

DDOT supports the Applicant's proposal with the conditions as previously stated. DDOT believes the BZA should consider those conditions and include them in the order if the case is approved.

KR:jj

Wednesday, September 23, 2009

Mr. Marc Loud  
Chair of the Board of Zoning Adjustment

Good Morning my name is Patricia Love with family residing at 5015 10<sup>th</sup> Street, NE Washington, D.C. and have been a Ward 5 resident since 1957. As a resident in good standards in this community I am protesting the proposed conceptual plan for 4975 South Dakota Ave. (APPLICATION #17963). The current property site next door to the mentioned proposed project is a gasoline service station with a mini store; the area does not need two gasoline service stations next door to each other. Approximately 2 to 3 years ago a mini market gasoline station was build several blocks from this location on South Dakota Ave. in the Riggs Park community.

Directly across from this site McDonalds and also a mini shopping center is currently open and operating. This small community does not need any more commercial businesses operating....enough is enough!

Thanking you in advance for your time in this matter.

*Patricia A. Love*  
*for*

Patricia Love  
Percy Love  
Fannie Love

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BOARD OF ZONING ADJUSTMENT  
District of Columbia

CASE NO. 17963

EXHIBIT NO. 30

**Testimony of Michael Goss**  
**Board of Zoning Adjustment Hearing**  
**Tuesday, September 22, 2009**  
**Application No. 17963**

Good morning Mr. Chairman and members of the Board. My name is Michael Goss and I am the agent for Mr. Koo Yuen and Euclid of Virginia, LLC. My home address is 1400 W Street, NW, Washington, DC 20009.

I would like to start my presentation with going over the site and area descriptions – what currently exists and what we are proposing.

**Site and Area Description**

Euclid of Virginia, LLC is proposing to raze the existing retail store and replace it with a newly constructed retail store and construct a new self-service gasoline station.

The site abuts a branded **24-hour** self-service gasoline station owned by BP Oil and is across the street from a Chinese restaurant, a McDonalds fast food restaurant, a dry cleaning establishment and a beauty parlor.

The property is an irregular shaped site fronting South Dakota Avenue, Northeast. The lot size is 15,748 square feet with an existing building consisting of 1,895 square feet. The portion of the site adjoining South Dakota Avenue is a paved lot currently used for parking for customers of the retail store. This parking area makes up the majority of the area of the lot.

**With Regard to the zoning**

The Site is located on the north side of South Dakota Avenue at 4975 South Dakota Avenue, N.E., within the C-1 Zone District and is bounded by 10<sup>th</sup> Street to the north, Emerson Street to the northeast, and South Dakota Avenue to the west.

A gasoline station is a special exception use in a C-1 District. (§706.1).

**Analysis**

**§706.1A gasoline service station established or enlarged after May 12, 1958, shall be permitted in a C-1 District as a special exception if approved by the Board of Zoning Adjustment under § 3104, subject to the provisions of chapter 23 of this title and the provisions of this section.**

BOARD OF ZONING ADJUSTMENT  
District of Columbia

CASE NO. 17963

EXHIBIT NO. 29



**§706.3 The station shall not be located within twenty five feet (25 ft.) of a Residence District unless separated from the Residence District by a street or alley.**

The subject site is surrounded by an R-2 District on all sides and is separated from that residential district by a street or alley. The nearest existing structure in the R-2 District to the west of the site is furthered buffered from the proposed self-service gasoline station use by the landscaping at the western portion of the subject lot.

Residential properties to the west are separated from the subject site by 10<sup>th</sup> Street and residential properties to the north are separated from the subject site by an alley and Emerson Street.

**§706.4 The operation of the use shall not create dangerous or other objectionable traffic conditions.**

The location of the proposed development is situated on a portion of South Dakota Avenue that is a high traffic densely developed neighborhood commercial district. There is no aspect of the physical characteristics of the property which would inhibit traffic flow or cause any adverse impact with the traffic of South Dakota Avenue in front of the site. To alleviate potential traffic hazards, the applicant is proposing to create one-way entrance from South Dakota Avenue and is proposing to close the second curb-cut onto the site from South Dakota Avenue closest to Emerson Street.

The self-service gasoline station would be situated so as to permit a free flow of traffic onto the site for access to the proposed convenience store. Patrons of the station will be drawn from vehicles already in the stream of traffic. The large area of the site provides for ample on-site vehicular circulation and safe pedestrian movement. The orientation of the three pump islands, all of which are parallel to South Dakota Avenue, will direct traffic away from the convenience store.

**§706.5 The Board may impose requirements pertaining to design, appearance, screening, or lighting, or other requirements it deems necessary to protect adjacent or nearby property.**

The convenience store building will be 65 by 36 feet. The building will also contain the cashier's booth plus coolers, restrooms, and a storage area. The main food and sales area will measure approximately 1,800 square feet. The building will be centered on the site facing South Dakota Avenue.

There will be three new pump islands each with one self-service multi-hose dispenser capable of serving one car on each side of the island. An 84 foot by 28 foot steel and Alucobond aluminum canopy will be constructed over the pump islands. The pumps and the canopy will be located in the center of the site and will be perpendicular to South

Dakota Avenue. The canopy will be lighted and will shelter customers pumping gas in all weather conditions.

Beyond the innovative and modern design aesthetic, also included are many environmental elements that make the project sustainable and beneficial to the ecosystem and the community as a whole.

#### LEED Credit Points

The Sustainable Aspects of the Site Include:

- Commuter rail within 1/2 mile of the site (Fort Totten)
- Bicycle rack within 200 yards of building
- Pedestrian and ADA compliant walks and ramps
- Planning for e-car charging stations
- Providing alternative fuel pumps
- Recycled materials used for construction backfill
- Planting 40% of the site area excluding the building foot print with native and adaptive species of trees and plants
- Installation of a vegetated roof for the purposes of energy conservation and efficiency and the treatment of storm water
- Interior lighting will be LED lighting and energy efficient light bulbs.

The exterior lighting that will be provided will be adequate for security and safety yet not too intrusive to the homes around the site.

The signage for the gasoline station will be low and unobtrusive.

**§706.6 Required parking spaces may be arranged so that all spaces are not accessible at all times. All parking spaces provided under this subsection shall be designed and operated so that sufficient access and maneuvering space is available to permit the parking and removal of any vehicles without moving any other vehicle onto public space.**

The building on the site will contain approximately 1,800 square feet of gross floor area and will therefore require six (6) parking spaces. Eight (8) parking spaces are shown on the site plan and all spaces are designed to be accessible at all times.

With regard to the bulk and yard requirements in a C-1 Zoned area:

**§770** allows for a maximum height of 40 feet. The current height of the building housing the convenience store is 16 feet and that height would be the same in the newly constructed store. The height of the canopy for the proposed gasoline station will be 14 feet.

**§771.2** - (Floor Area Ratio) allows for a maximum permitted floor area ratio of a building or structure for which an application for a building permit was filed on or after November

17, 1978, the maximum FAR is 1.0. The current FAR is .114 and would remain the same in the newly constructed convenience store.

**§772** – The maximum percentage of lot occupancy is 60%. The current lot occupancy is .59 and would remain the same in the newly constructed convenience store.

**§774** – A rear yard is required for each structure located in a commercial district. The minimum depth of the rear yard in a C-1 district is 20 feet and the current rear yard meets this requirement and the newly constructed convenience store will also meet this requirement.

**§775 .5** – No side yard shall be required; but if one is provided, it shall be at least 2 inches wide for each foot of height of building, but not less than six feet. The newly constructed convenience store will not have a side yard.

**2302.1** - A public storage garage, repair garage, mechanical parking garage, or gasoline service station established or enlarged in any district except in the C-M or M Districts shall be subject to the provisions in this section.

**2302.2** - No portion of the structure or premises to be used for any of the uses listed in §2302.1 shall be located within twenty five feet (25 ft.) of a Residence District unless separated from that Residence District by a street or alley.

**2302.3** - No use listed in §2302.1 shall have a vehicular entrance or exit connected with a street at a point closer than twenty five feet (25 ft.) to any Residence District existing at the time the use is established, unless separated from the Residence District by a street or alley.

**2302.4** - No driveway of any entrance or exit to any use listed in §2302.1 shall be closer than forty feet (40 ft.) to a street intersection as measured from the intersection of the curb lines extended.

The subject site is surrounded by an R-2 District on all sides and is separated from that residential district by a street or alley. The nearest existing structure in the R-2 District to the west of the site is furthered buffered from the self-service gasoline station use by the landscaping at the western portion of the subject lot.

Residential properties to the west are separated from the subject site by 10<sup>th</sup> Street and residential properties to the north are separated from the subject site by an alley and Emerson Street.

**2302.5** - Except in a C-M or M District, all grease pits or hoists constructed or established as part of a use listed in §2302.1 shall be within a building.

There will be no grease pits or hoists as part of the proposed use.

**2500.2 - An accessory building shall be located only in a rear yard, except as follows:**

**(a) An accessory private garage may be located in a side yard pursuant to § 2300; and**

**(b) A pump island canopy and any kiosk adjacent to the pumps used exclusively as an attendant's shelter of a gasoline service station may be located in any open area of a lot not within twenty-five feet (25 ft.) of a Residence District unless separated therefrom by a street or alley.**

Section 2500.2 indicates that pump island canopies are to be considered an accessory building. The proposed canopy would meet the locational requirements of this section.

**2500.4 - An accessory building in any zone district shall not exceed one (1) story or fifteen feet (15 ft.) in height, except as provided in § 2500.5.**

Section 2500.4 states that accessory buildings may not exceed 15 feet in height. The height of the canopy will be 14 feet and would meet this standard.

**With respect to meetings that have been held regarding this project.**

**On June 15, 2009**, my team and I met with Mr. Arthur Jackson from the Office of Planning at the site to review the proposed development.

**On August 20, 2009**, my team and I attended a joint meeting with the Office of Planning and the DC Department of Transportation. Present at this meeting were Mr. Arthur Jackson, from the Office of Planning, Ms. Deborah Crain the Ward 5 Community Planner with the Office of Planning, and Mr. Joel Lawson from DDOT. At this meeting we presented revised drawings and reviewed the project in great detail.

As a result of that meeting several important revisions to the proposed gasoline station and convenience store design were made:

#### **Curb Cuts**

First, the existing curb cut at South Dakota and Emerson Street will be closed. Second, the entrance at South Dakota Avenue just before Emerson Street will be designed so that it will be one –way and signage will be placed to show that it is “Enter only”

#### **Trucks and Delivery**

The trucks will be 30 ft trucks with 5,000 gallon capacity. The design of the gasoline station allows the ability of the truck to service the tanks without having to pull up to each pump.

## **Green Approach**

As I mentioned earlier, this project now includes many environmental elements that make the project sustainable and beneficial to the ecosystem and the community as a whole.

## **Goods to be sold in the Convenience Store**

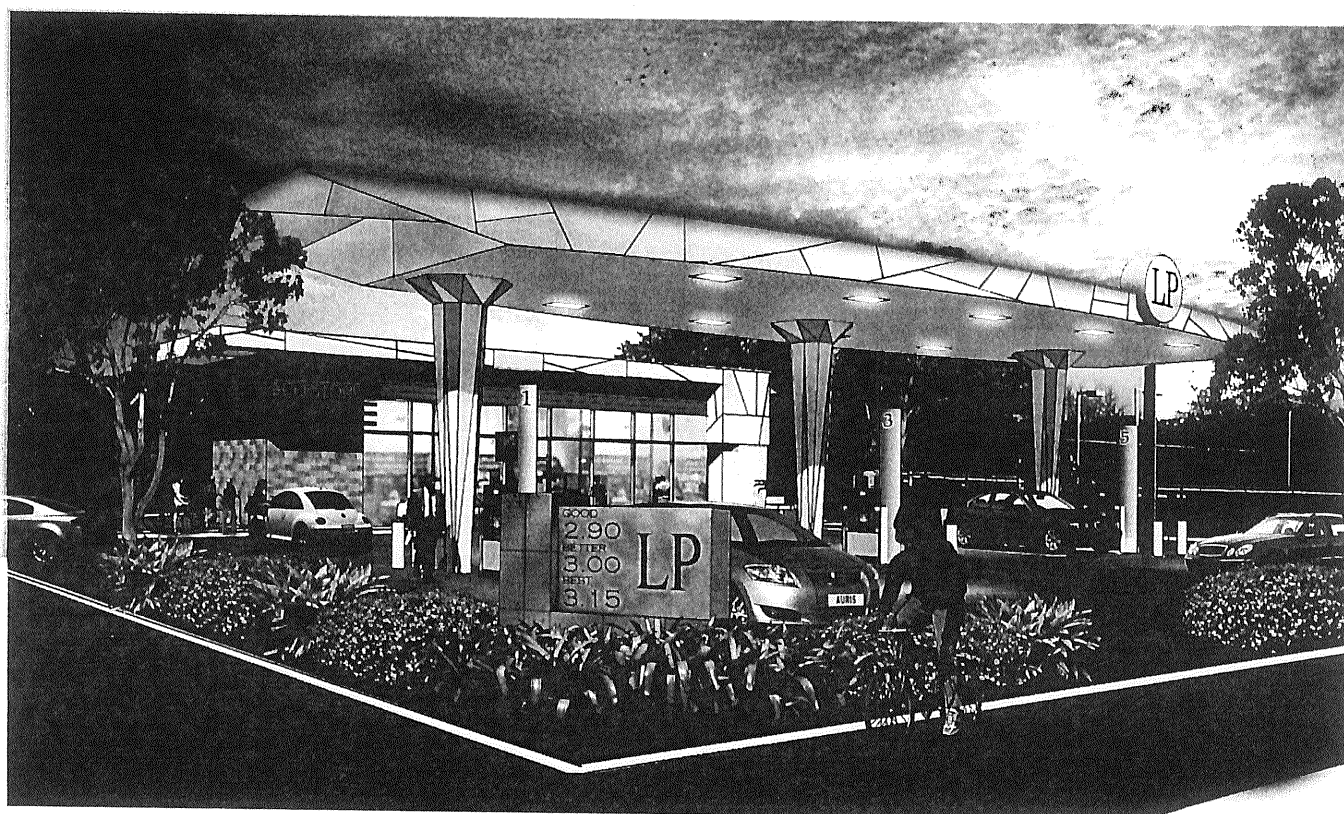
The newly constructed convenience store will offer an array of healthy foods, including fresh fruit and healthy snacks.

Finally, on September 2, 2009, my team and I attended the North Michigan Park Civic Association meeting and made a presentation to those present.

That concludes my testimony. I will be happy to answer any questions that you may have.

September 20, 2009

# ONE STOP CONVENIENCE



g2 design + Euclid, LLC  
Proposed One Stop  
Convenience Store  
and Self-Service Gas  
Station



g2 design collective

PROPOSED CONCEPTUAL PERSPECTIVE  
FOR 4975 SOUTH DAKOTA AVE.

S-A

SEPTEMBER 10, 2009

## A more collaborative approach to doing business

g2 Design is a minority-owned design and development company operated by Gary and Michael Goss. The Goss brothers have been engaged in development, construction, and neighborhood improvement projects in a number of cities including Washington, D.C., Baltimore, Atlanta and Miami.

They have been recognized by local governments and community organizations for their integration of high-design in neighborhoods, employing local talent, talent development, and respect for the communities where they live and work.

Born in Washington, D.C., they have been proud to be a part of the development that has contributed to the city's economic growth and international reputation.

The Goss brothers pride themselves on being active in the communities in which they live, work and build. Their first priority is the customer and making sure they establish strong relationships that allow for a more collaborative style of operating their facilities.



# The Future Service Station Environmentally Friendly

## 4975 South Dakota Ave, N.E. Project:

Currently, g2 is engaged in partnership with Euclid, LLC for a new, improved one-stop convenience store and self-service gas station at 4975 South Dakota Avenue, N.E., Washington, D.C. This effort would bring a modern and attractive facility, that provides one stop shopping and fueling. The new store will offer customers alternatives from the typical “soda + chips” type of convenience store. A large selection of fresh foods, fruits + salads will be highlighted. The enhanced site amenities are designed to better serve the community and consumers.

The project plans to tear down the existing convenience store and recycle the debris to construct a new convenience store and a new self-service gasoline station. The new facility will feature better lighting for safety and a number of environmental features that are sustainable and beneficial to the ecosystem and the community.

The location of the proposed development is situated on a portion of South Dakota Avenue that is a high traffic densely developed neighborhood commercial district. The new facility will not create dangerous or other objectionable traffic conditions. To minimize disruption, the project proposes to create one-way entrance from South Dakota Avenue and close the second curb-cut onto the site from South Dakota Avenue closest to Emerson Street. This would alleviate additional traffic to enter the location. Features of the proposed new facility:

“We want to give the community a new shopping experience that offers them convenience, fresh quality merchandise and unrivaled customer service. Our approach is to combine old fashioned courtesy with clean modern facilities to fully satisfy the needs of our customers.”

Michael Goss, g2 design

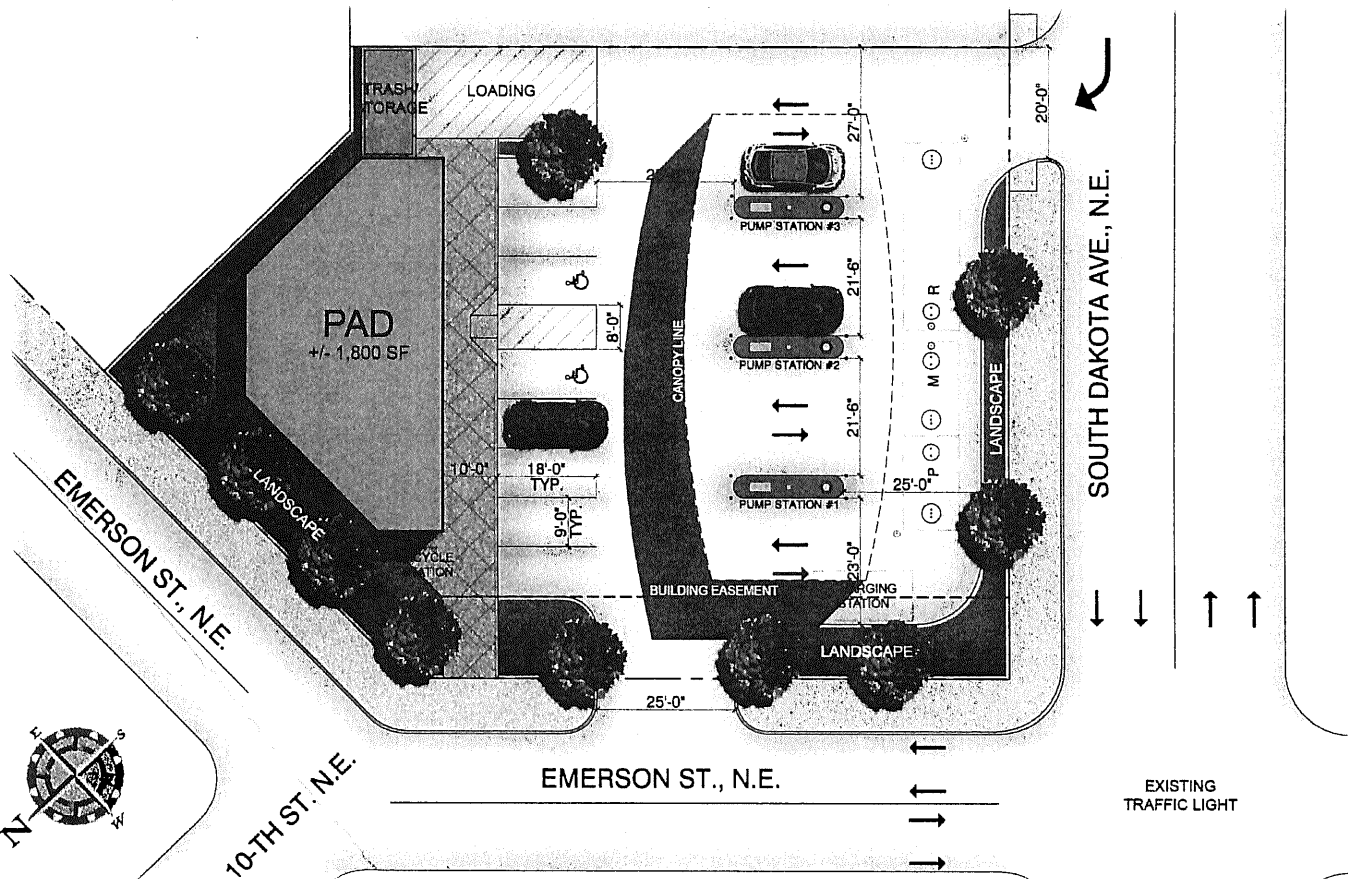
- Three new pump islands each with self-service multi-hose dispenser capable of serving one car on each side of the island.
- New covering of an aluminum canopy will be constructed over the pump islands.
- The canopy will feature recessed lights and shelter customers pumping gas in all weather conditions. The exterior lighting will be adequate for security and safety yet not intrusive to the homes around the site.
- Environmental features:**
- Bicycle Station with rack + air pumps
- Pedestrian and ADA compliant walks and ramps
- Planning for e-car charging stations
- Providing alternative fuel pumps
- Recycled materials used for construction backfill
- Planting 40% of the site area excluding the building foot print with native and adaptive species of trees and plants
- Installation of a vegetated roof for the purposes of energy conservation and efficiency and the treatment of storm water
- Interior lighting will be LED lighting and energy efficient light bulbs.

### WHY BUILD GREEN?

The team will consist of LEED certified contractor's trained in the techniques of building sustainable projects. The future is in our hands and planning for the next generation requires a new and more responsible approach to building service stations.

Building green has direct economical and environmental benefits to consumers and communities.

This project will utilize recycled materials and promote alternative fuel + energy sources. Some of which will be generated onsite.



PROPOSED CONCEPTUAL PLAN FOR 4975 SOUTH DAKOTA AVE.

P-A  
SCALE 1:20  
SEPTEMBER 10, 2009



g2 design, construction, and management

**WITNESS CARD**CASE NO. 17963

Proponent \_\_\_\_\_ OPPONENT \_\_\_\_\_

Name Rosalie A. SmithAddress 5006-12th St. NE

City \_\_\_\_\_ Zip \_\_\_\_\_

DAYTIME PHONE No. \_\_\_\_\_

**WITNESS CARD**CASE NO. 17963NAME Rosalie A. SmithDAYTIME PHONE NO. 202/269-3064ADDRESS 5006-12th St. NECITY WashingtonSTATE DCZIP 20017-2850

E-MAIL ADDRESS \_\_\_\_\_

PROPOSER ☐OPPOSER ☒**WITNESS CARD**CASE NO. 17963NAME Angel AlstonDAYTIME PHONE NO. 202-315-6057ADDRESS 4707 6th Place NECITY WDCSTATE DCZIP 20017E-MAIL ADDRESS angel.alston@dc.govPROPOSER ☐OPPOSER ☒BOARD OF ZONING ADJUSTMENT  
District of ColumbiaCASE NO. 17963EXHIBIT NO. 28



WITNESS CARD CASE NO. 1793

Proponent \_\_\_\_\_ OPPONENT \_\_\_\_\_

Name Shirley E. Felt

Address \_\_\_\_\_

City \_\_\_\_\_ Zip \_\_\_\_\_

DAYTIME PHONE No. \_\_\_\_\_

## WITNESS CARD

CASE NO.

NAME \_\_\_\_\_

**DAYTIME PHONE NO.**

**ADDRESS**

CITY

## STATE

ZIP

E-MAIL ADDRESS

**PROPONENT** ☐

**OPPONENT** ☒



## WITNESS CARD

**CASE NO.**

NAME \_\_\_\_\_

DAYTIME PHONE NO.

**ADDRESS**

CITY

STATE

ZIP

E-MAIL ADDRESS

**PROPONENT** ☒

OPPONENT ☒



BOARD OF ZONING ADJUSTMENT  
District of Columbia

CASE NO. 17963

EXHIBIT NO. 28

Just say NO!!!

GOVERNMENT OF THE DISTRICT OF COLUMBIA  
Board of Zoning Adjustment

To ATTN: Mark Loud



JUN 24 2009

2009 SEP 22 AM 9:20

OFFICE OF ZONING

APPLICATION NUMBER 17963  
TO WHOM IT MAY CONCERN:

Notice is hereby given that the Board of Zoning Adjustment will hold a public hearing in Suite 220 South, 441 4<sup>th</sup> Street, N.W., Washington, D.C., 20001 on Tuesday, September 22, 2009, on the following application:

**Application of Euclid of Virginia**, pursuant to 11 DCMR § 3104.1, for a special exception to allow a new self-service gasoline station and convenience store under section 706, in the C-1 District at premises 4975 South Dakota Avenue, N.E. (Square 3899, Lot 76).

However, you may submit written views about this case. All written views should be sent to the address below, and must be received by the Board before the hearing on the case is concluded. Individuals and organizations wishing party status in any case before the Board must request that status in writing not less than fourteen (14) days prior to the date set for the public hearing on the particular application. The request for party status must contain all the information required by 11 DCMR § 3106.2. This application is located within the boundaries of Advisory Neighborhood Commission 5A. This application will be heard between 9:30 a.m. and 12:00 p.m.

If you have any questions about this application, you may call the Office of Zoning on (202) 727-6311, or visit the office at 441 4<sup>th</sup> Street, N.W., Suite 210 South, Washington, D.C. 20001. Please refer to the application number when you write or call about this case.

SINCERELY,

RICHARD S. NERO, JR.  
Acting Director  
Office of Zoning

EXHIBIT NO.

CASE NO.

17963

27

BOARD OF ZONING ADJUSTMENT  
District of Columbia

Call today and  
say NO!!! We must  
stand together. We don't need  
another Gas station!

441 4<sup>th</sup> Street, N.W., Suite 200/210-S, Washington, D.C. 20001

Telephone: (202) 727-6311

Facsimile: (202) 727-6072

E-Mail: [dc02@dc.gov](mailto:dc02@dc.gov)

Web Site: [www.dcoz-dc.gov](http://www.dcoz-dc.gov)

From: Robert L. Fikard  
4936 11th Street N.E.  
Washington D.C. 20017

\*my house faces the back  
of this property. Lightin  
202-841-5769 15aprob

★ ★ ★

**ADVISORY NEIGHBORHOOD COMMISSION 5A****SLOWE SCHOOL DEMOUNTABLE**

1322 IRVING STREET, N.E. ▼ WASHINGTON, D.C. 20017

TELEPHONE: 202-635-6563 ▼ FAX: 202-635-6565 ▼ Email: ANC5ADC@aol.com

OfficerAngel Alston  
ChairJanac Grant  
Vice-ChairSandi Washington  
TreasurerPatricia Roberts  
Corresponding SecretaryJohn Feeley  
Recording SecretaryRobert King  
ParliamentaryCommissioners5A01 Sandi Washington  
5A02 Patricia Roberts  
5A03 Angel Alston  
5A04 Herman Preston  
5A05 Steven McCoy  
5A06 John Feeley  
5A07 Carolyn Steptoe  
5A08 Timothy Thomas  
5A-09 Shirley Rivers Smith  
5A-10 Phillip Blair  
5A-11 Janac Grant  
5A-12 Robert (Bob) King

Date: September 17, 2009

To: Chairman Marc D. Loud  
Board of Zoning Adjustment  
441 4th Street NW, Suite 220  
Washington, DC 20001

Ref: # 17963 Application of Euclid of Virginia 4975 South Dakota Avenue, N.E. (Square 3899, Lot 76).

Advisory Neighborhood Commission 5A held its monthly meeting on Wednesday, July 22, 2009 at 7:00pm and with a quorum we voted to support the community in its efforts to oppose applied for (Special Exceptions) variances pursuant to 11 DCMR § 3104.1, which allows for a special exception to place a new self-service gasoline station and convenience store under section 706, in the C-1 District at premises 4975 South Dakota Ave, NE, WDC 20017.

ANC 5A heard reports from Commissioner Alston regarding Application #17963 at ANC 5A meetings on Wednesday, June 24, 2009 and Wednesday, July 22, 2009. On Monday September 14, 2009, ANC 5A voted once again to support the residents of SMD 5A03 in their opposition of this proposed self-service gasoline station and convenience store.

With proper notification, ANC Commissioner Angel Alston held a Single Member District meeting on June 23, 2009. With over 65 community members in attendance, the community voted against granting the special variances at 4975 South Dakota Avenue, N.E. Commissioner Alston also met with abutting SMD 5A04 and that SMD also voted to support the residents of SMD 5A03 with their opposition of this proposed gas station.

The residents

BOARD OF ZONING ADJUSTMENT  
District of Columbia

CASE NO.

17963

EXHIBIT NO.

26

- The community does not need and would not benefit from a "low-price" gas station
- The proposed location would be directly adjacent to an existing BP Gas Station and 3 blocks away from a new self-service gas station with 24 hour convenient store
- The current property does not have adequate square footage to provide gas service to vehicles, parking for its convenience store clientele and provide parking for its refilling tankers. This will

result in temporary parking/idling on the street until space becomes available, causing a traffic congestion and road hazards.

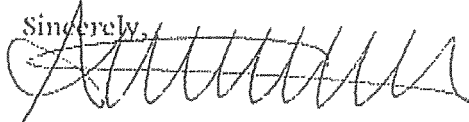
- The current location is surrounded by houses and a new gas station may potentially cause environmental problems to the community
- The current owners do not maintain their property and the community currently has issues with loitering and trash
- The current location is on a corner that is already congested due to the McDonald's across the street and the existing adjacent gas station
- The community does not support a 24 hour convenient store and has reservations about the inconsistency of information received regarding the proposed hours of operation
- The owners have not posted any visible signs or public notices of intent for the proposed use of the property.
- The current owners of the convenient store have been in our community for over 20 years and do not live in our neighborhood. They are also not active participants in our community events in 5A03 or ANC 5A.

ANC 5A request that the Board of Zoning Adjustment supports the request of the community and its ANC and deny the request of the applicant. The community is adamantly against this project and will not benefit in anyway.

As the Chair, I do authorize Commissioner Alston to represent her SMF at the hearing schedules for September 22, at 9:45a.m.

If you need any additional information please feel free to contact me at any time. Have a wonderful day!

Sincerely,



Angel Alston, Chair 5A  
202-556-5958 cell  
202-315-6057 cell  
202-635-6565 fax  
[angel.alston@anc.dc.gov](mailto:angel.alston@anc.dc.gov)

From the Desk of:  
**Angel Alston**  
Chair ANC 5A

RECEIVED  
D.C. OFFICE OF ZONING

2009 SEP 21 12:55  
Government of the District of Columbia  
Advisory Neighborhood Commission 5A  
1322 Irving Street NE,  
Washington, DC 20017  
202-635-6563 office  
202-635-6565 fax

**FAX**

**Fax Transmittal Form**

To:

**ATTN: Office of Zoning**

Government of the District of Columbia  
One Judiciary Square  
441 4th Street NW  
Suite 200 South  
Washington, DC 20001

From:

**Sender's Name: Angel Alston**

Advisory Neighborhood Commission 5A  
Phone: 202-556-5958 cell  
202-315-6057 cell  
angel.alston@anc.dc.gov  
angel.alston@yahoo.com

Fax number: (202) 727-6072

Urgent

For Review  
Please Comment  
Please Reply

Date sent: September 21, 2009

Time sent:

# of pages including cover page: 3

**Message:**

I am submitting this letter on behalf of ANC 5A. This is their official stance on BZA Applicant Number:

**Case Number :** 17963

**Case Name :** Application of Euclid of Virginia

**Case Summary :** (Special Exceptions) pursuant to 11 DCMR § 3104.1, for a special exception to allow a new self-service gasoline station and convenience store under section 706, in the C-1 District at premises 4975 South Dakota Avenue, N.E. (Square 3899, Lot 76).

**ANC :** 5A03

Please add this to the file to be heard at the hearing on tomorrow. Feel free to contact me via email at [angel.alston@yahoo.com](mailto:angel.alston@yahoo.com) or on my cell at 202-556-5958. Have a great day.

17963

# BEFORE THE BOARD OF ZONING ADJUSTMENT OF THE DISTRICT OF COLUMBIA

## AFFIDAVIT OF POSTING

Before completing this form, please review the instructions on the reverse side.  
Print or type all information unless otherwise indicated.

DISTRICT OF COLUMBIA, ss:

(Name of person posting the property)

Christopher Flack

, being first duly sworn, do hereby depose and say that:

I,

(Name of person posting the property)

Christopher Flack

, posted

(no. of posters)

2

zoning poster(s)

("Notice of Public Hearing") furnished by the Director of the Office of Zoning on private property known as

(address of premises)

4975 S. Dakota Ave, NE

on

(date of posting)

9/2/09

at

(time of posting)

5pm

The zoning posters were placed in plain view of the public on the street frontages as listed on this affidavit below. I have maintained the posting notice by checking the signs every five days, and by posting new notices as necessary.

I have taken, or authorized to be taken,

(no. of photos)

2

photograph(s) of the zoning posters where they have been

The photographs (not to exceed 8 1/2" X 11"), attached hereto, clearly show each zoning poster as seen by the public.

The photographs, are numbered and correspond to the following street frontages:

Photograph No.	Street Frontage
1	South Dakota (on front of property)
2	South Dakota (at street)
	BOARD OF ZONING ADJUSTMENT
	District of Columbia
	CASE NO. 17963
	EXHIBIT NO. 25

I/We certify that the above information is true and correct to the best of my/our knowledge, information and belief. Any person(s) using a fictitious name or address and/or knowingly making any false statement on this application/petition is in violation of D.C. Law and subject to a fine of not more than \$1,000 or 180 days imprisonment or both.  
(D.C. Official Code § 22-2405)

Date:

9/18/09

Signature:



Subscribed and sworn to before me this

(date)

18

day of

(month)

September, 2009.

(year)

(seal)

(Signature)

Notary Public, D.C.

My commission expires on:

(date)

My Commission Expires July 14, 2012

## INSTRUCTIONS

1. Attached photograph(s) showing each zoning poster ("Notice of Public Hearing"), as seen from the public street, in the space provided on the face of the affidavit. If more than one photograph is required, they should be mounted on separate sheets of 8 1/2" X 11" paper.
2. All photographs must be at least three inches by three inches (3"x3") and numbered to correspond to the street frontages listed on the face of the affidavit.
3. The Rules of Practice and Procedure before the Board of Zoning Adjustment require posting of the property, as follows:

**3113.14** Additional notice of the public hearing shall be given by the applicant by posting the property with notice of the hearing at least fifteen days in advance of the hearing.

**3113.15** Notice shall be posted at each street frontage on the property involved, and on the front of each building located on the subject property. Each notice shall be in plain view of the public.

**3113.16** Notices will be supplied by the Director showing the number of the application, the nature of the application, the name of the applicant, the property involved, the Advisory Neighborhood Commission (ANC) within which the property is located, and the location, time and date of the public hearing.

**3113.17** At least five (5) days prior to the public hearing, the applicant shall file with the Board a sworn affidavit demonstrating compliance with §3113.3. A form of affidavit supplied by the Board may be used but is not required.

**3113.18** The applicant shall attach to the affidavit described in §3113.15 a photograph of each sign after posting and as viewed by the public, identifying the street frontage and location of each sign.

**3113.19** The applicant shall make a reasonable effort to maintain the posted notice by checking the signs at least every five (5) days, and by posting new notice(s) as necessary.

**3113.20** In the case of an application for approval of a college or university campus plan, the notice shall be posted on all frontages of property included within the plan which face property not owned by the college or university.

4. At the conclusion of the hearing all Notices should promptly be removed to the property.





Photo 1

<http://www.me.com/wo/WebObjects/Webmail2.wba/wr?wodata=117...034.%3C3897DA63-57AA-4F87-8B6F-9320E440CE9C%40mac.com%3E-1>

9/18/09 1:05 PM

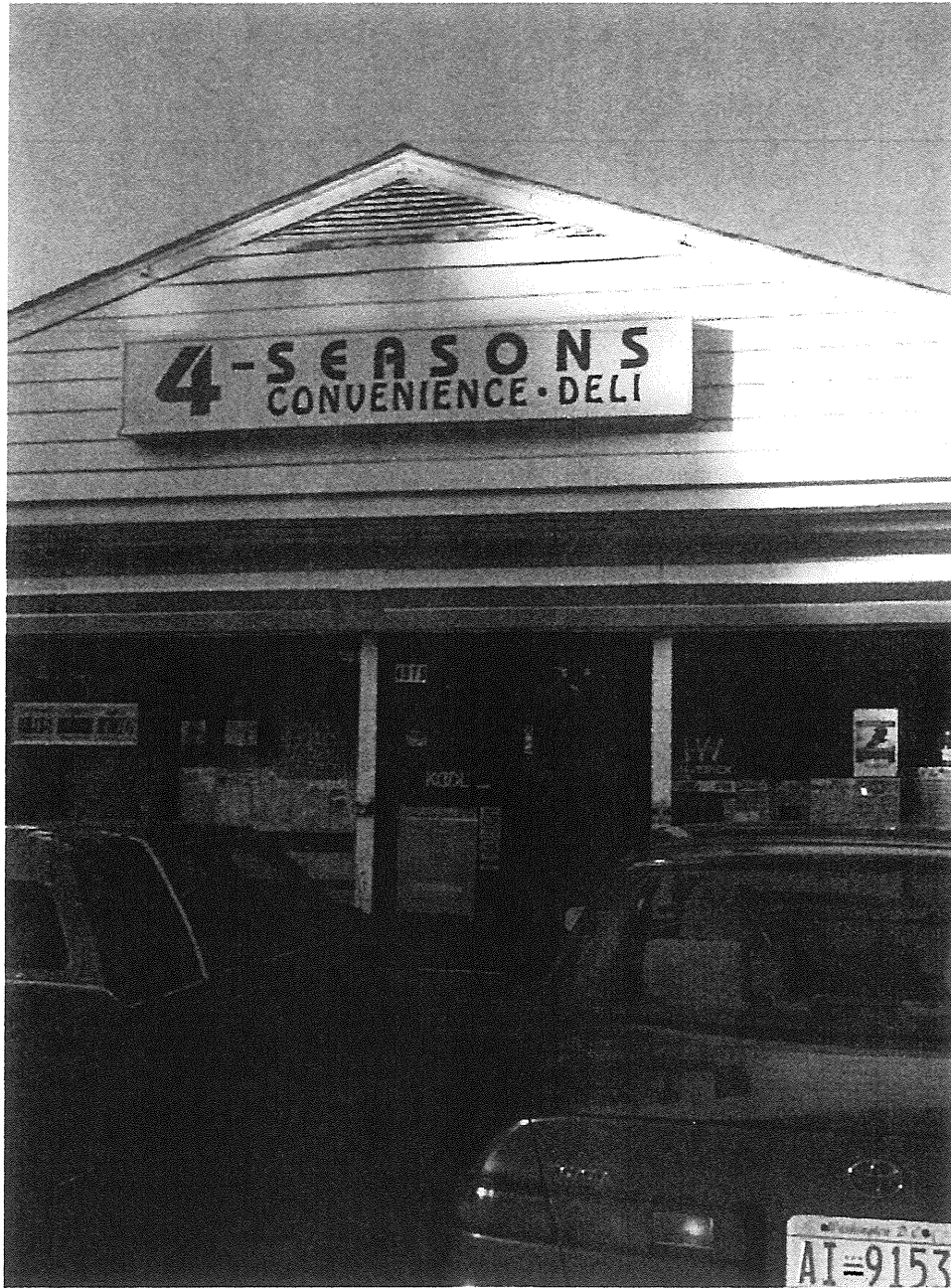
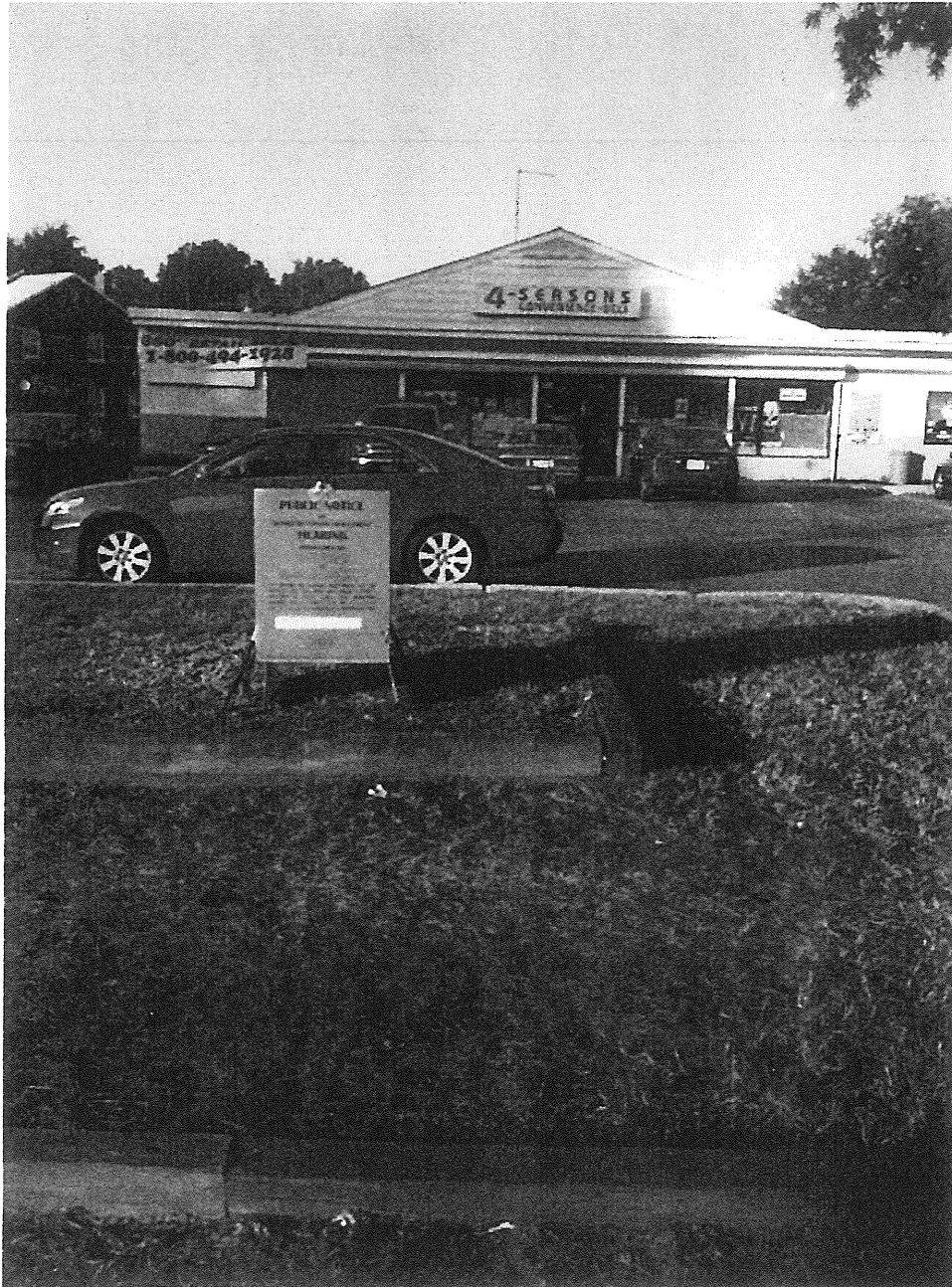




Photo 2

<http://www.me.com/wo/WebObjects/Webmail2.woa/wr?wodata=1171...926.%3CF0186BE2-C32E-4AF4-9E67-282DC6600E3E%40mac.com%3E-1> 9/18/09 1:05 PM



2009-09-11 11:38

**North Michigan Park Civic Association**

Post Office Box 29181  
Washington, DC 20017

September 11, 2009

Board of Zoning Adjustment  
441 Fourth Street, NW, Suite 220  
Washington, DC 20001

RE: Application Number 17963

Dear Sir:

It has come to our attention that Application Number 17963 has been submitted by Euclid of Virginia pursuant to 11 DCMR 3104.1, for a special exception to allow a new self-service gasoline station and convenience store under Section 706, in the C-1 District at premises 4975 South Dakota Avenue, NE (Square 3899, Lot 76). This area is located in the North Michigan Park Community.

Mr. Goss, representing the applicant, gave a presentation at the monthly announced meeting of the North Michigan Park Association on September 2, 2009. After much discussion 38 members voted to oppose and 9 members abstained. The reasons for the opposition are as indicated.

1. The proposed gasoline station would be next door to the existing Bp Station.
2. There would be three gasoline stations within four blocks on South Dakota Avenue.
3. There is already a high percentage of congestion at this area because of existing businesses such as the McDonald's across the street and another gasoline station would surely create a safety problem.
4. It is difficult to envision a gasoline station at the location because of the limited space.
5. The space is so limited, where would tankers park to refill the tanks, especially if other vehicles are on the premises?
6. This location is very close to homes and it could cause an environmental problem.
7. We have received conflicting information on the hours of operation and the intended use of the proposed facility.

BOARD OF ZONING ADJUSTMENT  
District of Columbia

CASE NO. 17963

EXHIBIT NO. 24

We appreciate the effort to bring additional services to our community, however, the North Michigan Park Civic that represents approximately 1700 homes support the Advisory Neighborhood Commission 5A in opposing the project. We, therefore request that the Board of Zoning Adjustment deny the application.

If additional information is needed, I can be reached on 202-526-7696.

Sincerely,


A handwritten signature in cursive script, appearing to read "Grace J. Lewis".

Grace J. Lewis  
President

Cc: Advisory Neighborhood Commission 5A

**MEMORANDUM**

**TO:** District of Columbia Board of Zoning Adjustment

**FROM:**  Jennifer Steingasser, Deputy Director Development Review & Historical Preservation

**DATE:** September 15, 2009

**SUBJECT:** BZA Case 17963 - Request for special exception relief at 4975 South Dakota Avenue, NE

**I. SUMMARY RECOMMENDATION**

The Office of Planning (OP) recommends **approval** of the requested special exception per §§ 726, 706 and 2302 for a gasoline station proposed on the referenced property as it adequately addressed the relevant review criteria. OP further recommends that approval be conditioned on the following:

- hours of operation shall be limited to 6:00 A.M. to 10 P.M.;
- flood lights shall be angled downward and shuttered in order to avoid light spillage; onto nearby residential properties;
- canopy lighting shall be recessed; and
- there shall be no exterior amplified sound system.

**II. AREA AND SITE DESCRIPTION**

Address:	4975 South Dakota Avenue, NE
Legal Description:	Square 3899, Lot 0076
Ward:	5A
Lot Characteristics:	An irregularly-shaped lot with frontage on Emerson, South Dakota, and Delafield Street, NE. Wide sidewalks extend along all three street frontages. The lot has been graded to be fairly level but a significant slope is retained along the northern and eastern property boundaries. A building restriction line extends 15-feet onto the property from the Delafield and Emerson Street frontages. The property also has three driveway curb cuts; two along South Dakota and one along Delafield Street.

Existing Development:	The one-story grocery (convenience) in a masonry building appears to be a former Seven-Eleven convenience store. The trash area just south of the building is surrounded and screened by a gated chain-link fence with wood slats. Most of the lot is paved to provide in excess of 12 on-site parking spaces. The chain-link fence and retaining wall behind the existing building extend along and perpendicular to the Delafield and Emerson Streets.
Zoning:	C-1 - food and grocery stores are allowed as matter of right per § 701.4, however a gasoline service station is subject to approval by the Board of Zoning Adjustment (BZA) in accordance with § 706.
Historic District:	None
Adjacent Properties:	Semi-detached one-family dwellings to the north and west across Emerson and Delafield Streets; other one-story commercial uses to the south across South Dakota Avenue; and a British Petroleum (BP) gasoline service station and grocery store on the adjacent property to the southeast.
Surrounding Neighborhood Character:	Predominantly commercial uses to the south in a C-1 district with moderate-density residential development beyond the commercial node in R-2 (refer to Exhibit 1).

### III. APPLICATION IN BRIEF

**Applicant:** Euclid of Virginia, LLC

**Proposal:** Raze the existing grocery store and construct a new 1,800 square foot building for the grocery with three gasoline pumps and a canopy over the pumps. Eight parking spaces would be provided on-site and one of the entrance driveways along South Dakota Avenue would be closed. No new driveway entrances or exits would be opened and both remaining driveway curb cuts would be a least 40 feet from the intersection of South Dakota Avenue with Delafield Street, NE.

**Relief and Zoning:** The applicant requests special exception approval in accordance with §§ 706.1 and 3104.1. Consistent with recent similar BZA cases, this proposal is also subject to the standards under § 2302.

The proposed grocery and gasoline station are subject to the following zoning provisions:

Standard	C-1 Requirement	Proposal	Relief Required?
Lot Area	N/A	15,748 sq. ft.	No
Building Area	–	1,800 square feet	No
Height, building (§770.1)	50 ft.- unlimited stories	16 feet	No
Height, accessory building (§ 2500.4)	15 feet	14 feet (canopy)	No
Lot Occupancy (§772)	60%	59%	No
Floor Area Ratio (§772)	1.0	0.11	No

Standard	C-1 Requirement	Proposal	Relief Required?
Yard, Rear (§774)	20 feet	45 feet <sup>1</sup>	No
Yard, Side (§775.5)	6 ft. (2 in./ft. of height, minimum 6 feet)	9 feet <sup>2</sup>	No
Parking (§2101.1)	1 space / 300 sq. ft. 3 (6 spaces)	8 spaces (excluding canopy spaces)	No

This proposal is also subject to the following additional standards.

#### IV. OFFICE OF PLANNING ANALYSIS

##### Consistency with § 706

*§ 706.3 The station shall not be located within twenty five feet (25 ft.) of a Residence District unless separated from the Residence District by a street or alley.*

The subject property is separated from the neighboring R-2 district by Delafield and Emerson Streets.

*§ 706.4 The operation of the use shall not create dangerous or other objectionable traffic conditions.*

The applicant provided traffic, queuing and truck maneuvering diagrams on the revised plans. One of the three existing driveway curb cuts into the site would be closed. District Department of Transportation (DDOT) comments were requested by OP regarding whether this proposal would result in any dangerous or objectionable traffic conditions.

*§ 706.5 The Board may impose requirements pertaining to design, appearance, screening, or lighting, or other requirements it deems necessary to protect adjacent or nearby property.*

In response to comments from OP and DDOT, the applicant made the following refinements to the submitted plans:

- changed the curb cut retained along South Dakota to only allow right-turn entry;
- indicated that sidewalks and ramps on and around the site would be ADA compliant;
- added bicycle parking;
- extended the trash enclosure, reducing the side yard setback to 0 feet;
- added windows along the Emerson street façade of the new building to provided more “eyes on the street” and increase security outside; and
- provided exterior lighting that is adequate for security and safety purposes, but does not intrude on surrounding residences.

<sup>1</sup> since the lot abuts right-of-way on three sides, the rear yard is measured to the Emerson Street centerline per §404.2

<sup>2</sup> the Zoning Administrator determined that the wall enclosing the trash area is not considered a structure and therefore is not subject to the required side yard setback under §775.5

<sup>3</sup> proposed 1,800 sq. ft. is less than the minimum 3,000 sq. ft. required to provide parking for retail uses in C-1

Beyond the innovative and modern design aesthetic, the applicant outlined the following additional environmental elements in a letter to OP dated September 4, 2009 intended to make this project more sustainable:

- locating this project within 1/2 mile of commuter rail (the Washington Metropolitan Transit Authority Fort Totten Rail Station);
- planning for e-car charging stations;
- providing alternative fuel pumps;
- recycling demolition materials into construction backfill;
- planting approximately 40% of the site area (excluding the building footprint) with native and adaptive species of trees and plants; and
- installing a vegetated roof for the purposes of energy conservation and efficiency, and the treatment of storm water.

In response to Metropolitan Police Department (MPD) concerns about extended hours on operation at this location, the applicant has proposed to limit the hours of operation for the new grocery and gasoline service station to 6:00 AM to 10:00 PM.

OP also recommends careful attention to the placement and orientation of any exterior lighting, and that there be no external amplified sound system.

*§ 706.6 Required parking spaces may be arranged so that all spaces are not accessible at all times. All parking spaces provided under this subsection shall be designed and operated so that sufficient access and maneuvering space is available to permit the parking and removal of any vehicles without moving any other vehicle onto public space.*

The applicant has provided parking in excess of the requirement.

#### Consistency with § 2302

*§ 2302.1 A public storage garage, repair garage, mechanical parking garage, or gasoline service station established or enlarged in any district except in the C-M or M Districts shall be subject to the provisions in this section.*

*§ 2302.2 No portion of the structure or premises to be used for any of the uses listed in §2302.1 shall be located within twenty five feet (25 ft.) of a Residence District unless separated from that Residence District by a street or alley.*

The subject property is separated from the neighboring R-2 district by Delafield and Emerson Streets.

*§ 2302.3 No use listed in §2302.1 shall have a vehicular entrance or exit connected with a street at a point closer than twenty five feet (25 ft.) to any Residence District existing at the time the use is established, unless separated from the Residence District by a street or alley.*

The vehicular entrance to the property on Delafield Street is separated from the R-2 district by Delafield Street.

*§ 2302.4 No driveway of any entrance or exit to any use listed in §2302.1 shall be closer than forty feet (40 ft.) to a street intersection as measured from the intersection of the curb lines extended.*

Two driveways remaining driveways along Delafield Street and South Dakota Avenue not meet this criterion.

*§ 2302.5 Except in a C-M or M District, all grease pits or hoists constructed or established as part of a use listed in §2302.1 shall be within a building.*

No grease pits or hoists are associated with this proposal.

Based on this information, approval of this request would be in harmony with the general purpose and intent of the Zoning Regulations and Zoning Maps, and would not tend to affect adversely the use of neighboring properties.

This application therefore meets the standards for special exception approval.

#### **V. AGENCY**

The applicant has met with DDOT staff separately and with OP. DDOT comments on this application are anticipated under a separate cover.

In response to an inquiry from OP, the Commander of the MPD Fourth District expressed opposition to the proposed gasoline station use due to community opposition to the this project, and out of concern that extended hours of operation at the subject property would require significant attention from the PSA 405 officers.

#### **VI. COMMUNITY COMMENTS**

The Advisory Neighborhood Commission (ANC) 5A Single-Member Representative indicated that there is significant neighborhood opposition to the proposed gasoline service station use. However, to date no final ANC resolution on this case has been added to the case record file.

#### **VII. CONCLUSION AND RECOMMENDATION**

OP recommends **approval** of the requested special exception per §§ 726, 706 and 2302 for a gasoline station proposed on the referenced property as it adequately addressed the relevant review criteria. OP further recommends that approval be conditioned on the following:

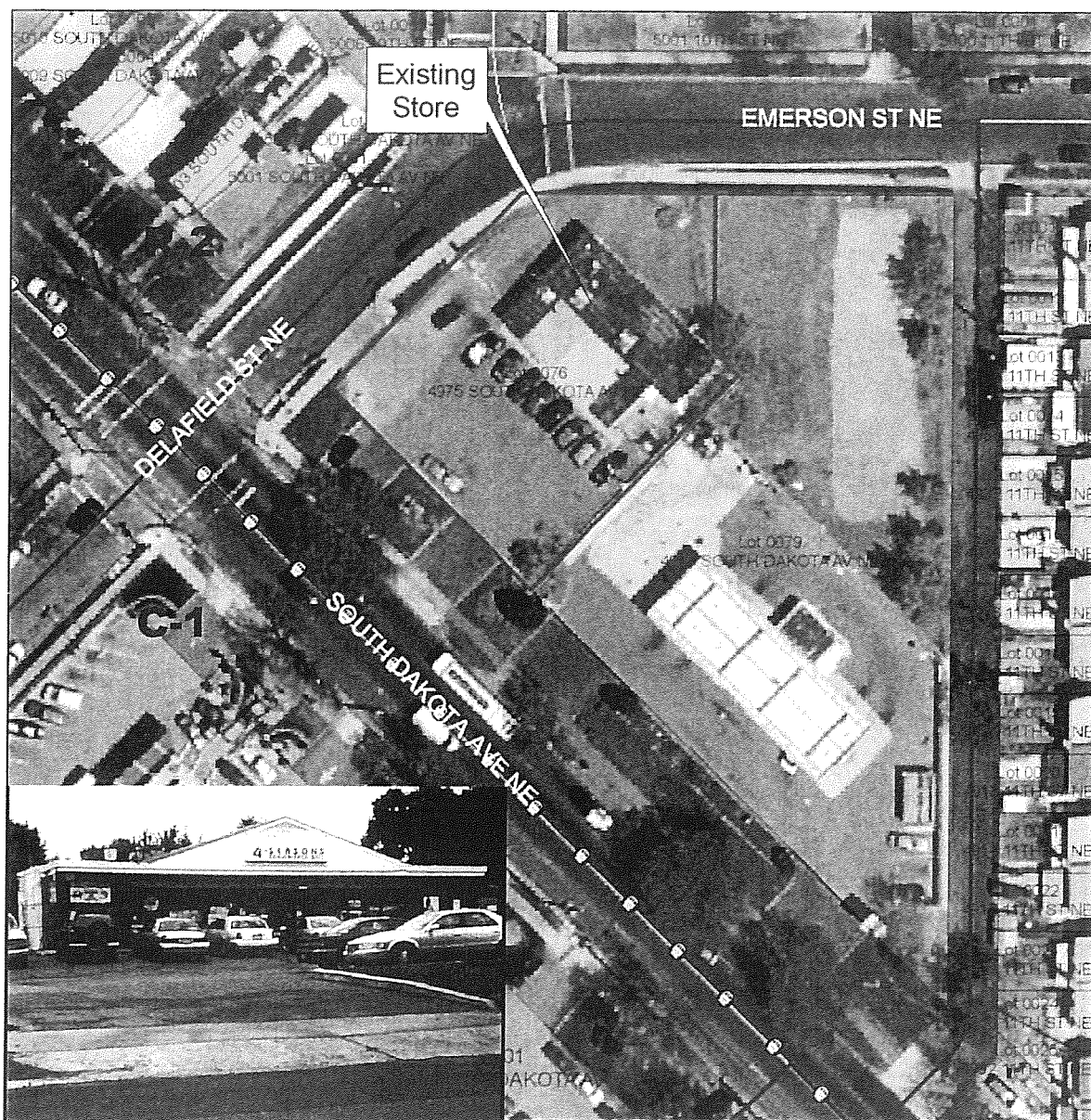
- hours of operation shall be limited to 6:00 A.M. to 10 P.M.;
- flood lights shall be angled downward and shuttered in order to avoid light spillage; onto nearby residential properties;
- canopy lighting shall be recessed; and
- there shall be no exterior amplified sound system.

Arthur Jackson, Case Manager

JS/afj

Attachments: Exhibit 1 Site Aerial with Zoning





**BZA Application No. 17963**  
**4975 South Dakota Ave NE**



- ★ ★ ★ Govt
- District of Columbia
- Adrian M. Fenty, Mayor

Office of Planning ~ August 3, 2009

This map was created for planning purposes from a variety of sources. It is neither a survey nor a legal document. Information provided by other agencies should be verified with them where appropriate.

**LEGEND**

- Metro Bus Routes
- Street Centerlines
- Water
- Parks

Koo Yuen Park

Owner

Euclid of Virginia, LLC

Developer

4975 South Dakota Avenue, NE

WASHINGTON, DC 20011

September 8, 2009

Supplemental Narrative in Support of BZA Application No. 17963

BOARD OF ZONING ADJUSTMENT  
District of Columbia

CASE NO. 17963

EXHIBIT NO. 22

## **Site and Area Description**

Property: Square 3899, Lot 76

ANC: 5A03

The Site is located on the north side of South Dakota at 4975 South Dakota Avenue, N.E., within the C-1 Zone District and is bounded by 10<sup>th</sup> Street to the north, Emerson Street to the northeast, and South Dakota Avenue to the west. Euclid of Virginia, LLC is proposing to raze the existing retail store and replace it with a newly constructed retail store and construct a new self-service gasoline station.

The site abuts a branded self-service gasoline station owned by BP Oil and is across the street from a Chinese restaurant, a fast food restaurant (McDonalds), a dry cleaning establishment and a beauty parlor.

The property is an irregular shaped site fronting South Dakota Avenue, Northeast. The lot size is 15,748 square feet with an existing building consisting of 1,895 square feet. The portion of the site adjoining South Dakota Avenue is a paved lot currently used for parking for customers of the retail store. This parking area makes up the majority of the area of the lot.

There are currently three existing curb cuts; two from South Dakota Avenue and one from Emerson Street.

The location of the proposed development is situated on a portion of South Dakota Avenue that is a high traffic densely developed neighborhood commercial district.

The subject site is surrounded by an R-2 District on all sides and is separated from that residential district by a street or alley. The nearest existing structure in the R-2 District to the west of the site is furthered buffered from the proposed self-service gasoline station use by the landscaping at the western portion of the subject lot.

Residential properties to the west are separated from the subject site by 10<sup>th</sup> Street and residential properties to the north are separated from the subject site by an alley and Emerson Street.

Two of the existing curb cuts will be used and are located more than 40 feet from the street intersection. No vehicular entrance or exit is connected with the street points closer than twenty-five (25) feet to any residential district.

## Zoning

The Site is located on the north side of South Dakota at 4975 South Dakota Avenue, N.E., within the C-1 Zone District. A gasoline station is a special exception use in a C-1 District. (§706.1).

The table below provides bulk and yard requirements.

Section	Item	C-1 Regulation	Current	Proposed
770	Height	40 feet	16	16
771	FAR	1.0	.114	.114
772	Lot Occupancy	60%	.59	.59
774	Rear Yard	20 feet	20	20
775	Side Yard	Not less than 6 feet	15	Side yard is being removed

## Analysis

Section 706 states that a gasoline service station established or enlarged after May 12, 1958, shall be permitted in the C-1 zone may be established pursuant to the special exception criteria of §§706 and 2302.

**706.1 A gasoline service station established or enlarged after May 12, 1958, shall be permitted in a C-1 District as a special exception if approved by the Board of Zoning Adjustment under § 3104, subject to the provisions of chapter 23 of this title and the provisions of this section.**

**706.3 The station shall not be located within twenty five feet (25 ft.) of a Residence District unless separated from the Residence District by a street or alley.**

The subject site is surrounded by an R-2 District on all sides and is separated from that residential district by a street or alley. The nearest existing structure in the R-2 District to the west of the site is furthered buffered from the proposed self-service gasoline station use by the landscaping at the western portion of the subject lot.

Residential properties to the west are separated from the subject site by 10<sup>th</sup> Street and residential properties to the north are separated from the subject site by an alley and Emerson Street.

**706.4 The operation of the use shall not create dangerous or other objectionable traffic conditions.**

The location of the proposed development is situated on a portion of South Dakota Avenue that is a high traffic densely developed neighborhood commercial district. There is no aspect of the physical characteristics of the property which would inhibit traffic flow or cause any adverse impact with the traffic of South Dakota Avenue in front of the site. To alleviate potential traffic hazards, the applicant is proposing to create one-way entrance from South Dakota Avenue and is proposing to close the second curb-cut onto the site from South Dakota Avenue closest to Emerson Street.

The self-service gasoline station would be situated so as to permit a free flow of traffic onto the site for access to the proposed convenience store. Patrons of the station will be drawn from vehicles already in the stream of traffic. The large area of the site provides for ample on-site vehicular circulation and safe pedestrian movement. The orientation of the three pump islands, all of which are parallel to South Dakota Avenue, will direct traffic away from the convenience store.

**706.5 The Board may impose requirements pertaining to design, appearance, screening, or lighting, or other requirements it deems necessary to protect adjacent or nearby property.**

The convenience store building will be 65 by 36 feet. The building will also contain the cashier's booth plus coolers, restrooms, and a storage area. The main food and sales area will measure approximately 1,800 square feet. The building will be centered on the site facing South Dakota Avenue.

There will be three new pump islands each with one self-service multi-hose dispenser capable of serving one car on each side of the island. An 84 foot by 28 foot steel and Alucobond aluminum canopy will be constructed over the pump islands. The pumps and the canopy will be located in the center of the site and will be perpendicular to South Dakota Avenue. The canopy will be lighted and will shelter customers pumping gas in all weather conditions.

Beyond the innovative and modern design aesthetic, also included are many environmental elements that make the project sustainable and beneficial to the ecosystem and the community as a whole.

**LEED Credit Points**

The Sustainable Aspects of the Site Include:

- Commuter rail within 1/2 mile of the site (Fort Totten)
- Bicycle rack within 200 yards of building
- Pedestrian and ADA compliant walks and ramps
- Planning for e-car charging stations
- Providing alternative fuel pumps

- Recycled materials used for construction backfill
- Planting 40% of the site area excluding the building foot print with native and adaptive species of trees and plants
- Installation of a vegetated roof for the purposes of energy conservation and efficiency and the treatment of storm water
- Interior lighting will be LED lighting and energy efficient light bulbs.

The exterior lighting that will be provided will be adequate for security and safety yet not too intrusive to the homes around the site.

**706.6 Required parking spaces may be arranged so that all spaces are not accessible at all times. All parking spaces provided under this subsection shall be designed and operated so that sufficient access and maneuvering space is available to permit the parking and removal of any vehicles without moving any other vehicle onto public space.**

The building on the site will contain approximately 1,800 square feet of gross floor area and will therefore require six (6) parking spaces. Eight (8) parking spaces are shown on the site plan and all spaces are designed to be accessible at all times.

- 2302.1 A public storage garage, repair garage, mechanical parking garage, or gasoline service station established or enlarged in any district except in the C-M or M Districts shall be subject to the provisions in this section.**
- 2302.2 No portion of the structure or premises to be used for any of the uses listed in §2302.1 shall be located within twenty five feet (25 ft.) of a Residence District unless separated from that Residence District by a street or alley.**
- 2302.3 No use listed in §2302.1 shall have a vehicular entrance or exit connected with a street at a point closer than twenty five feet (25 ft.) to any Residence District existing at the time the use is established, unless separated from the Residence District by a street or alley.**
- 2302.4 No driveway of any entrance or exit to any use listed in §2302.1 shall be closer than forty feet (40 ft.) to a street intersection as measured from the intersection of the curb lines extended.**

The subject site is surrounded by an R-2 District on all sides and is separated from that residential district by a street or alley. The nearest existing structure in the R-2 District to the west of the site is furthered buffered from the self-service gasoline station use by the landscaping at the western portion of the subject lot.

Residential properties to the west are separated from the subject site by 10<sup>th</sup> Street and residential properties to the north are separated from the subject site by an alley and Emerson Street.

**2302.5** Except in a C-M or M District, all grease pits or hoists constructed or established as part of a use listed in §2302.1 shall be within a building.

There will be no grease pits or hoists as part of the proposed use.

**2500.2** An accessory building shall be located only in a rear yard, except as follows:

- (a) An accessory private garage may be located in a side yard pursuant to § 2300; and
- (b) A pump island canopy and any kiosk adjacent to the pumps used exclusively as an attendant's shelter of a gasoline service station may be located in any open area of a lot not within twenty-five feet (25 ft.) of a Residence District unless separated therefrom by a street or alley.

Section 2500.2 indicates that pump island canopies are to be considered an accessory building. The proposed canopy would meet the locational requirements of this section.

**2500.4** An accessory building in any zone district shall not exceed one (1) story or fifteen feet (15 ft.) in height, except as provided in § 2500.5.

Section 2500.4 states that accessory buildings may not exceed 15 feet in height. The height of the canopy will be 14 feet and would meet this standard.



g2 design collective

1400 W STREET, NW, WASHINGTON, DC 20008  
202.550.1032

Seal:

A Project for:

4975 SOUTH DAKOTA AVE, NE  
WASHINGTON, DC 20017

Client:

EUCLID OF VIRGINIA, LLC

PO BOX 9492  
WASHINGTON, DC 20016

Revisions:

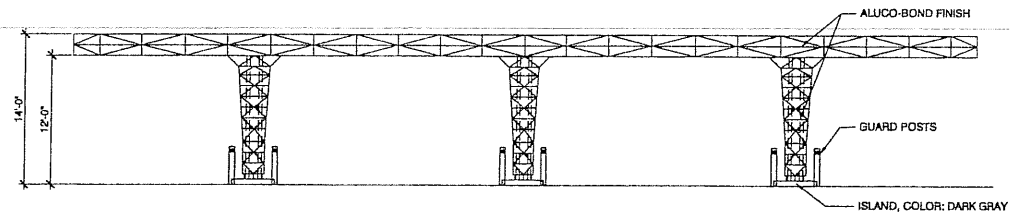
No.	Description	Date
Project No.:	09.002.01	
Drawn By:	JW	
Reviewed By:	JW	
Scale:	AS NOTED	
Date:	01-26-09	
Filename:		
Sheet Title:		

CANOPY  
ELEVATIONS,  
ELECTRICAL PLAN  
AND  
DETAILS

Sheet #:

CA1.0

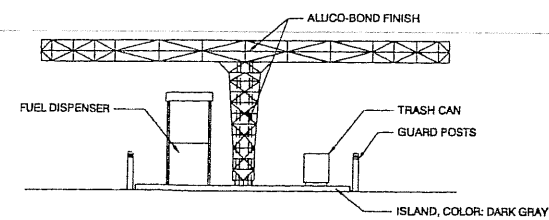
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CANOPY FRONT ELEVATION

SCALE  
1/8" = 1'-0"

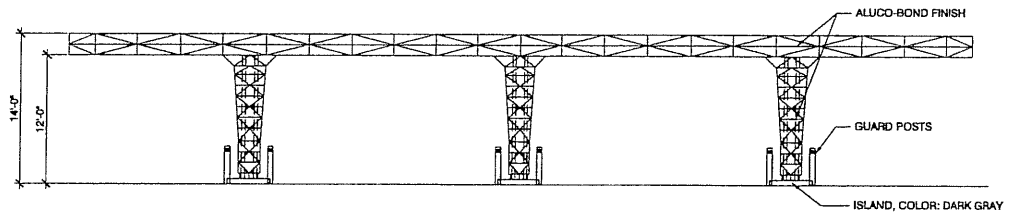
1



CANOPY SIDE ELEVATION

SCALE  
1/8" = 1'-0"

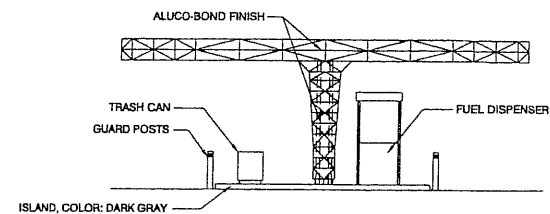
3



CANOPY REAR ELEVATION

SCALE  
1/8" = 1'-0"

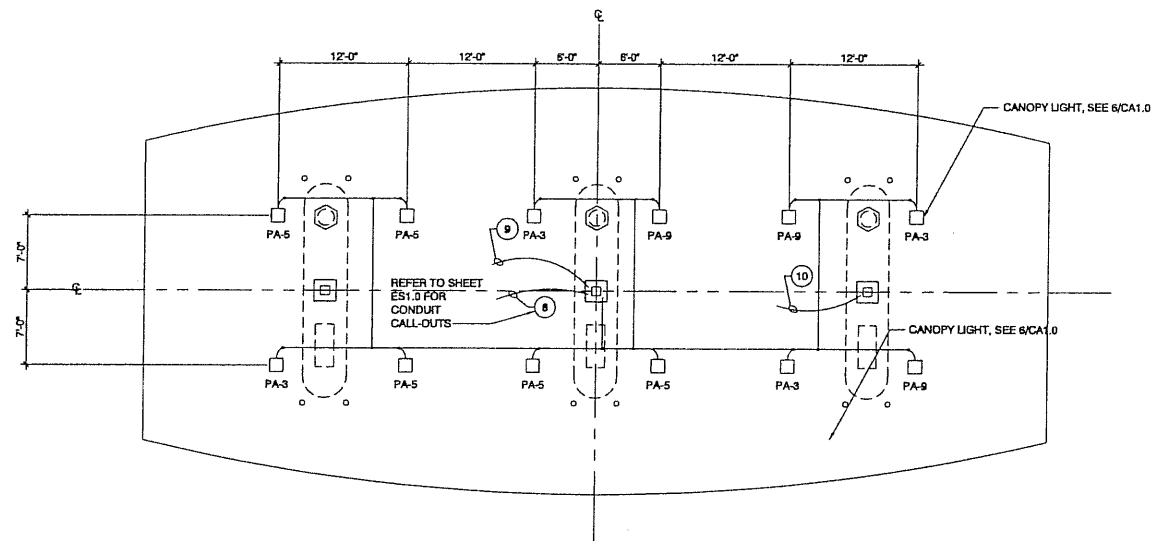
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CANOPY SIDE ELEVATION

SCALE  
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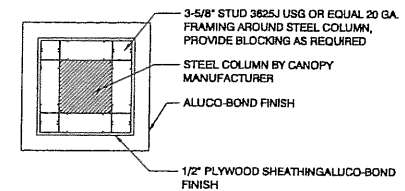
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CANOPY ELECTRICAL PLAN

SCALE  
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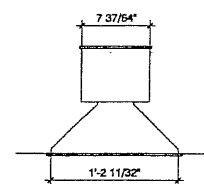
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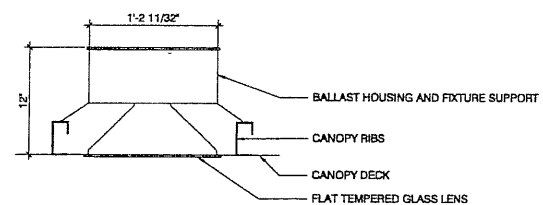
CANOPY COLUMN SECTION

SCALE  
3/4" = 1'-0"

7



END VIEW



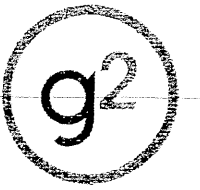
SIDE VIEW

CANOPY LIGHT (FLAT LENS SCOTTSDALE)

SCALE  
N.T.S.

6





g2 design collective

1400 W STREET, NW, WASHINGTON, DC 20008  
202.550.1032

Seal:

A Project for:

4975 SOUTH DAKOTA AVE, NE  
WASHINGTON, DC 20017

Client:

EUCLID OF VIRGINIA, LLC

PO BOX 9492  
WASHINGTON, DC 20016

Revisions:

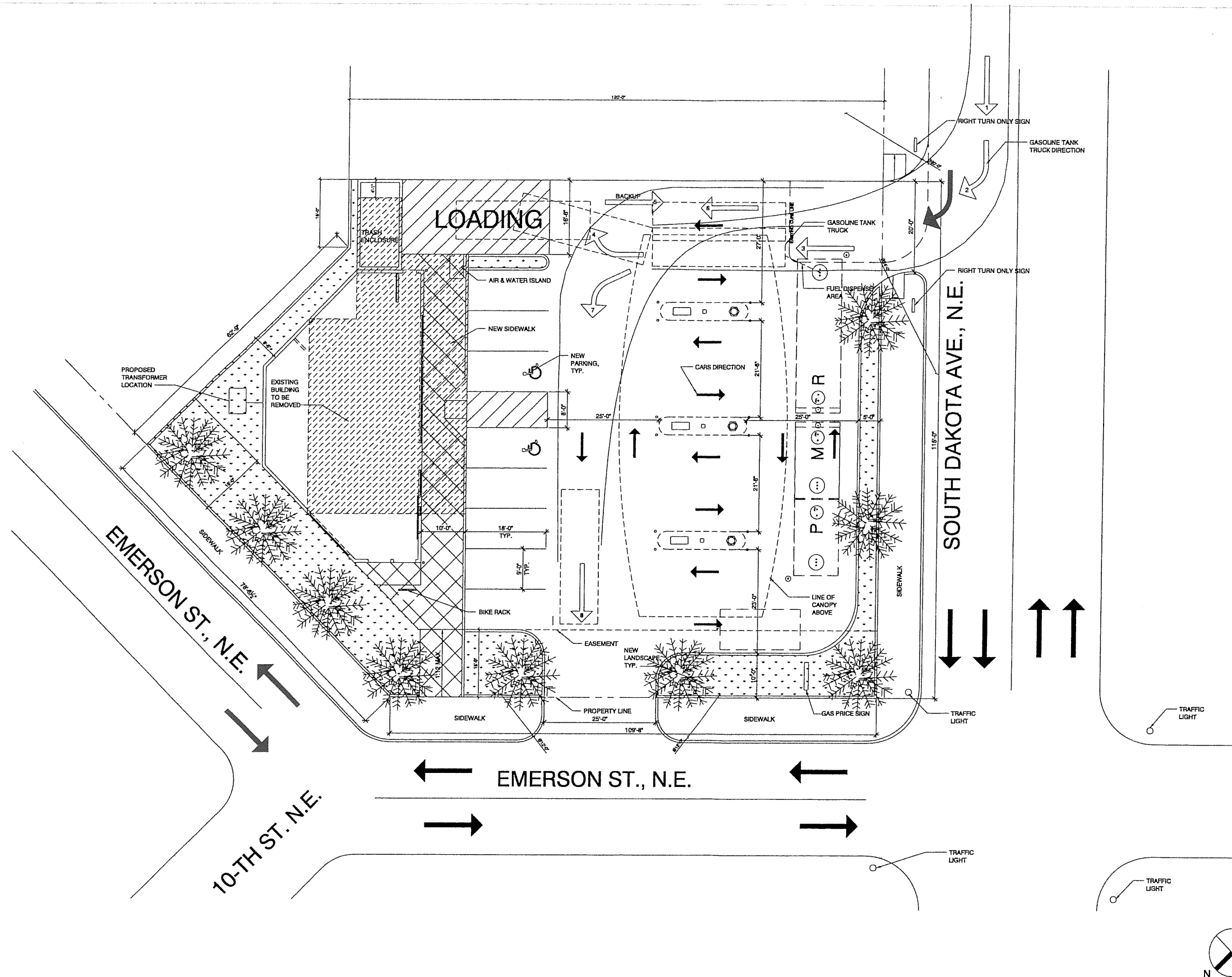
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No. Description	Date
Project No.:	09.002.01
Drawn By:	JW
Reviewed By:	JW
Scale:	3/32" = 1'-0"
Date:	01-26-09
Filename:	
Sheet Title:	

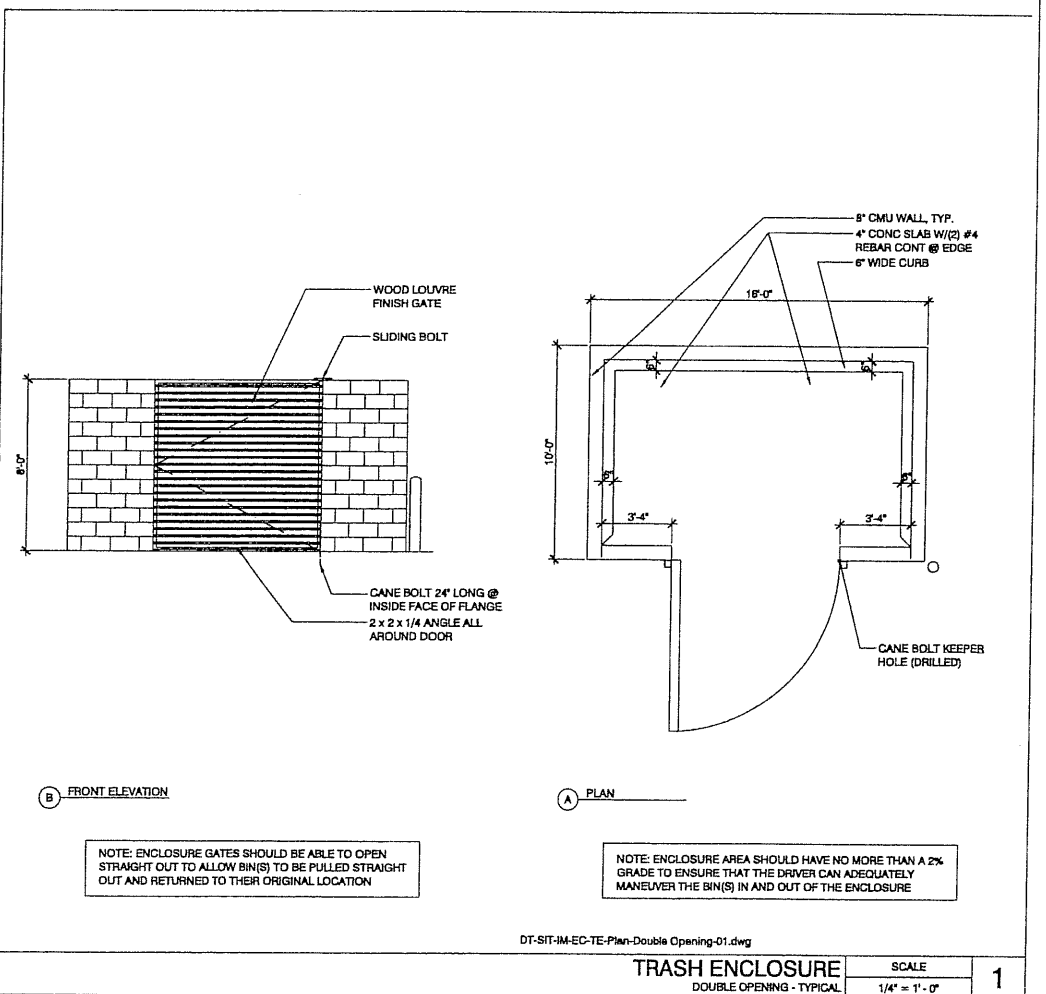
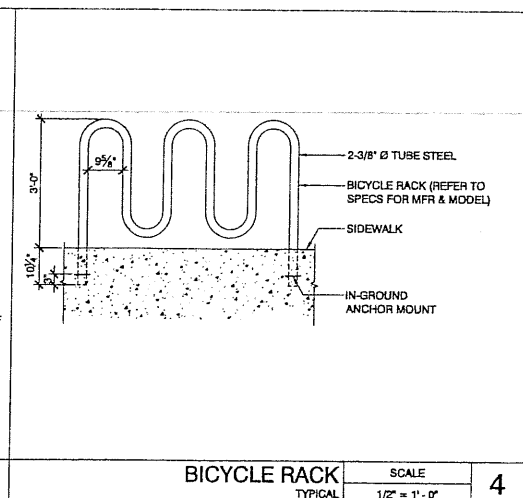
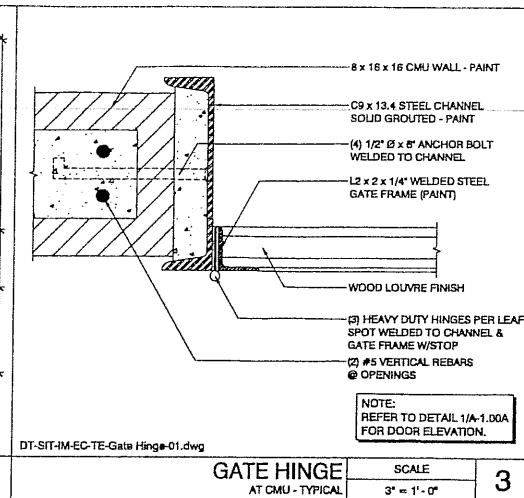
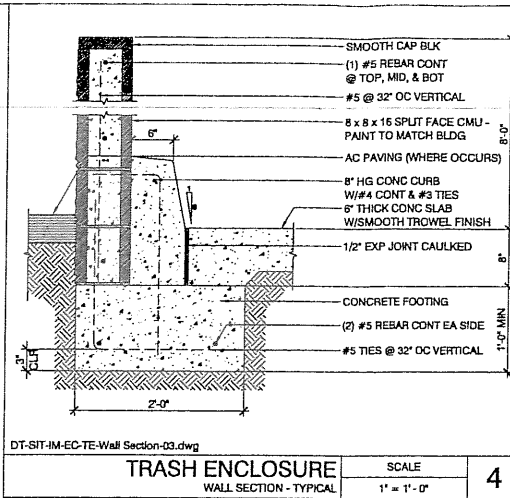
SITE  
PLAN

Sheet #:

AS-1.0

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1400 W STREET, NW, WASHINGTON, DC 20008  
 202.550.1032

Seal:

A Project for:

4975 SOUTH DAKOTA AVE, NE  
 WASHINGTON, DC 20017

Client:

**EUCLID OF VIRGINIA, LLC**  
 PO BOX 9492  
 WASHINGTON, DC 20016

Revisions:


City Comments	08-14-09
No. Description	Date
Project No.:	09.002.01
Drawn By:	CC
Reviewed By:	CC
Scale:	AS NOTED
Date:	01-26-09
Filename:	
Sheet Title:	

**TRASH ENCLOSURE DETAILS**

Sheet #:

**A4.0**

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1400 W STREET, NW, WASHINGTON, DC 20009  
202.550.1032

Seal:

A Project for:

4975 SOUTH DAKOTA AVE, NE  
WASHINGTON, DC 20017

Client:

EUCLID OF VIRGINIA, LLC

PO BOX 9492  
WASHINGTON, DC 20016

Revisions:

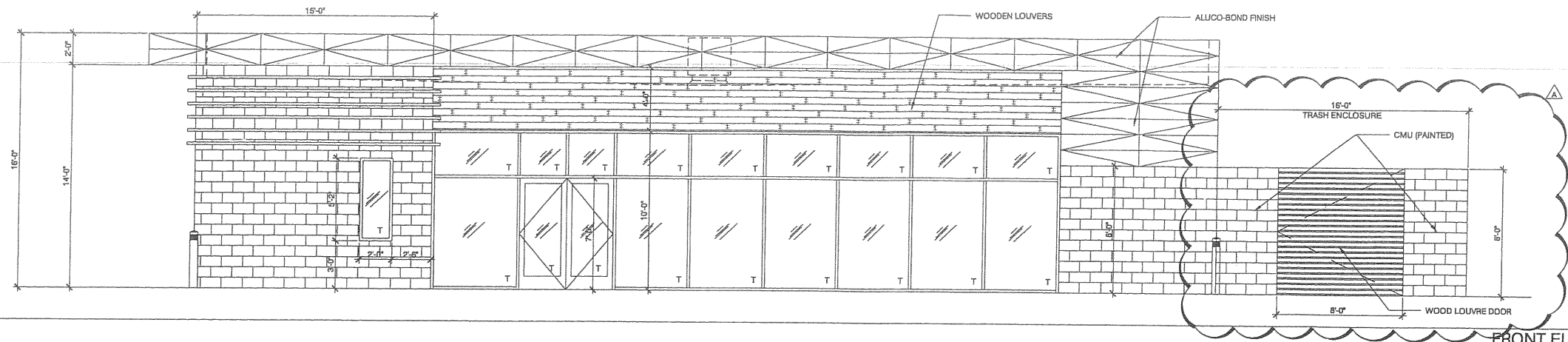
No.	Description	Date
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3	Drawn By:	CC
4	Reviewed By:	CC
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6	Date:	01-28-09
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8	Sheet Title:	

BUILDING  
ELEVATIONS

Sheet #:

A2.0

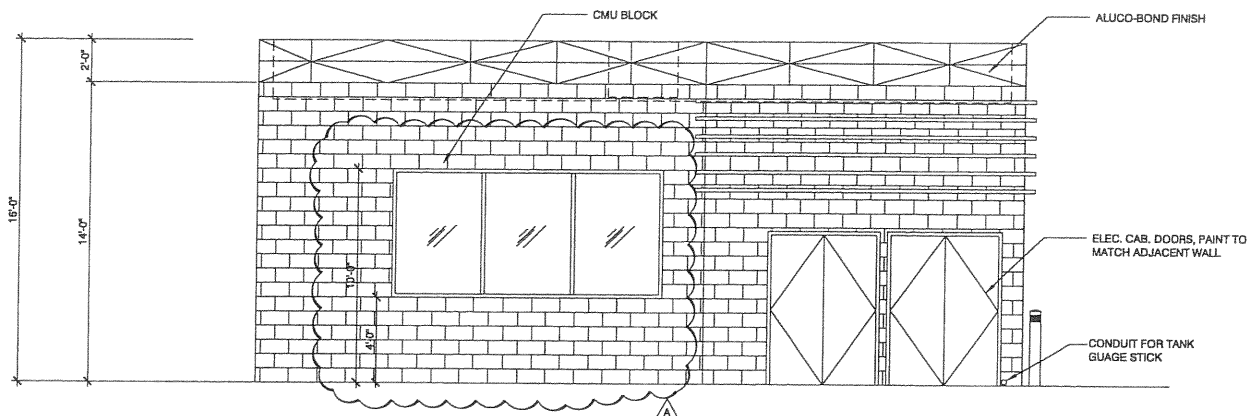
© G2 DESIGN - ALL RIGHTS RESERVED



FRONT ELEVATION

SCALE  
1/4" = 1' - 0"

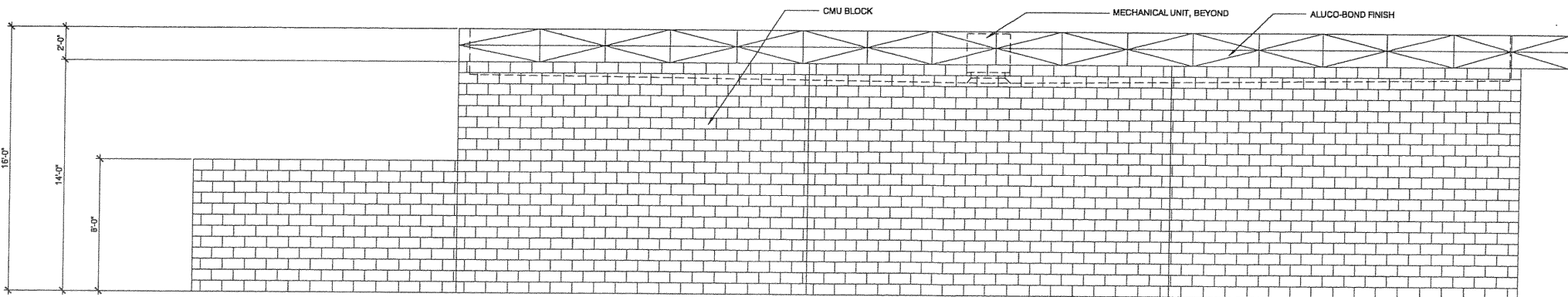
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SIDE ELEVATION

SCALE  
1/4" = 1' - 0"

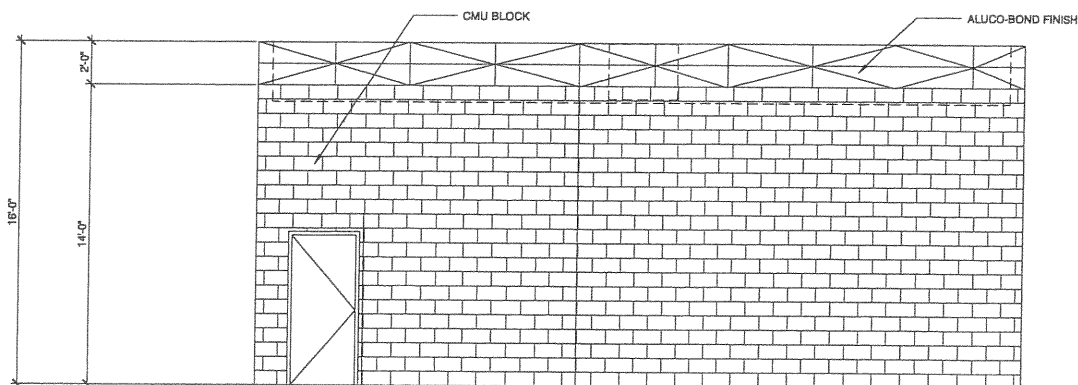
2



REAR ELEVATION

SCALE  
1/4" = 1' - 0"

3



SIDE ELEVATION

SCALE  
1/4" = 1' - 0"


4

GOVERNMENT OF THE DISTRICT OF COLUMBIA  
BOARD OF ZONING ADJUSTMENT



BZA APPLICATION NO: 17963

As Director of the Office of Zoning, I certify and attest that letters of notice have been prepared and mailed to all owners of property within 200 feet of the subject property based on the listing of owners submitted by the applicant as required by instructions on the BZA Application Form, and as determined by this office; the ANC Single Member District, and the City Council Member for the Ward of the subject application.

ATTESTED BY:   
RICHARD S. NERO, JR.  
Acting Director, Office of Zoning

DATE: JUN 24 2009

BOARD OF ZONING ADJUSTMENT  
District of Columbia

CASE NO. 17963

EXHIBIT NO. 21

GOVERNMENT OF THE DISTRICT OF COLUMBIA  
Board of Zoning Adjustment



JUN 24 2009

**APPLICATION NUMBER 17963**  
**TO WHOM IT MAY CONCERN:**


Notice is hereby given that the Board of Zoning Adjustment will hold a public hearing in Suite 220 South, 441 4<sup>th</sup> Street, N.W., Washington, D.C., 20001 on Tuesday, September 22, 2009, on the following application:

**Application of Euclid of Virginia**, pursuant to 11 DCMR § 3104.1, for a special exception to allow a new self-service gasoline station and convenience store under section 706, in the C-1 District at premises 4975 South Dakota Avenue, N.E. (Square 3899, Lot 76).

However, you may submit written views about this case. All written views should be sent to the address below, and must be received by the Board before the hearing on the case is concluded. Individuals and organizations wishing party status in any case before the Board must request that status in writing not less than fourteen (14) days prior to the date set for the public hearing on the particular application. The request for party status must contain all the information required by 11 DCMR § 3106.2. This application is located within the boundaries of Advisory Neighborhood Commission 5A. This application will be heard between 9:30 a.m. and 12:00 p.m.

If you have any questions about this application, you may call the Office of Zoning on (202) 727-6311, or visit the office at 441 4<sup>th</sup> Street, N.W., Suite 210 South, Washington, D.C. 20001. Please refer to the application number when you write or call about this case.

SINCERELY,



**RICHARD S. NERO, JR.**  
**Acting Director**  
**Office of Zoning**

BOARD OF ZONING ADJUSTMENT  
District of Columbia

CASE NO. 17963

EXHIBIT NO. 20

---

441 4<sup>th</sup> Street, N.W., Suite 200/210-S, Washington, D.C. 20001

Telephone: (202) 727-6311

Facsimile: (202) 727-6072

E-Mail: [dcz@dc.gov](mailto:dcz@dc.gov)

Web Site: [www.dcoz.dc.gov](http://www.dcoz.dc.gov)

GOVERNMENT OF THE DISTRICT OF COLUMBIA  
Board of Zoning Adjustment



JUN 24 2009

Chairperson  
Advisory Neighborhood Commission 5A  
1322 Irving Street, N.E.  
Washington, D.C. 20017

**Re: BZA Application No. 17963**

Dear Chairperson:

This is to advise you that a public hearing has been scheduled by the Board of Zoning Adjustment to consider the following application that is located within the boundaries of your ANC area:

**Application of Euclid of Virginia**, pursuant to 11 DCMR § 3104.1, for a special exception to allow a new self-service gasoline station and convenience store under section 706, in the C-1 District at premises 4975 South Dakota Avenue, N.E. (Square 3899, Lot 76).

The hearing will be held on Tuesday, September 22, 2009, in Suite 220, 441 4<sup>th</sup> Street, N.W., Washington, D.C. 20001. This application will be heard between 9:30 a.m. and 12:00 p.m.

You are advised that 11 DCMR Section 3115.1 sets out certain requirements related to reports of ANCs, and specifically that Section 3115 requires that the written report of the ANC be filed with the Board at least seven days in advance of the hearing. A copy of Section 3115 is enclosed for your convenience. Information on the procedures which will govern consideration of these cases are also available from the Office of Zoning, in Suite 200-S, 441 4<sup>th</sup> Street, N.W. Washington, D.C. 20001. The telephone number for the Office of Zoning is (202) 727-6311.

BOARD OF ZONING ADJUSTMENT  
District of Columbia

CASE NO. 17963

EXHIBIT NO. 19

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441 4<sup>th</sup> Street, N.W., Suite 200/210-S, Washington, D.C. 20001

Telephone: (202) 727-6311

Facsimile: (202) 727-6072

E-Mail: [dcoz@dc.gov](mailto:dcoz@dc.gov)

Web Site: [www.dcoz.dc.gov](http://www.dcoz.dc.gov)

## EXCERPT FROM TITLE 11 DCMR, "ZONING

### 3115 ADVISORY NEIGHBORHOOD COMMISSIONS

**3115.1** The written report of the ANC shall be submitted to the Board at least seven (7) days in advance of the hearing and shall contain the following information:

- (a) An identification of the appeal or application;
- (b) When the public meeting of the ANC to consider the appeal or application was held;
- (c) Whether proper notice of that meeting was given by the ANC;
- (d) The number of members of the ANC that constitutes a quorum and the number of members present at the meeting;
- (e) The issues and concerns of the ANC about the appeal or application, as related to the standards of the Zoning Regulations against the appeal or application must be judged;
- (f) The recommendation, if any, of the ANC as to the disposition of the appeal or application;
- (g) The vote on the motion to adopt the report to the Board;
- (h) The name of the person who is authorized by the ANC to present the report; and
- (i) The signature of the chairperson or vice-chairperson of the ANC.


**3115.2** The Board shall give "great weight" to the written report of the ANC, as required by No. 1-261, D.C. Code, 1982 ed.

**3115.3** In the event the ANC submits its report on the basis of understandings, agreements, or meetings with the appellant, or applicant which later are modified by appellant or applicant, the designated representative of the ANC may comment orally concerning the specific inconsistencies. No other new matters may be presented orally by the designated representative of the ANC.

**BZA APPLICATION NOS. 17963**  
**PAGE NO. 2**

If you wish to forward comments in writing directly to the Board, such comments should be addressed to the Board of Zoning Adjustment at 441 4<sup>th</sup> Street, N.W., Suite 210 South, Washington, D.C. 20001.

**SINCERELY,**



**RICHARD S. NERO, JR**  
**Acting Director**  
**Office of Zoning**

Enclosure



GOVERNMENT OF THE DISTRICT OF COLUMBIA  
Board of Zoning Adjustment



JUN 24 2009

Michael Goss  
1400 W Street, N.W.  
Washington, D.C. 20009

Koo Yuen Park  
P.O. Box 9492  
Washington, D.C. 20016

**Re: BZA Application No. 17963**

Dear Mr. Goss:

Your application has been accepted as complete. You are hereby notified to appear before the Board of Zoning Adjustment (Board) on Tuesday, September 22, 2009, at 441 4<sup>th</sup> Street, N.W., Suite 220 South, Washington, D.C., 20001 for a public hearing concerning the following application:

**Application of Euclid of Virginia**, pursuant to 11 DCMR § 3104.1, for a special exception to allow a new self-service gasoline station and convenience store under section 706, in the C-1 District at premises 4975 South Dakota Avenue, N.E. (Square 3899, Lot 76).

The property involved in this application is located within the boundaries of Advisory Neighborhood Commission 5A. This application will be heard between 9:30 a.m. and 12:00 p.m.

**PLEASE NOTE THAT A SIGN(S) MUST BE OBTAINED FROM THE OFFICE OF ZONING TO BE POSTED ON THE PROPERTY.** The Board's Rules of Practice and Procedure (11 DCMR §§ 3113.14 through 3113.20) provides that the notice sign must be posted at least 15 days prior to the hearing. The posted notice must also be checked at least once every five days, and replaced when necessary. An affidavit concerning the original posting of the sign must be filed with the Board at least 5 days prior to the hearing. The sign and the affidavit should be picked up from the Office of Zoning, Suite 200 South, 441 4<sup>th</sup> Street, N.W., Washington, D.C. 20001.

You should be aware that letters and other documents may be submitted to the Board by other individuals, organizations and government agencies both in support of and in opposition to your application. At least one week prior to the public hearing, you should

---

441 4<sup>th</sup> Street, N.W., Suite 200/210-S, Washington, D.C. 20001

Telephone: (202) 727-6311

Facsimile: (202) 727-6072

E-Mail: [dcoz@dc.gov](mailto:dcoz@dc.gov)

Web Site: [www.dcoz.dc.gov](http://www.dcoz.dc.gov)

BOARD OF ZONING ADJUSTMENT  
District of Columbia

CASE NO. 17963

BZA APPLICATION NO. 17963

PAGE NO. 2

review the file in your application so that you are prepared to respond to any issues that may be raised regarding your application.

Further, if you are not clear about what information you must present to the Board in support of your application, please contact the Office of Zoning as soon as possible. If you have any questions or require any additional information, feel free to call the Office of Zoning at (202) 727-6311.

SINCERELY,

A handwritten signature in black ink, appearing to read 'R. Nero, Jr.', written over the printed name.

**RICHARD S. NERO, JR.**

**Acting Director**

**Office of Zoning**

GOVERNMENT OF THE DISTRICT OF COLUMBIA  
Office of Zoning



MEMORANDUM

To: Director  
Department of Transportation  
KARINA RICKS

From: Richard S. Nero, Jr. RN  
Acting Director

Date: APR 23 2009

Re: Notice of Filing Date. 4/22/09 BZA Application No. 17963

The above cited application is being referred to you for informational purposes only, unless DDOT's review and report on this matter is otherwise requested by the Office of Planning (OP). The contact person at the OP is Jennifer Steingasser, who can be reached on 202-442-8808. The approximate public hearing date for this matter is Sept 09. If you have any questions, please call Clifford Moy, Secretary to the BZA, on 202-727-6311.

BOARD OF ZONING ADJUSTMENT  
District of Columbia

CASE NO. 17963

EXHIBIT NO. 17

GOVERNMENT OF THE DISTRICT OF COLUMBIA  
Office of Zoning



MEMORANDUM

To: City Council Member For Ward

5

From: Richard S. Nero, Jr. RN  
Acting Director

Date: APR 23 2009

Re: Notice of Filing Date . 4/22/09 BZA Application No. 17963

This is to inform you that the above-numbered case has been filed with the Board of Zoning Adjustment for zoning relief. The property that is the subject of this case is within your Ward. Therefore, I am attaching a copy of the case, as this will affect your Ward. For additional information, please contact the Office of Zoning at 202-727-6311.

APPROXIMATE HEARING DATE: Sept 09

BOARD OF ZONING ADJUSTMENT  
District of Columbia

CASE NO. 17963

EXHIBIT NO. 16

GOVERNMENT OF THE DISTRICT OF COLUMBIA  
Office of Zoning



MEMORANDUM

To: Director, Office of Planning  
Development Review & Coordination

From: Richard S. Nero, Jr. *RN*  
Acting Director

Date: *APR 23 2009*

Re: BZA Application No. *17963*

Attached hereto is a copy of the above-numbered application with related memorandum or a Notice of Certification from the Zoning Administrator's Office. This application was filed on *4/22/09*. Please note that 11 DCMR, subsection 3114.2, requires that your report be filed at least seven (7) days prior to the date set for the public hearing in this case.

APPROXIMATE HEARING DATE: *SEPT 09*

BOARD OF ZONING ADJUSTMENT  
District of Columbia

CASE NO. *17963*

EXHIBIT NO. *13*

GOVERNMENT OF THE DISTRICT OF COLUMBIA  
Office of Zoning



MEMORANDUM

To: Single Member District – ANC  
5A03

From: Richard S. Nero, Jr. RN  
Acting Director

Date: APR 23 2009

Re: Notice of Filing Date: 4/22/09 BZA Case No. 17963

This is to inform you that the above-numbered case has been filed before the Board of Zoning Adjustment for zoning relief. The property that is the subject of this case is within your Single Member District Boundary. Therefore, I am attaching a copy of the case, as your Single Member District may be affected. For additional information, please contact the Office of Zoning at 202-727-6311.

APPROXIMATE HEARING DATE: Sept 09

A copy of 11 DCMR Section 3115 is on the reverse side.

BOARD OF ZONING ADJUSTMENT  
District of Columbia

CASE NO. 17963

EXHIBIT NO. 15

**GOVERNMENT OF THE DISTRICT OF COLUMBIA**  
**Office of Zoning**



**11 DCMR SECTION 3115**

**3115 ADVISORY NEIGHBORHOOD COMMISSION REPORTS**

**3115.1** The written report of the ANC shall be submitted to the Board at least seven (7) days in advance of the hearing and shall contain the following information:

- (a) An Identification of the appeal or application;
- (b) When the public meeting of the ANC to consider the appeal or application was held;
- (c) Whether proper notice of that meeting was given by the ANC;
- (d) The number of members of the ANC that constitute a quorum and the number of members present at the meeting;
- (e) The issues and concerns of the ANC about the appeal or application as related to the standards of the Zoning Regulations against which the appeal or application must be judged;
- (f) The recommendation, if any, of the ANC as to the disposition of the appeal or application;
- (g) The vote on the motion to adopt the report to the Board;
- (h) The name of the person authorized by the ANC to present the report; and
- (i) The signature of the ANC chairperson or vice-chairperson.

**3115.2** The Board shall give "great weight" to the written report of the ANC, as required by D.C. Code , 2001 Ed. §1-309.10.(d) (formerly codified at D.C. Code § 1-261 (1994 Repl. 1999 Supp)).

**3115.3** In the event the ANC submits its report on the basis of understandings, agreements, or meetings with the appellant or applicant that later are modified by the appellant or applicant, the designated ANC representative may comment orally concerning the specific inconsistencies. No other new matters may be presented orally by the designated ANC representative.

GOVERNMENT OF THE DISTRICT OF COLUMBIA  
Office of Zoning



MEMORANDUM

To: Advisory Neighborhood Commission  
5A

From: Richard S. Nero, Jr. RN  
Acting Director

Date: APR 23 2009

Re: BZA Application No. 17963

In accordance with DCMR 3112.7 and 3113.7, you are hereby advised that the above-mentioned application, which falls within the boundaries of your Advisory Neighborhood Commission, was filed with the BZA on 4/22/09. A copy of the above-numbered application is attached for your information and use. This is not a notice of public hearing on the application. You will receive notice of the hearing by mail and through the D.C. Register approximately forty (40) days in the advance of the hearing. In accordance with 11 DCMR 3115, the written report of an ANC shall contain certain information specified in that section. A copy of Section 3115 is on the reverse side. If you have any questions, please contact the Office of Zoning at 202-727-6311.

APPROXIMATE HEARING DATE: SEPT 09

BOARD OF ZONING ADJUSTMENT  
District of Columbia

CASE NO. 17963

EXHIBIT NO. 14



GOVERNMENT OF THE DISTRICT OF COLUMBIA  
Office of Zoning



11 DCMR SECTION 3115

**3115 ADVISORY NEIGHBORHOOD COMMISSION REPORTS**

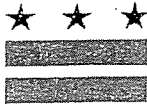
**3115.1** The written report of the ANC shall be submitted to the Board at least seven (7) days in advance of the hearing and shall contain the following information:

- (a) An Identification of the appeal or application;
- (b) When the public meeting of the ANC to consider the appeal or application was held;
- (c) Whether proper notice of that meeting was given by the ANC;
- (d) The number of members of the ANC that constitute a quorum and the number of members present at the meeting;
- (e) The issues and concerns of the ANC about the appeal or application as related to the standards of the Zoning Regulations against which the appeal or application must be judged;
- (f) The recommendation, if any, of the ANC as to the disposition of the appeal or application;
- (g) The vote on the motion to adopt the report to the Board;
- (h) The name of the person authorized by the ANC to present the report; and
- (i) The signature of the ANC chairperson or vice-chairperson.

**3115.2** The Board shall give "great weight" to the written report of the ANC, as required by No. 1-261, D.C. Code, 1982 ed.

**3115.3** In the event the ANC submits its report on the basis of understandings, agreements, or meetings with the appellant or applicant that later are modified by the appellant or applicant, the designated ANC representative may comment orally concerning the specific inconsistencies. No other new matters may be presented orally by the designated ANC representative.

Government of the District of Columbia  
BOARD OF ZONING ADJUSTMENT



Consideration of your Application No. 17963, filed on 4/22/09, will be governed by the Board of Zoning Adjustment Rules of Practice and Procedure, effective on October 1, 1999. Under these Rules, your application will be set for public hearing on the first available date. The Board holds at least **TWO HEARINGS** per month, but typically **FOUR HEARINGS**. Cases are set for hearing on a first come, first serve basis. You will be notified of the date of the hearing approximately 40 days in advance.

When you receive the notice of public hearing, it will then be your responsibility as the applicant to post a notice of the public hearing on the property. You must pick up a posting sign from the Office of Zoning, and place it on each street frontage of the property a **MINIMUM of FIFTEEN (15) DAYS** prior to the date set for public hearing. You must check the sign at least once every five (5) days to be sure that it is in place, and must repost as necessary.

At the time of the initial posting, you must take a photograph of the sign. That photograph must be filed with the Office of Zoning a **MINIMUM of FIVE (5) DAYS** before the date set for public hearing, as an attachment to the affidavit of posting, which the Board will supply.

Please be further advised that it is also the responsibility of the applicant to carry the burden of proof for an application. That means that you should review the Zoning Regulations which apply to your application, as set forth in the notice of public hearing and should be prepared to tell the Board how your application meets the requirements of the Regulations against which it must be judged. (For variances refer to 11 DCMR 3103.2 and for special exceptions 3104.1)

The site of your application is within the jurisdiction of Advisory Neighborhood Commission (ANC) 5A. If that ANC submits written comments on your application, the Board will give those comments "great weight" in its decision. The Board encourages you to contact the ANC to discuss your application at the earliest practical time. The telephone number of ANC 5A03 is 635-6563 (202)

You are also advised that if you intend to submit any additional materials in support of the application, any documentary evidence must be filed with the Office of Zoning at least **FOURTEEN (14) DAYS** prior to the **date of the hearing**. Documentary evidence includes statements, information, briefs, letters, reports, reports of experts and other witnesses, plans and other material.

Should you have any questions during the period your application is pending before the Board, please call the staff at (202) 727-6311.

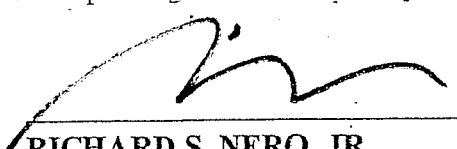
BOARD OF ZONING ADJUSTMENT  
District of Columbia

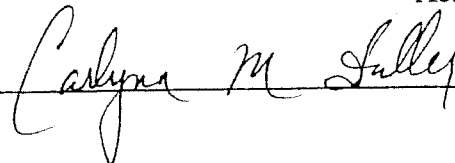
CASE NO. 17963



EXHIBIT NO. 12

ACKNOWLEDGEMENT

sab/12-00  
Form OZ-01

  
RICHARD S. NERO, JR.  
Acting Director



<b>HUNTINGTON CAR CARE</b> P. O BOX 9492 WASHINGTON, DC 20016		<b>CAP</b> Asset Management Account 4/20/2009 DATE	1484 15-122/540 BRANCH 06185
PAY TO THE ORDER OF <u>D.C. Treasury</u>		\$ <u>4000.00</u>	
<u>Four Thousand</u>		DOLLARS	
 <b>WACHOVIA SECURITIES</b> Wachovia Bank, N.A. ACH R/T 054001220		CODE <input type="checkbox"/>	
FOR <u>4975 S.D.</u>			

**DC Office of Zoning**  
**Board of Zoning Adjustment**  
 441 4th Street, NW, Suite 200-S  
 Washington, DC 20001

**RECEIPT**

BZA Case No. <u>17963</u>	Check No. <u>1484</u>
Name <u>Koo Yuen Park</u>	Date <u>4/20/2009</u>
Address <u>4975 South Dakota Ave, NE</u>	Amount <u>\$7100.00</u>

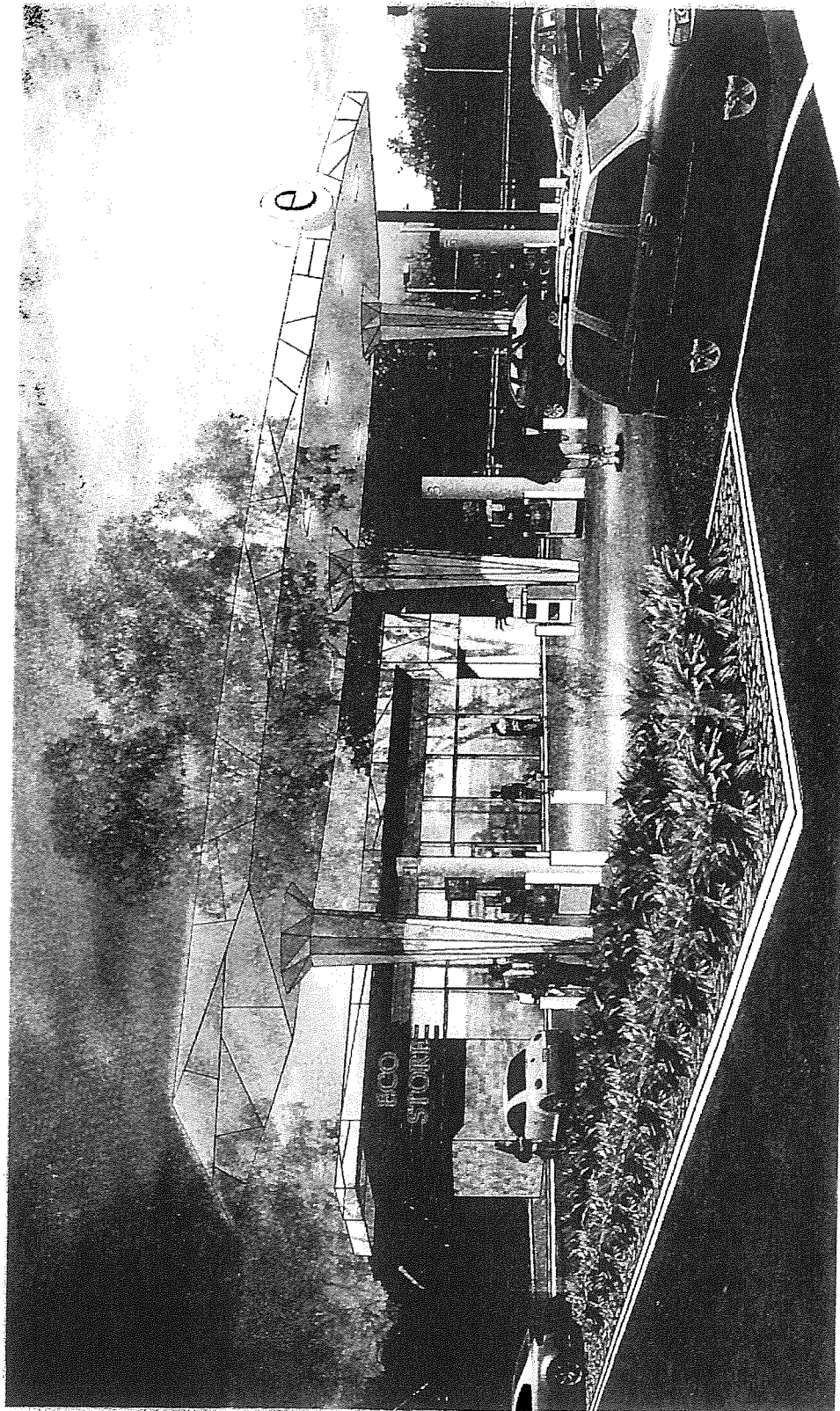
Description  
 Euclid of Virginia, LLC is proposing to ~~have~~ the existing convenience store and build a new convenience store and self-service gasoline station consisting of three new pump islands each with one self-service.  
 Sq. 3899 Lot 76 (Zoning District C-1)

Green - File Copy

BOARD OF ZONING ADJUSTMENT  
 District of Columbia

CASE NO. 17963

EXHIBIT NO. 11



PROPOSED CONCEPTUAL PERSPECTIVE  
FOR 4975 SOUTH DAKOTA AVE.

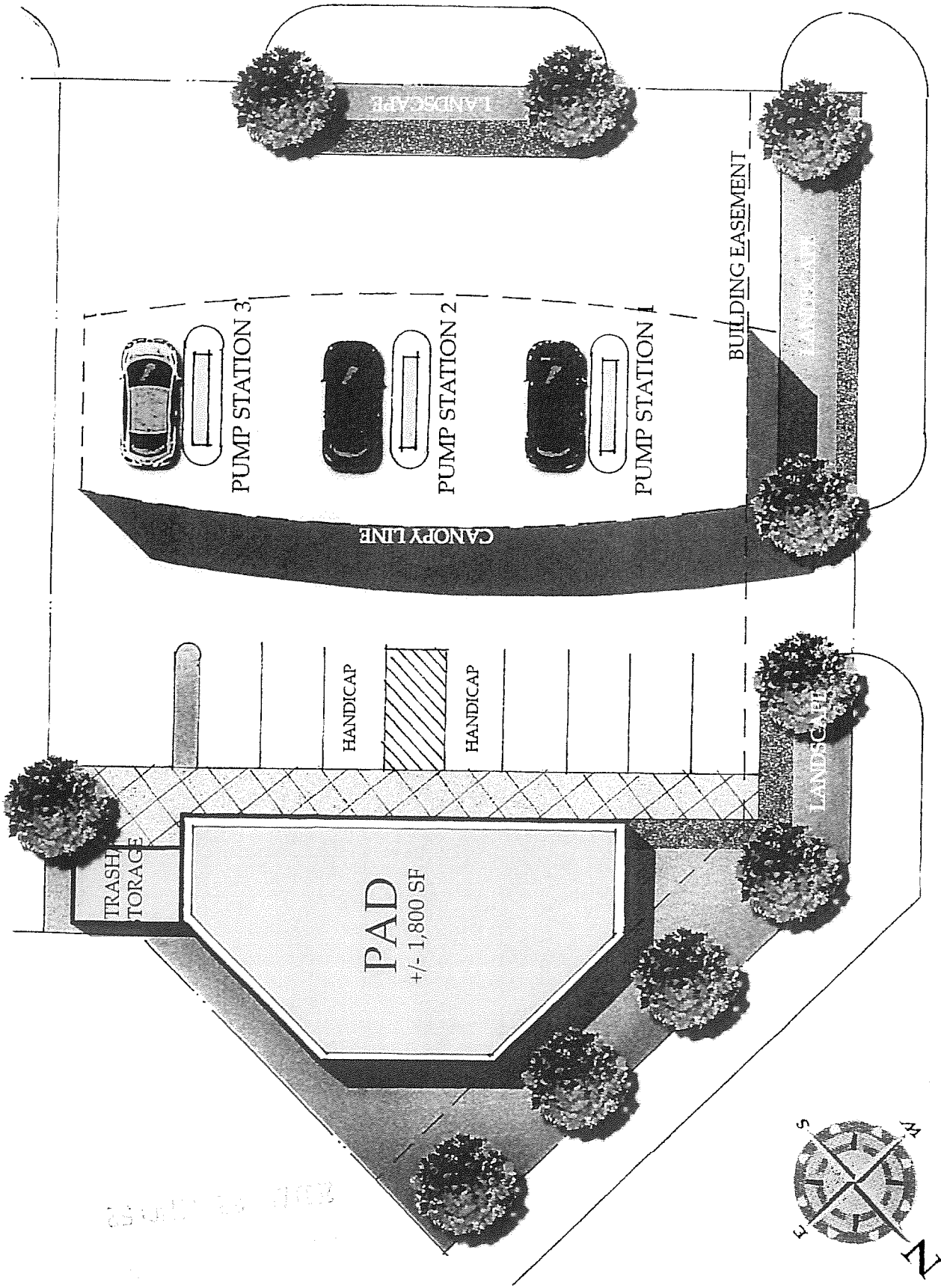
S-A  
DECEMBER 10, 2008

BOARD OF ZONING ADJUSTMENT  
District of Columbia

CASE NO. 17963

EXHIBIT NO. 10





EMERSON STREET

# PROPOSED CONCEPTUAL PLAN FOR 4975 SOUTH DAKOTA AVE.



EXHIBIT NO. 9

6 maximum spacing of 2 x 4 spacers

2 x 4 anchors

2 x 4 weir

1 x 4 spacer

7" stone

Filter cloth

2 minimum length of 2 x 4

sand bag or alternative weight

INLET

In pipe

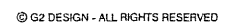
STANDARD SYMBOL

U.S. DEPARTMENT OF AGRICULTURE  
SOIL CONSERVATION SERVICE  
COLLEGE PARK, MARYLAND

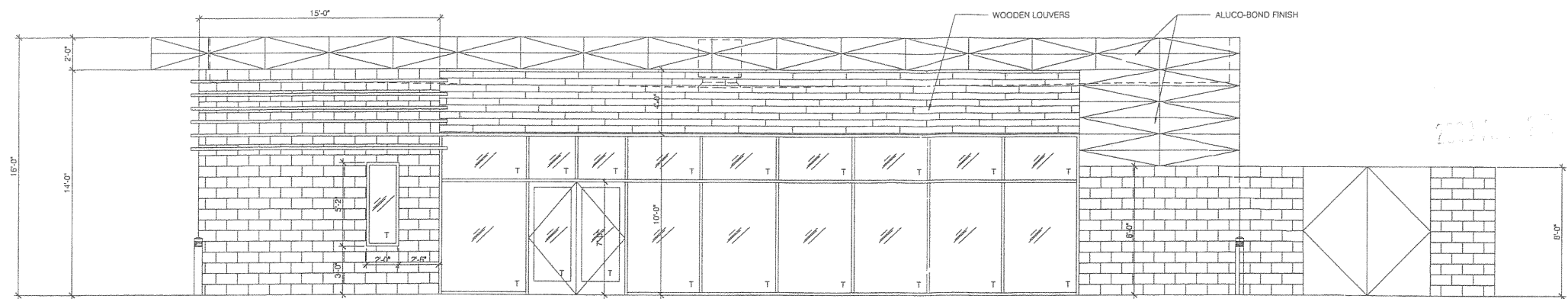
INLET PROTECTION  
DETAIL

STANDARD DRAWING  
IPO-1

\_\_\_\_\_



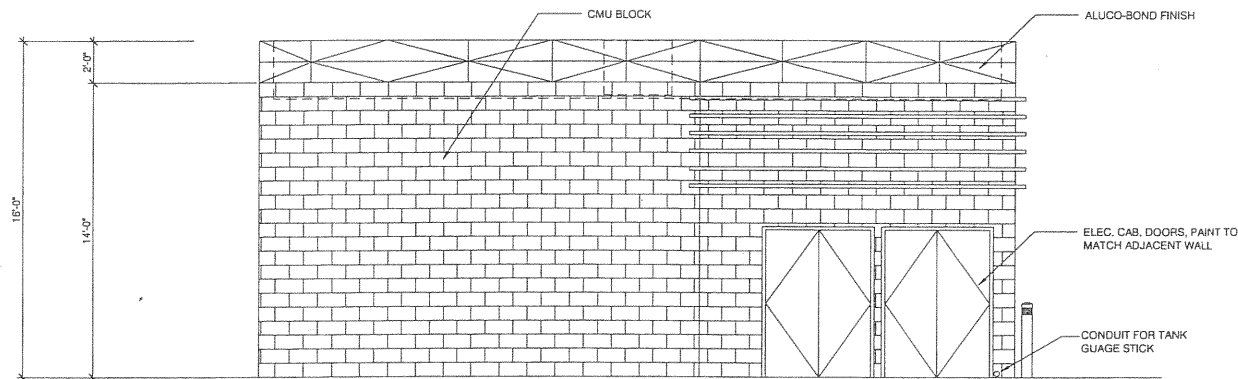




FRONT ELEVATION

SCALE  
1/4" = 1' - 0"

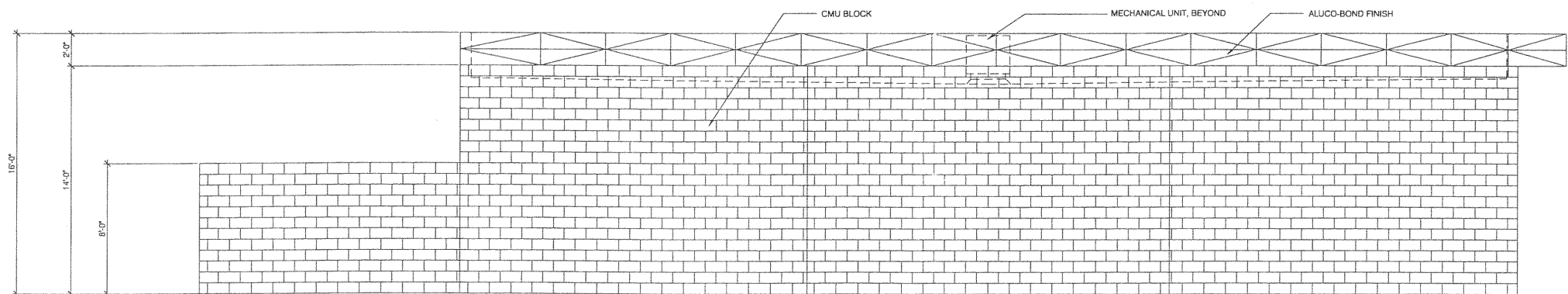
1



SIDE ELEVATION

SCALE  
1/4" = 1' - 0"

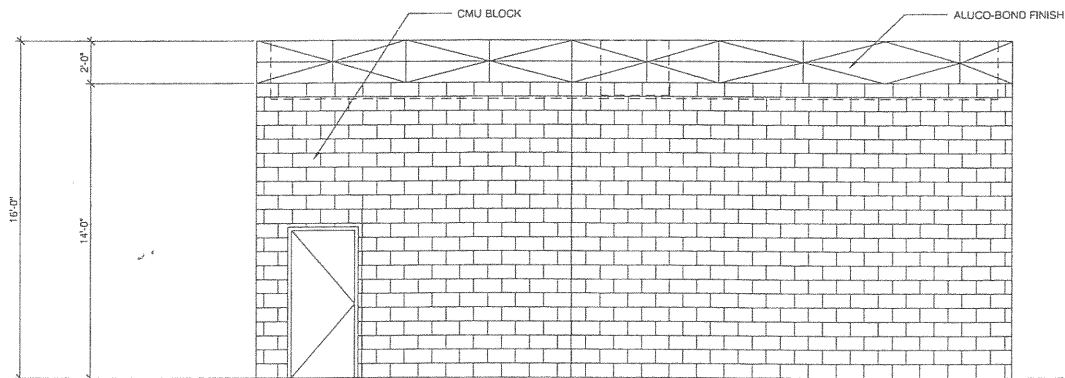
2



REAR ELEVATION

SCALE  
1/4" = 1' - 0"

3



SIDE ELEVATION

SCALE  
1/4" = 1' - 0"

4



1400 W STREET, NW, WASHINGTON, DC 20009  
202.550.1032

Seat:

A Project for:

4975 SOUTH DAKOTA AVE, NE  
WASHINGTON, DC 20017

Client:

EUCLID OF VIRGINIA, LLC  
PO BOX 9492  
WASHINGTON, DC 20016

Revisions:

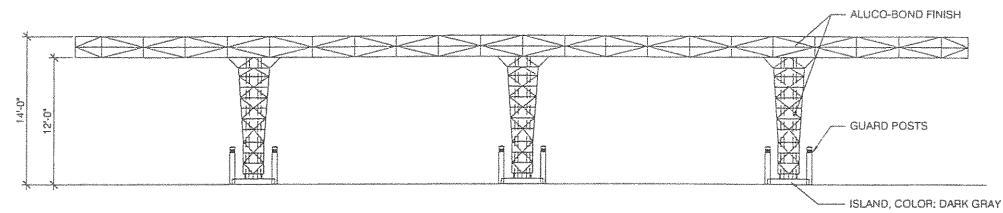
No.	Description	Date
Project No.:	09.002.01	
Drawn By:	JW	
Reviewed By:	JW	
Scale:	AS NOTED	
Date:	01-26-09	
Filename:		
Sheet Title:		

BUILDING  
ELEVATIONS

Sheet #:

A2.0

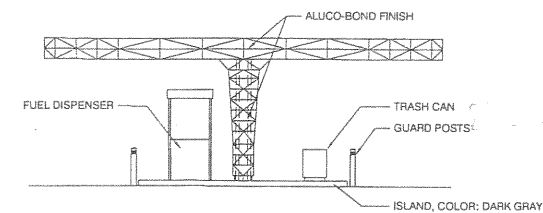
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CANOPY FRONT ELEVATION

SCALE  
1/8" = 1' - 0"

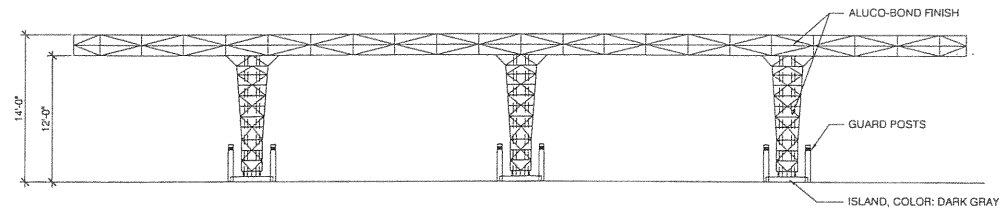
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CANOPY SIDE ELEVATION

SCALE  
1/8" = 1' - 0"

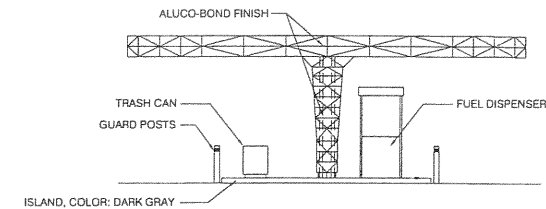
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CANOPY REAR ELEVATION

SCALE  
1/8" = 1' - 0"

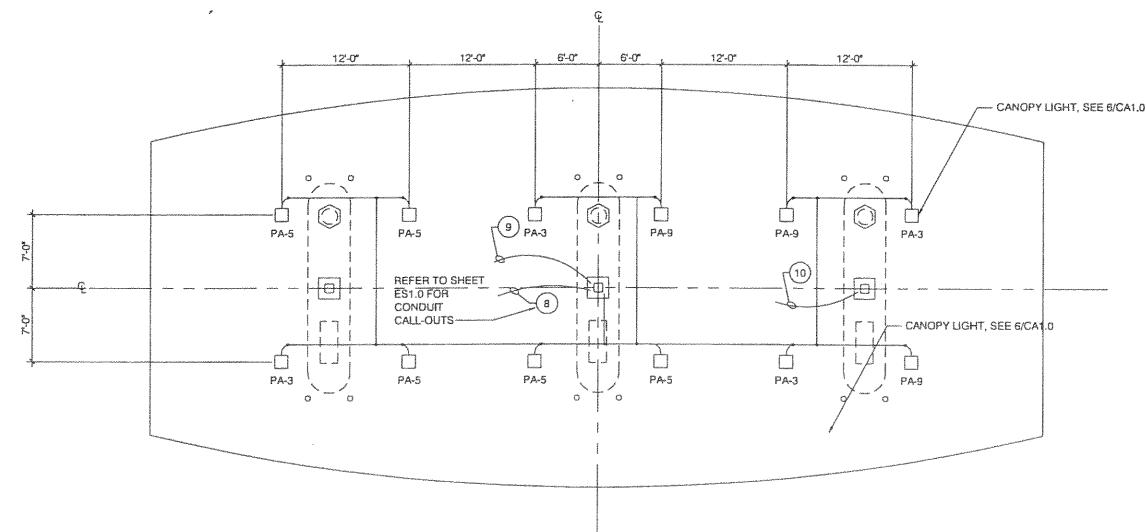
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CANOPY SIDE ELEVATION

SCALE  
1/8" = 1' - 0"

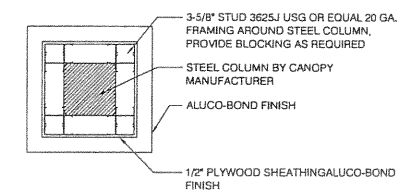
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CANOPY ELECTRICAL PLAN

SCALE  
1/8" = 1' - 0"

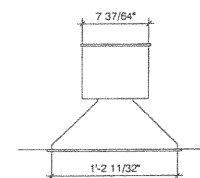
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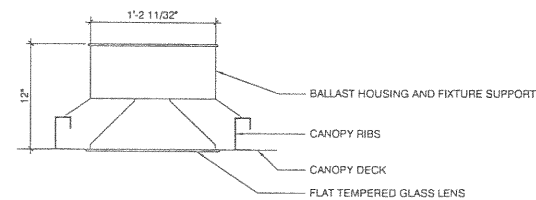
CANOPY COLUMN SECTION

SCALE  
3/4" = 1' - 0"

7



END VIEW



SIDE VIEW

CANOPY LIGHT (FLAT LENS SCOTTSDALE)

SCALE  
N.T.S.

6

Seal:

A Project for:

4975 SOUTH DAKOTA AVE, NE  
WASHINGTON, DC 20017

Client:

EUCLID OF VIRGINIA, LLC

PO BOX 9492  
WASHINGTON, DC 20016

Revisions:

No.	Description	Date
Project No.:	09.002.01	
Drawn By:	JW	
Reviewed By:	JW	
Scale:	AS NOTED	
Date:	01-26-09	
Filename:		
Sheet Title:		

CANOPY  
ELEVATIONS,  
ELECTRICAL PLAN  
AND  
DETAILS

Sheet #:

CA1.0

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## ROOM FINISH SCHEDULE

ROOM NO.	FLOOR	WALL	COVE BASE	CEILING
100 SALES		ARMSTRONG EXCELON 12"x12"x1/8" VINYL COMPOSITION TILE, # 51908 "PEWTER", SURFACE LEVELING COMPOUND REQUIRED TO PREPARE FLOOR	4" BLACK CONTINUOUS VINYL COVE BASE, ARMSTRONG OR EQUAL	48" X 24" 3/4" ARMSTRONG ACT. COLOR: WHITE, FINE FISSURED #1760 (SECOND LOOK)
101 CASHIER	SAME AS 100	PAINTED (WHITE), SEMI-GLOSS GYPSUM BOARD FULL HEIGHT	4" BLACK CONTINUOUS VINYL COVE BASE, ARMSTRONG OR EQUAL	48" X 24" 3/4" ARMSTRONG ACT. COLOR: WHITE, FINE FISSURED #1760 (SECOND LOOK)
102 JANITOR ROOM	SAME AS 100	F.R.P. (WHITE) FULL HEIGHT ON GYPSUM BOARD	4" BLACK CONTINUOUS VINYL COVE BASE, ARMSTRONG OR EQUAL	48" X 24" 3/4" ARMSTRONG ACT. COLOR: WHITE, FINE FISSURED #1760 (SECOND LOOK)
103 & 104 REST ROOM	SAME AS 100	F.R.P. (WHITE) FULL HEIGHT ON MOISTURE RESISTANT GYPSUM BOARD	6" BLACK VINYL INTEGRAL COVE BASE (CONTINUATION OF FLOOR COVER), ARMSTRONG OR EQUAL - WELD ALL SEAMS	48" X 24" X 3/4" (ITEM 870) ARMSTRONG ACT. COLOR: WHITE, FINE CLEAN ROOM VL. WET-FORMED MINERAL FIBER VINYL-FACED MEMBRANE
105 STORAGE ROOM	SAME AS 100	PAINTED (WHITE), SEMI-GLOSS GYPSUM BOARD FULL HEIGHT	4" BLACK CONTINUOUS VINYL COVE BASE, ARMSTRONG OR EQUAL	48" X 24" 3/4" ARMSTRONG ACT. COLOR: WHITE, FINE FISSURED #1760 (SECOND LOOK)

## ADA NOTES

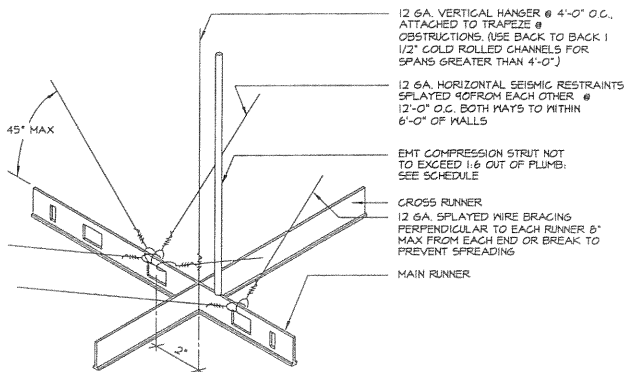
- THRESHOLDS AND FLOOR SURFACE TRANSITIONS SHALL BE 1/2" HIGH @ 2:1 SLOPE MAXIMUM.
- MAXIMUM DOOR OPENING EFFORTS SHALL BE 8.5 LBS. AT EXTERIOR, 5 LBS. AT INTERIOR AND 15 LBS. AT FIRE DOORS.
- SWITCHES/CONTROLS/THERMOSTATS SHALL BE INSTALLED MAXIMUM 48" ABOVE FLOOR.
- RECEPTACLE OUTLETS SHALL BE INSTALLED AT LEAST 15" ABOVE FLOOR. INSTALL AT LEAST (1) ONE FIRE EXTINGUISHER
- CONTRACTOR SHALL PROVIDE SIGNAGE AT THE EMERGENCY SHUT OFF SWITCH DISTINCTLY LABELED AS "EMERGENCY FUEL SHUTDOWN". SIGNS SHALL BE VISIBLE AT THE PUMP ISLAND IN APPROVED LOCATIONS.
- PROVIDE 80" HEADROOM TO ALL OVERHANGING OBSTRUCTIONS ALONG ANY PEDESTRIAN WALKWAYS.

## DOOR SCHEDULE

NO.	SIZE	HINGES	HARDWARE	DOOR MATERIAL	FRAME MATERIAL	REMARKS
1	NEW (2) 3'-0" X 7'-0" (ENTRANCE DRS)	MANUFACT.	KEY CYLINDER PUSH/PULL CLOSER PROVIDE THUMB TURN ON INTERIOR	ALUMINUM & GLASS	ALUMINUM	NEW HARDWARE AND ADA THRESHOLD SEE DOOR NOTE 4
2	NEW (1) 3'-0" X 7'-0" (JANITOR RM. DR.)	1-1/2 PR.	PASSAGE SET LEVER LATCH (D405) W/ CLOSER	1 3/4" S.C. WD.	H.M.	NEW KICK PLATE AND FLOOR STOPS
3	NEW (1) 3'-0" X 7'-0" (TOILET DOOR)	1-1/2 PR.	PRIVACY LEVER LOCK (D405) W/ CLOSER	1 3/4" S.C. WD.	H.M.	NEW KICK PLATE AND FLOOR STOPS
4	NEW (1) 3'-0" X 7'-0" (TOILET DOOR)	1-1/2 PR.	PRIVACY LEVER LOCK (D405) W/ CLOSER	1 3/4" S.C. WD.	H.M.	NEW KICK PLATE AND FLOOR STOPS
5	NEW (1) 3'-0" X 7'-0" (STORAGE RM. DR.)	1-1/2 PR.	PASSAGE SET LEVER LATCH (D405) W/ CLOSER	1 3/4" S.C. WD.	H.M.	NEW KICK PLATE AND FLOOR STOPS
6	NEW (1) 3'-0" X 7'-0" (EGRESS DOOR)	MANUFACT.	EXIT ALARM CLOSER W/ HOLD-OPEN AND THRESHOLD WEATHER-STRIPPING	H.M.	H.M.	NEW HARDWARE AS REQUIRED FOR NEW TENANT REQ. INSTALL 1800 VIEWER
7	NEW (2) 2'-6" X 7'-0" (ELEC. RM. DOOR)	MANUFACT.	816 STEEL STIFFENED DOOR WITH ASTRAGAL	H.M.	H.M.	NEW KICK PLATE AND FLOOR STOPS
8	NEW (2) 2'-6" X 7'-0" (ELEC. RM. DOOR)	MANUFACT.	816 STEEL STIFFENED DOOR WITH ASTRAGAL	H.M.	H.M.	NEW KICK PLATE AND FLOOR STOPS

## DOOR NOTES

- THE CONTRACTOR SHALL PROVIDE ALL NECESSARY LABOR, PARTS, AND SUPPORT PRODUCTS FOR A COMPLETE DOOR INSTALLATION. ALL DOOR HANDLE HARDWARE SHALL BE ADA ACCESSIBLE LEVER TYPE (SCHLAGE "ATHENS" STYLE OR EQUAL WITH 626 SATIN CHROME PLATED FINISH). DOOR HARDWARE INCLUDING DOOR CLOSERS SHALL MEET ADA REQUIREMENTS.
- ALL DOORS TO RECEIVE SILENCERS, AND KICK-PLATE ON THE PUSH SIDE.
- EXIT DOOR SHALL BE OPERABLE WITHOUT USE OF KEYS, SPECIAL KNOWLEDGE OR EFFORT.
- ADAMS RITE MNF CORP. NARROW STYLE RIM EXIT DEVICE 8800 SERIES LIFE SAFETY, 8801-36 (CLEAR ANODIZED FINISH WITH RIM CYLINDER LOCK 8738) - FOR ALUM AND GLASS DOOR APPLICATION
- REAR EXIT DOOR WITH SIRELOCK ALARM MODEL 250 BY ALARM LOCK SYSTEMS (631-785-4871). NON-REMOVABLE PINS ON HINGE AND NO VISIBLE HARDWARE OUTSIDE. - FOR HOLLOW METAL DOOR AND FRAME APPLICATION
- ALL DOORS SHALL HAVE A MINIMUM CLEAR OPENING WIDTH OF 32".
- ALL DOOR LATCHES SHALL BE EQUIPPED WITH LEVER-TYPE HANDLES OR PANIC BARS. DOOR OPENING HARDWARE SHALL BE MOUNTED BETWEEN OR 30" OR 44" ABOVE FINISH FLOOR.
- FLOOR AREAS AT EACH SIDE OF ALL DOORS SHALL BE CLEAR AND LEVEL. MAXIMUM THRESHOLD HEIGHTS SHALL BE 1/2".



COMPRESSION STRUT	LENGTH	DIAMETER
UP TO 4'-0"	5/4"	1"
5'-0"	1"	1-1/2"
6'-0"	1-1/2"	1-1/2"

## NOTES:

- ALL HANGERS & SUPPORTS TO BE ATTACHED WITH 4 TURNS IN 1 1/2" (TYP.)
- SUPPORT EACH LIGHT FIXTURE INDEPENDENTLY OF CEILING GRID WITH 1-#12 WIRE AT TWO CORNERS DIAGONALLY ACROSS FROM ONE ANOTHER.
- ALL MAIN AND CROSS TEES, THEIR SPLICES AND INTERSECTING CONNECTIONS SHALL BE ADEQUATE TO RESIST 12 LBS. IN TENSION.

## SUSPENDED CEILING SUPPORT

DETAIL

SCALE

3" = 1' - 0"

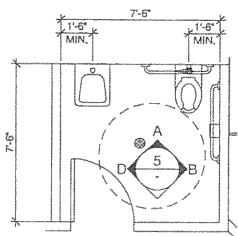
4

## RESTROOM ACCESSORIES KEYED NOTES

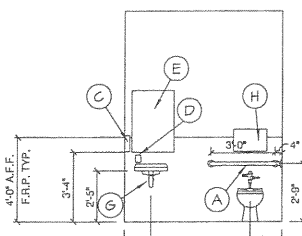
- A 36" MIN. GRAB BAR  
B 42" MIN. GRAB BAR  
C PAPER TOWEL DISPENSER  
D SOAP DISPENSER  
E 24" X 36" MIRROR  
F SURFACE MOUNTED TOILET PAPER DISPENSER  
G INSULATED DRAIN FOR LAVATORY  
H TOILET SEAT COVER DISPENSER

## NOTE:

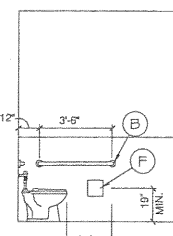
- PROVIDE SOLID BLOCKING OR OTHER SUITABLE BACKING AT LOCATIONS INCLUDING, BUT NOT LIMITED TO, THE FOLLOWING: EDGES WHERE FINISH MATERIALS CHANGE, GRAB BARS, TOILET PARTITIONS, DOOR STOPS, SHELF BRACKETS, HANDRAILS AND ALL MOUNTED EQUIPMENT, INCLUDING EQUIPMENT FURNISHED BY OWNER. PROVIDE STEEL BACKING FOR GRAB BARS ATTACHED TO PARTITIONS. SEE SPECIFICATIONS FOR LOADING RESISTANCE REQUIRED.



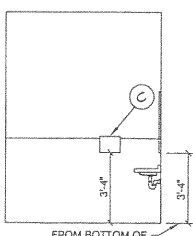
PLAN



A ELEVATION



B ELEVATION



C ELEVATION

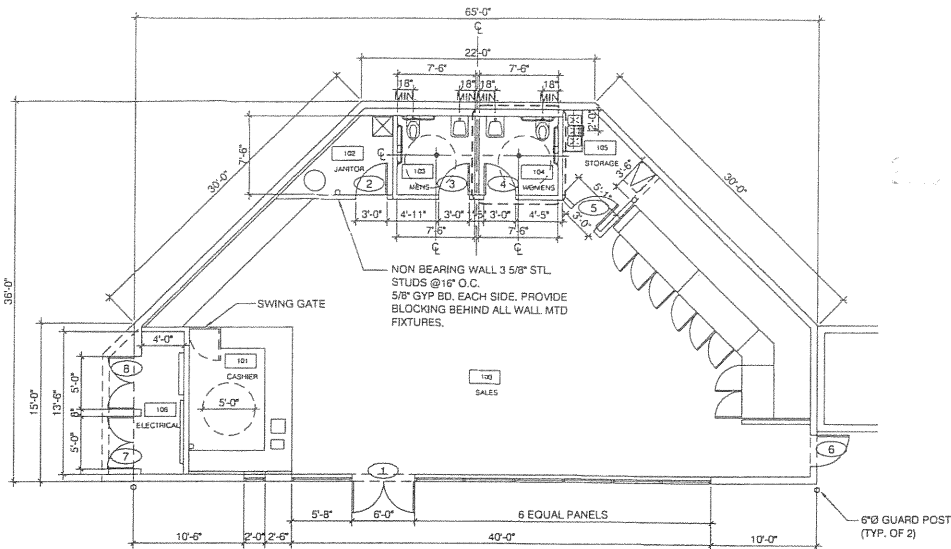
## ENLARGED RESTROOM PLAN AND ELEVATIONS

PLAN &amp; ELEVATIONS

SCALE

1/4" = 1' - 0"

5

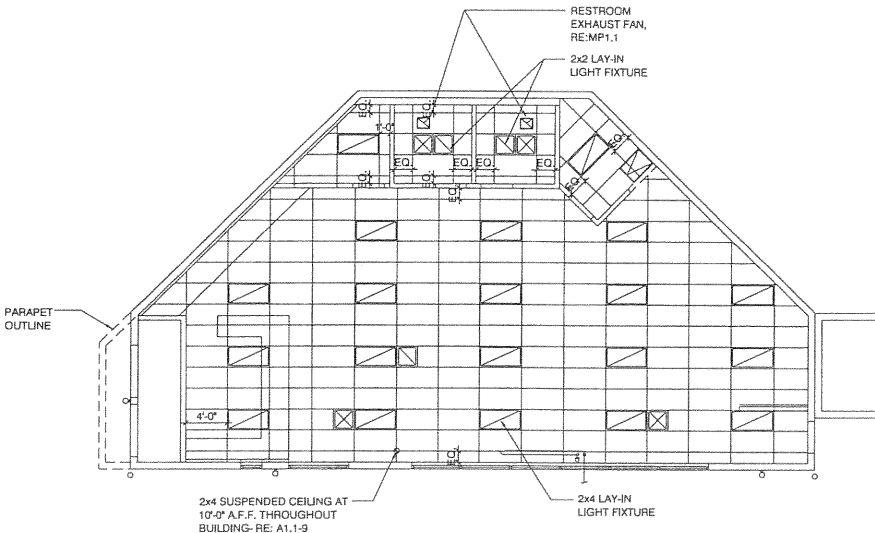


## KIOSK FLOOR PLAN

SCALE

1/8" = 1' - 0"

1



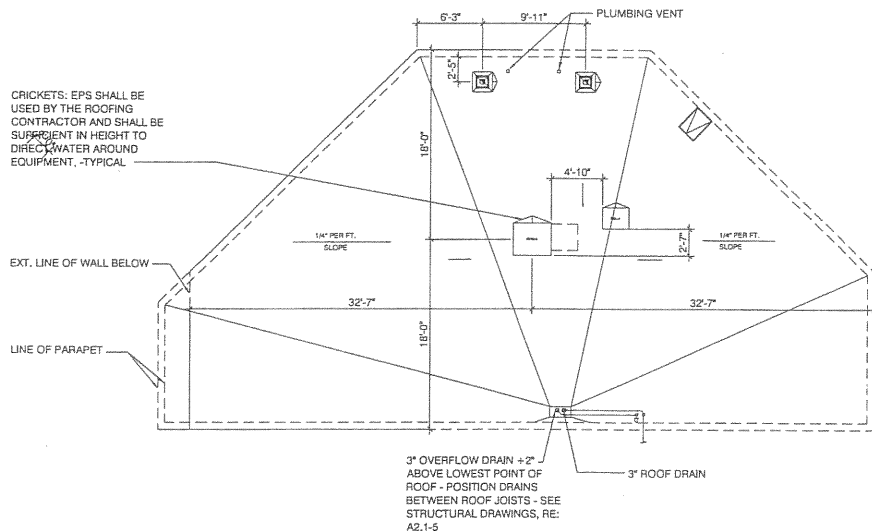
## KIOSK REFLECTED CEILING PLAN

DETAIL

SCALE

1/8" = 1' - 0"

2



## KIOSK ROOF PLAN

DETAIL

SCALE

1/8" = 1' - 0"

3

1400 W STREET, NW, WASHINGTON, DC 20009  
202.550.1032

Seal:

A Project for:

4975 SOUTH DAKOTA AVE, NE  
WASHINGTON, DC 20017

Client:

EUCLID OF VIRGINIA, LLC

PO BOX 9492  
WASHINGTON, DC 20016

Revisions:

No.	Description	Date
Project No.:	09.002.01	
Drawn By:	JW	
Reviewed By:	JW	
Scale:	AS NOTED	
Date:	01-26-09	
Filename:		
Sheet Title:		

KIOSK  
FLOOR PLAN,  
REFLECTED CEILING PLAN  
AND  
ROOF PLAN

Sheet #:

A-1.0

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Department of Consumer and Regulatory Affairs  
 Building and Land Regulation Administration  
 394 North Capitol Street N.E. Room 2100  
 Washington D.C. 20002  
 Tel: (202) 442-4470 Fax: (202) 442-4862

Government  
 of the District  
 of Columbia  
 B.L.R.A. 91A

# C of O

## CERTIFICATE OF OCCUPANCY

PERMIT NO.

**CO 102500**

THIS PERMIT IS VALID ONLY FOR THE PREMISES  
 OF THE PROJECT ADDRESS

DATE : 8/5/2005

ADDRESS <b>4975 SOUTH DAKOTA AVE NE</b>	FLOOR(S) <b>1ST FL.</b>	PRCLID: <b>3899</b> <small>(square)</small>	<b>-0000-</b>	<b>0076</b> <small>(lot)</small>
		WARD: <b>5</b>	ZONE: <b>C1</b>	

PERMISSION IS HEREBY GRANTED TO: <b>SOLE PROPRIETOR: KOO YUEN</b>	TRADING AS: <b>CHECK N GO</b>
--	----------------------------------

APPROVED USES: <b>OTHER - SEE DESCRIPTION</b>	PREVIOUS USES: <b>CONVENIENCE STORE</b>
--	--

TYPE: <b>USE CHANGE</b>	BZA NO.:	OCCUPIED SQ. FOOTAGE: <b>1,000</b>	OCCUP. LOAD:	EXPIRATION DATE: <b>NONE</b>
----------------------------	----------	---------------------------------------	--------------	---------------------------------

DESCRIPTION OF USE: <b>CHECK N GO. THE ADDRESS IS 4975A SOUTH DAKOTA AVE N.E.</b>	FEE: <b>\$75.00</b>
--	------------------------

THIS CERTIFICATE SHALL BE POSTED CONSPICUOUSLY ON THE ABOVE PREMISES AT ALL TIMES. IT IS VALID INDEFINITELY, unless an expiration date is stated, VALID ONLY for the premise at the above address or part thereof, and for the purpose(s) indicated above, and IS NOT TRANSFERABLE to another person or premises under ANY conditions. ANY CHANGE in the type of business, ownership of business, or part of premises used therefor, will render this Certificate VOID and a NEW Certificate must be obtained.

David A. Clark DIRECTOR	PERMIT CLERK: <b>THINGOC MACXOAN</b>
----------------------------	---

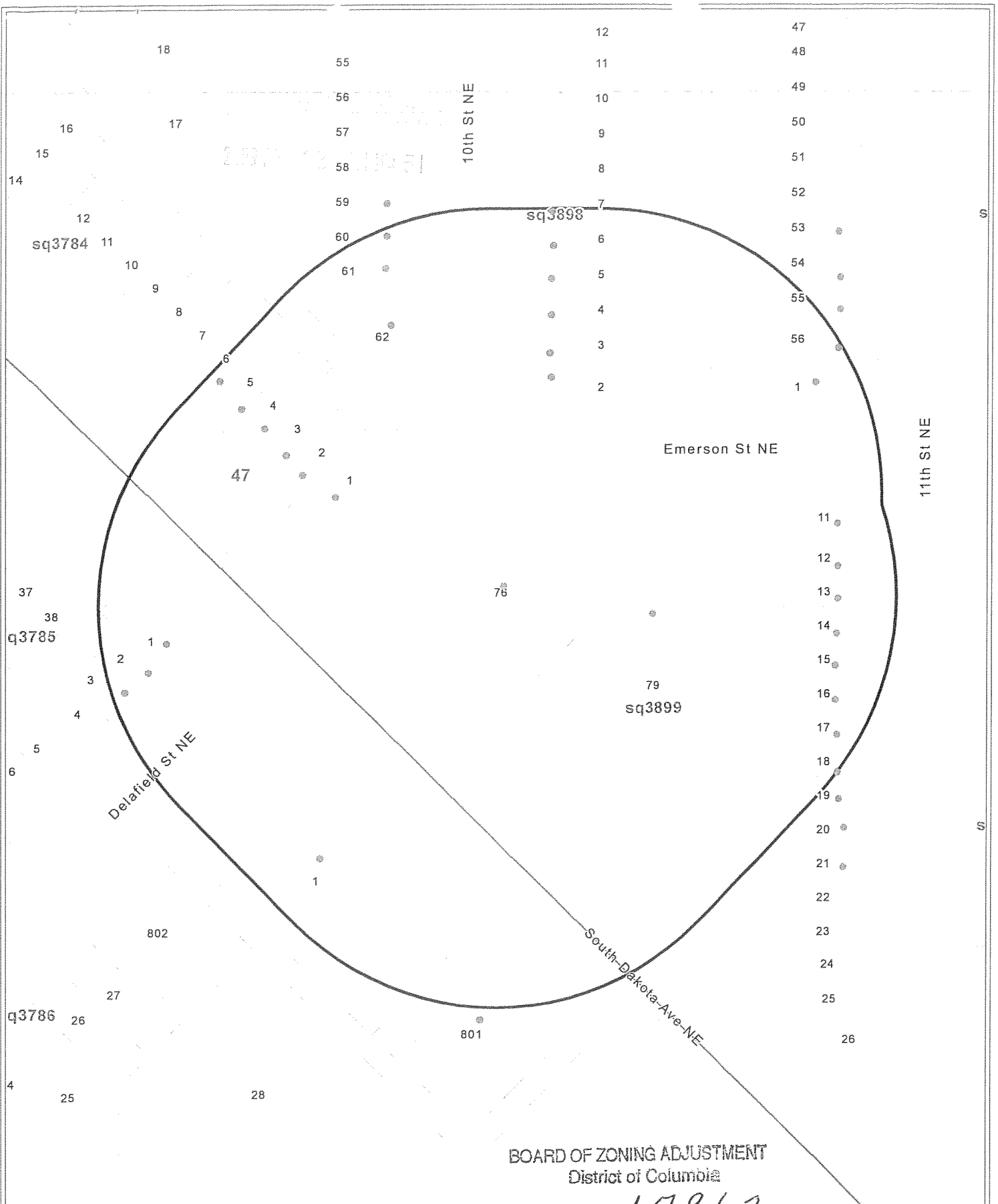
BOARD OF ZONING ADJUSTMENT  
 District of Columbia

CASE NO.

**17963**

EXHIBIT NO.

**8**



BOARD OF ZONING ADJUSTMENT  
District of Columbia

# Square 3899 Lot 0076

CASE NO. 17963

EXHIBIT NO. 7

- Property Lines
- 200 Foot Radius

\*\*\* Office of Tax and Revenue  
OTR Real Property Assessment Division  
GIS Geographic Information Systems



Map Created: 04-13-2009  
For planning purposes only

Square Suffix and Lot: 3898- 0001 \* Lot Address: 5000 11TH ST NE  
Owner: JAMES O MAULDIN \* Owner's Address: 5000 11TH ST NE WASHINGTON, DC 20017-2848

Square Suffix and Lot: 3898 0002 \* Lot Address: 5001 10TH ST NE  
Owner: VELITA LEE \* Owner's Address: 5001 10TH ST NE WASHINGTON, DC 20017-2845

Square Suffix and Lot: 3898 0003 \* Lot Address: 5003 10TH ST NE  
Owner: JAMES M CORNISH \* Owner's Address: 7409 16TH PL HYATTSVILLE, MD 20783-4324

Square Suffix and Lot: 3898 0004 \* Lot Address: 5007 10TH ST NE  
Owner: LORENA B THOMAS \* Owner's Address: 5007 10TH ST NE WASHINGTON, DC 20017-2845

Square Suffix and Lot: 3898 0005 \* Lot Address: 5009 10TH ST NE  
Owner: FAYE B DEAL \* Owner's Address: 4883 SELDON WAY SE SMYRNA, GA 30080-9265

Square Suffix and Lot: 3898 0006 \* Lot Address: 5013 10TH ST NE  
Owner: WENDY HARRISON \* Owner's Address: 5013 10TH ST NE WASHINGTON, DC 20017-2845

Square Suffix and Lot: 3898 0007 \* Lot Address: 5015 10TH ST NE  
Owner: PERCY E LOVE \* Owner's Address: 5015 10TH ST NE WASHINGTON, DC 20017-2845

Square Suffix and Lot: 3898 0053 \* Lot Address: 5012 11TH ST NE  
Owner: JOHN H SAMUEL \* Owner's Address: 5012 11TH ST NE WASHINGTON, DC 20017-2848

Square Suffix and Lot: 3898 0054 \* Lot Address: 5008 11TH ST NE  
Owner: DOROTHY M SMITH \* Owner's Address: 5008 11TH ST NE WASHINGTON, DC 20017-2848

Square Suffix and Lot: 3898 0055 \* Lot Address: 5006 11TH ST NE  
Owner: VALIDA WALKER \* Owner's Address: 5006 11TH ST NE WASHINGTON, DC 20017-2848

Square Suffix and Lot: 3898 0056 \* Lot Address: 5002 11TH ST NE  
Owner: WILLIAM C REED \* Owner's Address: 5002 11TH ST NE WASHINGTON, DC 20017-2848

Square Suffix and Lot: 3899 0011 \* Lot Address: 4944 11TH ST NE  
Owner: CELIA A PRICE \* Owner's Address: 4944 11TH ST NE WASHINGTON, DC 20017-2846

Square Suffix and Lot: 3899 0012 \* Lot Address: 4942 11TH ST NE  
Owner: SHERLYN R ANDERSON \* Owner's Address: 4942 11TH ST NE WASHINGTON, DC 20017-2846

Square Suffix and Lot: 3899 0013 \* Lot Address: 4938 11TH ST NE  
Owner: RENE O PINEDA \* Owner's Address: 4938 11TH ST NE WASHINGTON, DC 20017-2846

Square Suffix and Lot: 3899 0014 \* Lot Address: 4936 11TH ST NE  
Owner: ROBERT L RIKARD \* Owner's Address: 4936 11TH ST NE WASHINGTON, DC 20017-2846

Square Suffix and Lot: 3899 0015 \* Lot Address: 4932 11TH ST NE  
Owner: LISA C NEWMAN \* Owner's Address: 4932 11TH ST NE WASHINGTON, DC 20017-2846

Square Suffix and Lot: 3899 0016 \* Lot Address: 4930 11TH ST NE  
Owner: ZERLINE WILSON \* Owner's Address: 4930 11TH ST NE WASHINGTON, DC 20017-2846

Square Suffix and Lot: 3899 0017 \* Lot Address: 4926 11TH ST NE  
Owner: STELLA TORRENCE \* Owner's Address: 4926 11TH ST NE WASHINGTON, DC 20017-2846

Square Suffix and Lot: 3899 0018 \* Lot Address: 4924 11TH ST NE  
Owner: KATHI BRITT \* Owner's Address: 4924 11TH ST NE WASHINGTON, DC 20017-2846

Square Suffix and Lot: 3899 0019 \* Lot Address: 4920 11TH ST NE  
Owner: ALEXIS RICE C/O C/O UNIT 101 \* Owner's Address: 2115 YOUNG ST SE WASHINGTON, DC 20020-5336

Square Suffix and Lot: 3899 0020 \* Lot Address: 4918 11TH ST NE  
Owner: MOHAMED B SANKOH \* Owner's Address: 3300 16TH ST NW # 0031 WASHINGTON, DC 20010-2269

Square Suffix and Lot: 3899 0021 \* Lot Address: 4914 11TH ST NE  
Owner: GEORGE W MOORE \* Owner's Address: 4914 11TH ST NE WASHINGTON, DC 20017-2846

Square Suffix and Lot: 3899 0076 \* Lot Address: 4975 SOUTH DAKOTA AV NE  
Owner: 4975 SOUTH DAKOTA ASSOCIATES LTD \* Owner's Address: PO BOX 9492 WASHINGTON, DC 20016-9492

Square Suffix and Lot: 3899 0079 \* Lot Address: 4925 SOUTH DAKOTA AV NE  
Owner: GURVIN INVESTMENTS LLC \* Owner's Address: 4925 S DAKOTA AVE NE WASHINGTON, DC 20017-3927

Square Suffix and Lot: 3784 0001 \* Lot Address: 5001 SOUTH DAKOTA AV NE  
Owner: EMANUEL BROWN \* Owner's Address: 5001 S DAKOTA AVE NE WASHINGTON, DC 20017-2368

Square Suffix and Lot: 3784 0002 \* Lot Address: 5003 SOUTH DAKOTA AV NE  
Owner: JAMES A HOLLOWAY \* Owner's Address: 5003 S DAKOTA AVE NE WASHINGTON, DC 20017-2368

Square Suffix and Lot: 3784 0003 \* Lot Address: 5007 SOUTH DAKOTA AV NE  
Owner: ASHBY L DALTON \* Owner's Address: 5007 S DAKOTA AVE NE WASHINGTON, DC 20017-2368

Square Suffix and Lot: 3784 0004 \* Lot Address: 5009 SOUTH DAKOTA AV NE  
Owner: HERBERT C RODERICK \* Owner's Address: 5009 S DAKOTA AVE NE WASHINGTON, DC 20017-2368

Square Suffix and Lot: 3784 0005 \* Lot Address: 5013 SOUTH DAKOTA AV NE  
Owner: ELIZABETH MCCRAY \* Owner's Address: 5013 S DAKOTA AVE NE WASHINGTON, DC 20017-2368

Square Suffix and Lot: 3784 0006 \* Lot Address: 5015 SOUTH DAKOTA AV NE  
Owner: WANDA C FRANKLIN \* Owner's Address: 5015 S DAKOTA AVE NE WASHINGTON, DC 20017-2368

Square Suffix and Lot: 3784 0059 \* Lot Address: 5014 10TH ST NE  
Owner: JOCELYN K MUSHALA \* Owner's Address: 5014 10TH ST NE WASHINGTON, DC 20017-2844

Square Suffix and Lot: 3784 0060 \* Lot Address: 5012 10TH ST NE  
Owner: LAUVERN F WILLIAMS \* Owner's Address: 5012 10TH ST NE WASHINGTON, DC 20017-2844

Square Suffix and Lot: 3784 0061 \* Lot Address: 5008 10TH ST NE  
Owner: DEBORAH D THOMAS \* Owner's Address: 5008 10TH ST NE WASHINGTON, DC 20017-2844

Square Suffix and Lot: 3784 0062 \* Lot Address: 5006 10TH ST NE  
Owner: MARY R BRILEY \* Owner's Address: 5006 10TH ST NE WASHINGTON, DC 20017-2844

Square Suffix and Lot: 3785 0001 \* Lot Address: 0844 DELAFIELD ST NE  
Owner: LASALLE BANK NATIONAL ASSOCIATION C/O HOME LOAN SERVICES \* Owner's Address: PO BOX 1838 PITTSBURGH, PA 15230-1838

Square Suffix and Lot: 3785 0002 \* Lot Address: 0842 DELAFIELD ST NE  
Owner: SHEILA T HUTTON \* Owner's Address: 842 DELAFIELD ST NE WASHINGTON, DC 20017-3913

Square Suffix and Lot: 3785 0003 \* Lot Address: 0838 DELAFIELD ST NE  
Owner: KEITH O LEFTWICH \* Owner's Address: 838 DELAFIELD ST NE WASHINGTON, DC 20017-3913

Square Suffix and Lot: 3786 0001 \* Lot Address: 4950 SOUTH DAKOTA AV NE  
Owner: GOLDEN ARCH REALTY CORPORATION C/O MCDONALD'S CORP \* Owner's Address: PO BOX 182571 COLUMBUS, OH 43218-2571

Square Suffix and Lot: 3786 0801 \* Lot Address: 4940 SOUTH DAKOTA AV  
Owner: YUNG LI LLC \* Owner's Address: 6520 SUNNY HILL CT MC LEAN, VA 22101-1639

JAMES O MAULDIN  
5000 11TH ST NE  
WASHINGTON, DC 20017-2848

VELITA LEE  
5001 10TH ST NE  
WASHINGTON, DC 20017-2845

JAMES M CORNISH  
7409 16TH PL  
HYATTSVILLE, MD 20783-4324

LORENA B THOMAS  
5007 10TH ST NE  
WASHINGTON, DC 20017-2845

FAYE B DEAL  
4883 SELDON WAY SE  
SMYRNA, GA 30080-9265

WENDY HARRISON  
5013 10TH ST NE  
WASHINGTON, DC 20017-2845

PERCY E LOVE  
5015 10TH ST NE  
WASHINGTON, DC 20017-2845

JOHN H SAMUEL  
5012 11TH ST NE  
WASHINGTON, DC 20017-2848

DOROTHY M SMITH  
5008 11TH ST NE  
WASHINGTON, DC 20017-2848

VALIDA WALKER  
5006 11TH ST NE  
WASHINGTON, DC 20017-2848

WILLIAM C REED  
5002 11TH ST NE  
WASHINGTON, DC 20017-2848

CELIA A PRICE  
4944 11TH ST NE  
WASHINGTON, DC 20017-2846

SHERLYN R ANDERSON  
4942 11TH ST NE  
WASHINGTON, DC 20017-2846

RENE O PINEDA  
4938 11TH ST NE  
WASHINGTON, DC 20017-2846

ROBERT L RIKARD  
4936 11TH ST NE  
WASHINGTON, DC 20017-2846

LISA C NEWMAN  
4932 11TH ST NE  
WASHINGTON, DC 20017-2846

ZERLINE WILSON  
4930 11TH ST NE  
WASHINGTON, DC 20017-2846

STELLA TORRENCE  
4926 11TH ST NE  
WASHINGTON, DC 20017-2846

KATHI BRITT  
4924 11TH ST NE  
WASHINGTON, DC 20017-2846

ALEXIS RICE C/O C/O UNIT 101  
2115 YOUNG ST SE  
WASHINGTON, DC 20020-5336

MOHAMED B SANKOH  
3300 16TH ST NW # 00316  
WASHINGTON, DC 20010-2269

GEORGE W MOORE  
4914 11TH ST NE  
WASHINGTON, DC 20017-2846

4975 SOUTH DAKOTA ASSOCIATES LTD  
PO BOX 9492  
WASHINGTON, DC 20016-9492

GURVIN INVESTMENTS LLC  
4925 S DAKOTA AVE NE  
WASHINGTON, DC 20017-3927

EMANUEL BROWN  
5001 S DAKOTA AVE NE  
WASHINGTON, DC 20017-2368

JAMES A HOLLOWAY  
5003 S DAKOTA AVE NE  
WASHINGTON, DC 20017-2368

ASHBY L DALTON  
5007 S DAKOTA AVE NE  
WASHINGTON, DC 20017-2368

HERBERT C RODERICK  
5009 S DAKOTA AVE NE  
WASHINGTON, DC 20017-2368

ELIZABETH MCCRAY  
5013 S DAKOTA AVE NE  
WASHINGTON, DC 20017-2368

WANDA C FRANKLIN  
5015 S DAKOTA AVE NE  
WASHINGTON, DC 20017-2368

JOCELYN K MUSHALA  
5014 10TH ST NE  
WASHINGTON, DC 20017-2844

LAUVERN F WILLIAMS  
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WASHINGTON, DC 20017-2844

DEBORAH D THOMAS  
5008 10TH ST NE  
WASHINGTON, DC 20017-2844

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842 DELAFIELD ST NE  
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KEITH O LEFTWICH  
838 DELAFIELD ST NE  
WASHINGTON, DC 20017-3913

GOLDEN ARCH REALTY CORPORATION  
C/O MCDONALD'S CORP  
PO BOX 182571  
COLUMBUS, OH 43218-2571

YUNG LI LLC  
6520 SUNNY HILL CT  
MC LEAN, VA 22101-1639





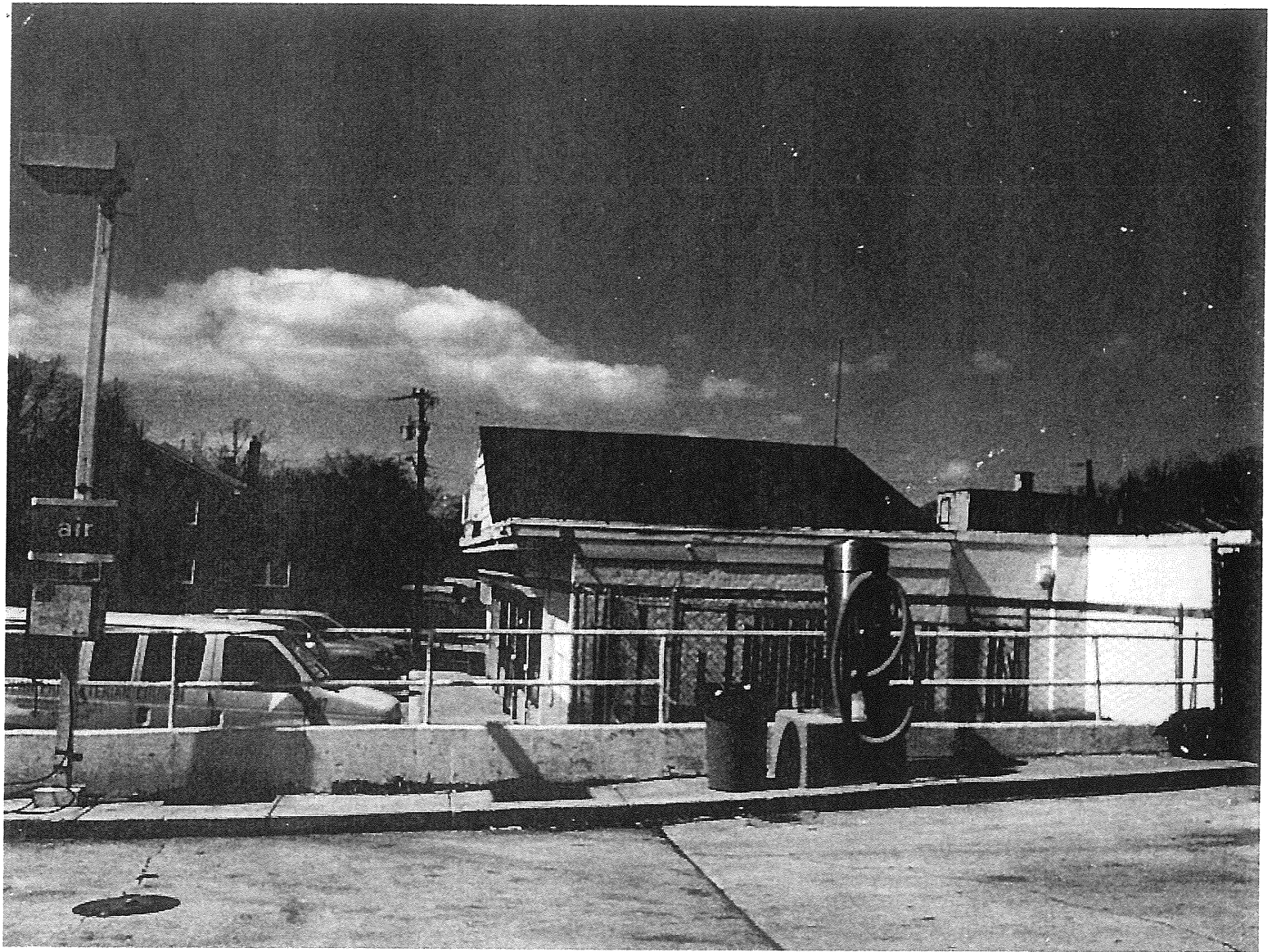
— 4975 South Dakota Ave, NE - Front —

BOARD OF ZONING ADJUSTMENT  
District of Columbia

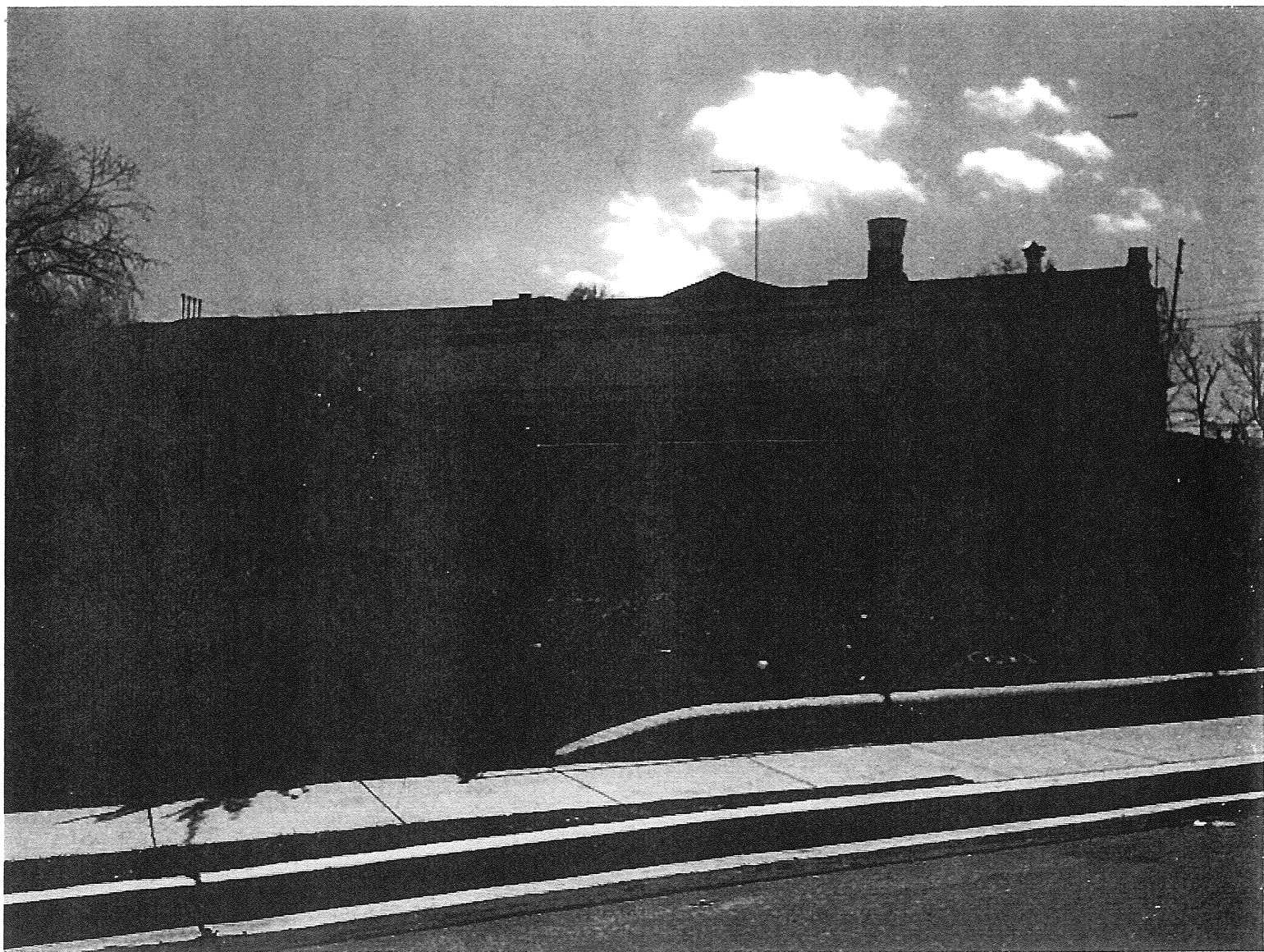
CASE NO. 17963

EXHIBIT NO. 6





— 4975 South Dakota Ave, NE - Side —




4975 South Dakota Ave, NE - Rear

Koo Yuen  
P.O. Box 9492  
Washington, DC 20016

AGENT ASSIGNMENT FOR 4975 SOUTH DAKOTA AVENUE, NE

Dear Sir or Madam:

The purpose of this letter is to assign Michael Goss as my agent for all matters regarding the Zoning Appeal and all DCRA related matters for the property known as 4975 South Dakota Avenue, NE, Washington, DC.

  
Koo Yuen, Owner  
4975 South Dakota Avenue, NE  
Washington, DC


4/16/2009  
Date

WASHINGTON  
DISTRICT OF COLUMBIA

*State of Maryland  
Montgomery County*

BEFORE ME, the undersigned authority, on this day personally appeared KOO YUEN, known to me to be the person whose name is subscribed to the foregoing instrument and acknowledged to me that he executed the same for the purposes and consideration therein expressed.

GIVEN UNDER MY HAND and seal of office this 16<sup>th</sup> day of April, 2009

  
Notary Public - Signature

*Thomas DeCun  
Notary Public, Maryland  
my commission expires 5/1/2010*

BOARD OF ZONING ADJUSTMENT  
District of Columbia

CASE NO. 17963

EXHIBIT NO. 5





## ZONING SELF-CERTIFICATION FORM



Project Address(es)	Square	Lot(s)	Zone District(s)	ANC(s)/Single Member
4975 South Dakota Avenue, NE	3899	76	C-1	5A03
4975 South Dakota Avenue, NE				
4975 South Dakota Avenue, NE				

## CERTIFICATION

The undersigned agent hereby certifies that the following zoning relief is required from the Board of Zoning Adjustment in this matter pursuant to:

Relief Sought	<input type="radio"/> §3103.2 - Use Variance	<input type="radio"/> §3103.2 - Area Variance	<input checked="" type="radio"/> §3104.1-Special Exception
Pursuant to Subsections			706

Pursuant to 11 DCMR §3113.2, the undersigned agent certifies that:

- (1) the agent is duly licensed to practice law or architecture in the District of Columbia;
- (2) the agent is currently in good standing and otherwise entitled to practice law or architecture in the District of Columbia; and
- (3) the applicant is entitled to apply for the variance or special exception sought for the reasons stated in the application.

The undersigned agent and owner acknowledge that they are assuming the risk that the owner may require additional or different zoning relief from that which is self-certified in order to obtain, for the above-referenced project, any building permit, certificate of occupancy, or other administrative determination based upon the Zoning Regulations and Map. Any approval of the application by the Board of Zoning Adjustment (BZA) does not constitute a Board finding that the relief sought is the relief required to obtain such permit, certification, or determination.

The undersigned agent and owner further acknowledge that any person aggrieved by the issuance of any permit, certificate, or determination for which the requested zoning relief is a prerequisite may appeal that permit, certificate, or determination on the grounds that additional or different zoning relief is required.

The undersigned agent and owner hereby hold the District of Columbia Office of Zoning and Department of Consumer and Regulatory Affairs harmless from any liability for failure of the undersigned to seek complete and proper zoning relief from the BZA.

The undersigned owner hereby authorizes the undersigned agent to act on the owner's behalf in this matter.

I/We certify that the above information is true and correct to the best of my/our knowledge, information and belief. Any person(s) using a fictitious name or address and/or knowingly making any false statement on this application/petition is in violation of D.C. Law and subject to a fine of not more than \$1,000 or 180 days imprisonment or both.  
(D.C. Official Code § 22-2405)

 Owner's Signature		Owner's Name (Please Print) Koo Yuen Park	
 Agent's Signature		Agent's Name (Please Print) Carlynn M. Fuller	
Date	4/16/2009	D.C. Bar No.	977427
		or	Architect Registration No.

## OFFICE OF ZONING DETERMINATION

(11 DCMR §3113.2)

Based upon review of the application and self-certification, this application is

<input type="checkbox"/>	Accepted for filing.
<input type="checkbox"/>	Referred to the Office of the Zoning Administrator, Department of Consumer and Regulatory Affairs, for determination of proper zoning relief required.
<input type="checkbox"/>	Rejected for failure to comply with the provisions of <input type="checkbox"/> 11 DCMR §3113.2; or <input type="checkbox"/> 11 DCMR - Zoning Regulations. Explanation _____
Signature	Date



District of Columbia Office of Zoning  
 441 4th Street, N.W. Ste. 200-S, Washington, D.C. 20001  
 (202) 727-6311 \* (202) 727-6072 fax \* www.dcoz.dc.gov \* dcoz@dc.gov

BOARD OF ZONING ADJUSTMENT

District of Columbia

CASE NO. 17963

4

INSTRUCTIONS

Any request for self-certification that is not completed in accordance with the following instructions shall not be accepted.

1. All self-certification applications shall be made on Form 135. All certification forms must be completely filled out (front and back) and be typewritten or printed legibly. All information shall be furnished by the applicant. If additional space is necessary, use separate sheets of paper to complete this form.
2. Complete one self-certification form for each application filed. Present this form with the Form 120 Application to the Office of Zoning, 441 4<sup>th</sup> Street, N.W., Suite 200-S, Washington, D.C. 20001.

ITEM	EXISTING CONDITIONS	MINIMUM REQUIRED	MAXIMUM ALLOWED	PROVIDED BY PROPOSED CONSTRUCTION	VARIANCE Deviation/Percent
Lot Area (sq. ft.)	15748	N/A	N/A	15748	0
Lot Width (ft. to the tenth)	120	54	N/A	120	0
Lot Occupancy (building area/lot area)	9363/15748 = .59	N/A	60%	.59	0
Floor Area Ratio (FAR) (floor area/lot area)	1800/15748 = .114	1.0	1.0	.114	0
Parking Spaces (number)	7	6	N/A	8	0
Loading Berths (number and size in ft.)	0	1	N/A	1	0
Front Yard (ft. to the tenth)	60	N/A	N/A	18	0
Rear Yard (ft. to the tenth)	20	20	N/A	20	0
Side Yard (ft. to the tenth)	15	6	N/A	15	0
Court, Open (width by depth in ft.)	N/A	N/A	N/A	N/A	0
Court, Closed (width by depth in ft.)	N/A	N/A	N/A	N/A	0
Height (ft. to the tenth)	16	N/A	40	16	0

Koo Yuen Park  
Owner

Euclid of Virginia, LLC  
Developer

4975 South Dakota Avenue, NE  
WASHINGTON, DC 20011

April 16, 2009

Narrative in Support of an Application for  
Special Exception from Section 706.1 and Section 3104.1

BOARD OF ZONING ADJUSTMENT  
District of Columbia

CASE NO. 17963

EXHIBIT NO. 3

2009 APR 22 PM 10:51

## DEVELOPMENT SUMMARY

Euclid of Virginia, LLC (Developer) is proposing to develop on behalf of Koo Yuen Park (Owner) a self-service gasoline station and convenience store (the "Development") at 4975 South Dakota Avenue, N.E., in Ward 5 (the "Site"). Euclid of Virginia, LLC is proposing to raze the existing convenience store and construct a new convenience store and a new self-service gasoline station. The Site is currently zoned C-1 Commercial.

### I. THE APPLICATION'S CONSISTENCY WITH THE GENERAL INTENT OF THE ZONING REGULATIONS

In applying for the Development, Owner is requesting a Special Exception approval by the Board of Zoning Adjustment (BZA) pursuant to 11 DCMR §3104.1 and §706.1, as required for all new self-service gasoline stations established or enlarged after May 12, 1958.

#### 1. The Special Exception

As mentioned above, Euclid of Virginia, LLC is requesting a special exception to construct a new self-service gasoline station with accessory convenience store located at 4975 South Dakota Avenue, N.E. in an area zoned C-1 Neighborhood Shopping District. Permitted as a matter-of-right is neighborhood shopping and low density development to a maximum lot occupancy of 60% for residential use, a maximum FAR of 1.0, and a maximum height of three (3) stories/forty (40) feet. Establishments offering the following services are matter of right permitted uses to include banks or other financial institutions; bars or cocktail lounges; barber or beauty shops; shoe repair shops; collection stations for dry-cleaning, dyeing, or laundry; dressmaking shops or establishments; frozen food lockers for family or individual use only; self-service gasoline stations existing on May 12, 1958; laundries, self-service, not exceeding twenty-five hundred square feet (2,500 ft.<sup>2</sup>) of gross floor area; laundries or dry cleaning establishments, not exceeding twenty-five hundred square feet (2,500 ft.<sup>2</sup>) of gross floor area; locksmiths; newspaper distribution stations; opticians and optometrists; radio or television repairs; shoeshine parlors; tailor shops or valet shops not exceeding twenty-five hundred square feet (2,500 ft.<sup>2</sup>) of gross floor area; and watch repair shops. (11 DCMR §701.1)

Retail services such as, art supplies stores; automatic ice delivery stations; automobile accessories sales, excluding installation; bakeries; provided, that any manufacture of bakery goods shall be limited to goods retailed on the premises; bicycle sales and repairs; book stores; camera or photographic supplies stores; cosmetics or toiletries stores; drug stores or pharmacies; electric appliance stores, including television and radio sales; flower stands or florist shops; food or grocery stores; gift shops; hardware stores; hobby shops, including the sale of toys; jewelry stores; restaurants, but not including fast food restaurants, drive-in restaurants, or food delivery services; music stores; newsstands; notions or novelty stores; off-premises alcoholic beverages sales; paint stores; sporting goods stores; stationery stores; tobacco products stores; and variety stores. (11 DCMR §701.4)

The proposed construction of a new self-service gasoline station and new convenience store is consistent with the intent of the C-1 Neighborhood Shopping District which is "designed to provide convenient retail and personal service establishments for the day-to-day needs of a small tributary area, with a minimum impact upon surrounding residential development." (11 DCMR §700.1) The proposed development of the self-service gasoline station and convenience store will incorporate a local serving commercial retail establishment that provides basic goods and services in a residential area within a walkable community.

The proposed convenience store is a matter of right use in the C-1 District and is not subject to BZA approval.

## II. THE SITE

The Site is located on the north side of South Dakota at 4975 South Dakota Avenue, N.E., within the C-1 Zone District and is bounded by 10<sup>th</sup> Street to the north, Emerson Street to the northeast, and South Dakota Avenue to the west. Euclid of Virginia, LLC is proposing to raze the existing convenience store and replace it with a newly constructed convenience store and construct a self-service gasoline station.

The site abuts a branded self-service gasoline station owned by BP Oil and is across the street from a Chinese restaurant, a fast food restaurant and a dry cleaning establishment and a beauty parlor.

The property is an irregular shaped site fronting South Dakota Avenue, Northeast. The lot size is 15,748 square feet with an existing building consisting of 1,895 square feet. The portion of the site adjoining South Dakota Avenue is a paved lot currently used for parking for customers of the convenience store. This parking area makes up the majority of the area of the lot.

The convenience store building will be 65 by 36 feet. The building will also contain the cashier's booth plus coolers, restrooms, and a storage area. The main food and sales area will measure approximately 1,800 square feet. There will be no food preparation on the site. The building will be centered on the site facing South Dakota Avenue. There will be three new pump islands each with one self-service multi-hose dispenser capable of serving one car on each side of the island. An 84 foot by 28 foot steel and Alucobond aluminum canopy will be constructed over the pump islands. The pumps and the canopy will be located in the center of the site and will be parallel to South Dakota Avenue. The canopy will be lighted and will shelter customers pumping gas in all weather conditions. The self-service gasoline station would be situated so as to permit a free flow of traffic onto the site for access to the proposed convenience store. The two existing entrances off of South Dakota Avenue and the one entrance off Emerson Street will remain in place. The existing curb cuts will be used and are all located more than 40 feet from the street intersection. No vehicular entrance or exit is connected with the street points closer than twenty-five (25) feet to any residential district.

There is presently very little landscaping along the Emerson Street boundary of the site which buffers the adjacent residential uses. Euclid of Virginia, LLC proposes to enhance the landscape buffer area and provide additional plantings in that area. Euclid of Virginia, LLC will also provide additional plantings to the landscaped area along South Dakota Avenue.

The owner of the property, who is the applicant here, is involved in managing a number of self-service gasoline stations. The applicant will experience significant unwarranted financial hardship if the Board of Zoning Adjustment does not grant a special exception permitting the self-service gasoline station to be constructed.

The building on the site will contain approximately 1,800 square feet of gross floor area and will therefore require six (6) parking spaces. Eight (8) parking spaces are shown on the site plan and all spaces are designed to be accessible at all times.



### **III. No Detriment to the Public**

The location of the proposed development is situated on a portion of South Dakota Avenue that is a high traffic densely developed neighborhood commercial district. This neighborhood, while served by a branded self-service gasoline station, is not serviced by an unbranded self-service gasoline station, such as the station which is proposed to be established on the site. Unbranded self-service gasoline stations can generally offer gasoline to the public at a retail rate that is lower than the branded stations can offer.

There is no aspect of the physical characteristics of the property which would inhibit traffic flow or cause any adverse impact with the traffic of South Dakota Avenue in front of the site.

### **IV. The Proposed Use will not Adversely Affect the Use of the Neighboring Property**

The subject site is surrounded by an R-2 District on all sides and is separated from that residential district by a street or alley. The nearest existing structure in the R-2 District to the west of the site is furthered buffered from the self-service gasoline station use by the landscaping at the western portion of the subject lot.

Residential properties to the west are separated from the subject site by 10<sup>th</sup> Street and residential properties to the north are separated from the subject site by an alley and Emerson Street.

The proposed development will not create any dangerous or otherwise objectionable traffic conditions. Patrons of the station will be drawn from vehicles already in the stream of traffic. The large area of the site provides for ample on-site vehicular circulation and safe pedestrian movement. The orientation of the three pump islands, all of which are parallel to South Dakota Avenue, will direct traffic away from the convenience store.

Lighting under the canopy area will be directed down to the general dispensing area and therefore confined to the site. There will also security lights attached to the poles at the rear of the site which will be confined to the site. Additional lights will be erected in the driveway areas for safety purposes and will be confined to the site. The only lighting on the fascia of the canopy will be on the side which faces South Dakota Avenue.

The proposed facility will offer twenty-four hour self-service gasoline, convenience foods, soft drinks and cigarettes. The structure will also contain storage areas, coolers and restrooms. There will be no food preparation on the premises. Vehicular repairs will not be offered at the subject site. There will be no grease pits or hoists.

The proposed development of the self-service gasoline station will add to the District's tax base by generating additional sales tax on the sale of gasoline and the other products to be sold at the station, income taxes as a result of providing employment and an increase in property taxes.

DISTRICT OF COLUMBIA GOVERNMENT  
OFFICE OF THE SURVEYOR

Washington, D.C., January 5, 2009

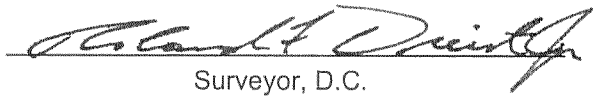
Plat for Building Permit of: SQUARE 3899 LOT 76

Scale: 1 inch = 20 feet Recorded in Book 138 Page 156

Receipt No. 00853

Furnished to: MICHAEL GOSS

I hereby certify that all existing improvements shown hereon, are completely dimensioned, and are correctly platted; that all proposed buildings or construction, or parts thereof, including covered porches, are correctly dimensioned and platted and agree with plans accompanying the application; that the foundation plans as shown hereon is drawn, and dimensioned accurately to the same scale as the property lines shown on this plat; and that by reason of the proposed improvements to be erected as shown hereon the size of any adjoining lot or premises is not decreased to an area less than is required by the Zoning Regulations for light and ventilation; and it is further certified and agreed that accessible parking area where required by the Zoning Regulations will be reserved in accordance with the Zoning Regulations, and that this area has been correctly drawn and dimensioned hereon. It is further agreed that the elevation of the accessible parking area with respect to the Highway Department approved curb and alley grade will not result in a rate of grade along centerline of driveway at any point on private property in excess of 20% for single-family dwellings or flats, or in excess of 12% at any point for other buildings. (The policy of the Highway Department permits a maximum driveway grade of 12% across the public parking and the private restricted property.)

  
Surveyor, D.C.

By: A.S. 

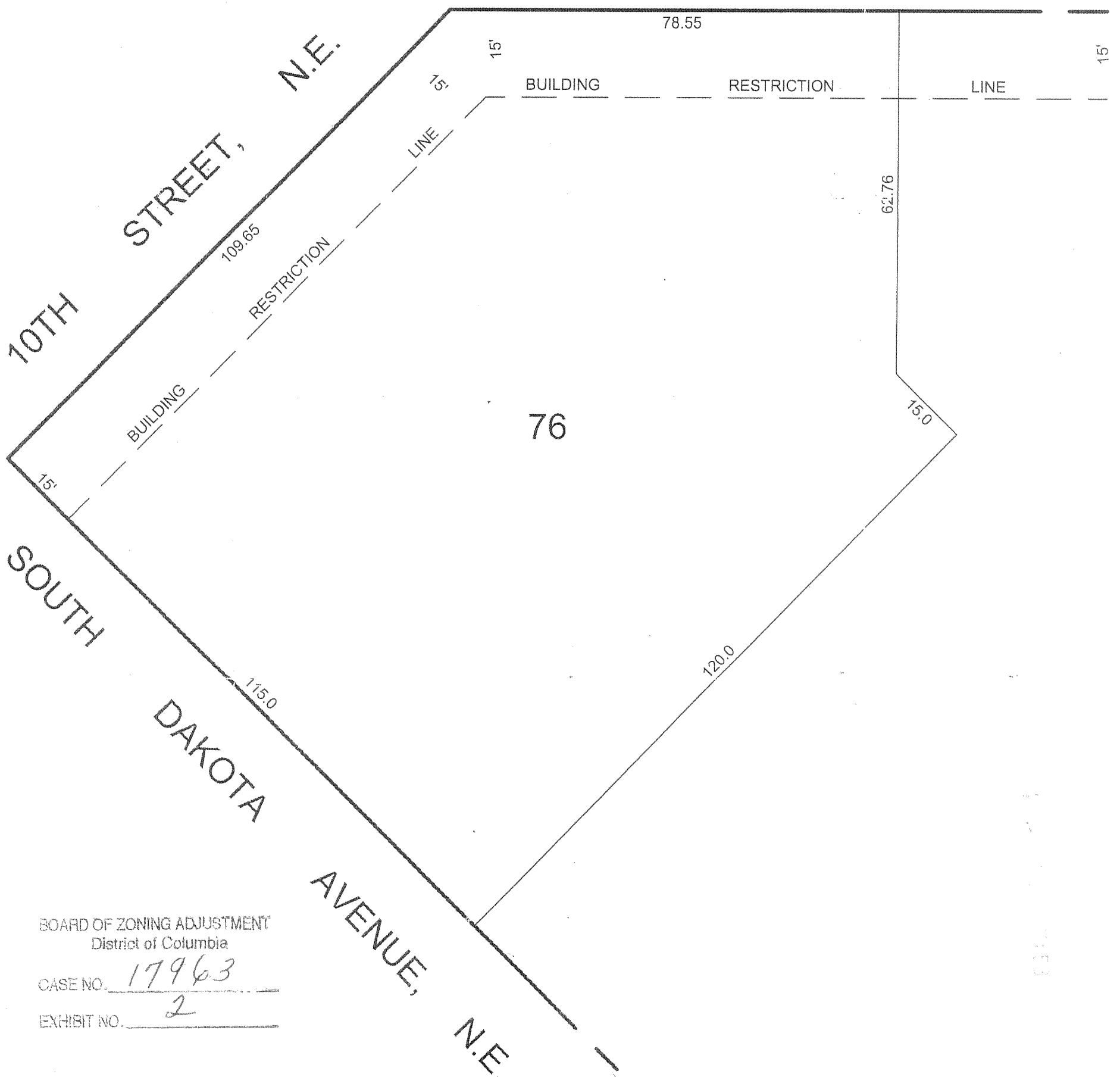
Date: 4-21-09



(Signature of owner or his authorized agent)

NOTE: Data shown for Assessment and Taxation Lots or Parcels are in accordance with the records of the Department of Finance and Revenue, Assessment Administration, and do not necessarily agree with deed description.

EMERSON STREET, N.E.



BOARD OF ZONING ADJUSTMENT  
District of Columbia

CASE NO. 17963

EXHIBIT NO. 2



**BEFORE THE BOARD OF ZONING ADJUSTMENT  
OF THE DISTRICT OF COLUMBIA**



**APPLICATION**

Before completing this form, please review the instructions on the reverse side.  
Print or type all information unless otherwise indicated.

Pursuant to Sections §3103.2 - Use Variance, §3103.2 - Area Variance and/or §3104.1 - Special Exception of Title 11 DCMR-  
Zoning Regulations an application is hereby made, the details of which are as follows:

Address(es)	Square	Lot No(s).	Zoning Districts	Relief Being Sought Area Variance • Use Variance Special Exception	Section No(s).
4975 South Dakota Ave NE	3899	76	C-1	Special Exception	3104.1, 706.1

Present use(s) of Property: Convenience Store

Proposed use(s) of Property: Self-Service Gasoline Station and Convenience Store

Owner of Property: Koo Yuen Park

Telephone No:

Address of Owner: P.O. Box 9492 Washington, DC 20016

Written paragraph specifically stating the "who, what, and where of the proposed action(s)". This will serve as the Public

Hearing Notice: Euclid of Virginia, LLC is proposing to raze the existing convenience store and build a new convenience store and self-service gasoline station consisting of three new pump islands each with one self-service multi-hose dispenser capable of serving one car on each side of the island. A aluminum and steel canopy will be constructed to cover the pump islands.

Estimated construction cost \$ 1,400,000

Advisory Neighborhood Commission  
Single-Member District(s)

**5A03**

I/We certify that the above information is true and correct to the best of my/our knowledge, information and belief. Any person(s) using a fictitious name or address and/or knowingly making any false statement on this application/petition is in violation of D.C. Law and subject to a fine of not more than \$1,000 or 180 days imprisonment or both.  
(D.C. Official Code § 22-2405)

Date:

4.21.09

Signature:

[Signature] Applicant \*

\* The Owner of the Property for which the application is made or his/her authorized agent. In the event an authorized agent files an application on the behalf of the Owner, a letter signed by the Owner authorizing the agent to act on his/her behalf shall accompany the notice of application.

To be notified of hearing and decision (Owner or Authorized Agent\*):

Name: Michael Goss

Address: 1400 W Street, NW, Washington, DC 20009

Phone No.: 202-550-1032

Fax No.:

E-Mail:

michaelgoss@mac.com

**ANY APPLICATION THAT IS NOT COMPLETED IN ACCORDANCE WITH THE INSTRUCTIONS ON THE BACK OF THIS FORM WILL NOT BE ACCEPTED.**

CASE NO. 17963

## INSTRUCTIONS

Any notice of application for action provided in the District of Columbia Zoning Regulations (11 DCMR Zoning) that is not completed in accordance with the following instructions shall not be accepted.

1. All applications shall be made on Form 120. All applications must be completely filled out and be typewritten or printed. If additional space is necessary, use separate sheets of 8½" x 11" paper to complete the form (drawings and plans may be no larger than 11"x17").
2. Present this form and supporting documents, in person, to the Office of Zoning at 441 4<sup>th</sup> Street, N.W., Suite 200-S, Washington, D.C. 20001.
3. At the time of filing an application before the Board of Zoning Adjustment, the Petitioner shall pay a filing fee in accordance with the Board of Zoning Adjustment Schedule of Fees – 11 DCMR §3180. (Check or money order is payable to the "DC Treasurer"; cash will not be accepted.)
4. **REQUIRED information to be submitted when making an application submission, includes one (1) original and twenty (20) copies of the following (except for labels of which (1) original and (1) copy is required):**
  - A. All applicants shall obtain from the Zoning Administrator (DCRA) a memorandum directing the applicant to the Board of Zoning Adjustment prior to filing this application or file through self-certification using Form 135. For self-certification, a licensed architect or attorney must certify the proposed relief requested.
  - B. A plat, drawn to scale and certified by a DC licensed survey engineer or the D.C. Office of the Surveyor showing boundaries and dimensions of the existing and proposed buildings and accessory buildings. Also required are architectural plans and elevations in sufficient detail to clearly illustrate any proposed building to be erected or altered, proposed landscaping/screens and building materials. Submittal of plat and plans shall not exceed 11" x 17" in size. (See Form 130 – Plat, Plan and Elevation Specifications for the required information on these drawings.)
  - C. A detailed statement of existing and intended use of such building or part of building.
  - D. A detailed statement explaining how the application meets the specific tests identified in the Zoning Regulations for variance (area and/or use), special exception or other specific relief being sought. (See Form 121 – Applicant's Burden of Proof for Variance and Special Exception Applications.)
  - E. Three color photograph views of appropriate size, not-to-exceed 8½" x 11", showing pertinent features of the building and the property involved (front, rear and sides, if possible).
  - F. The names and mailing addresses of the owners of all property within 200 feet, in all directions from all boundaries of the property involved in the application, **and self-stick labels of the names and mailing addresses of the owners of the properties.** (Note: This information is most readily available from the D.C. Department of Tax and Revenue, Tax Assessors, 941 North Capitol Street, N.E., 1<sup>st</sup> Floor, Washington, D.C. 20002.)
  - G. The name and mailing address of any person who has a lease with the owner for all or part of any building located on the property involved in the application.
  - H. Documentation or a copy of the Certificate of Occupancy showing the current authorized use. In cases where a change in one conforming use to another non-conforming use is requested, provide a copy of the past authorized uses.

**Note:** All applications are referred for review and recommendation to the D.C. Office of Planning (OP) and the Advisory Neighborhood Commission within which the affected property is located. Their reports are given "great weight" in the BZA decision-making process. Applicants are strongly recommended, at the time of filing a Form 120 application, to make contact with these agencies to discuss the merits of their application. OP can be reached at (202) 442-7600.



If you need a reasonable accommodation for a disability under the Americans with Disabilities Act (ADA) or Fair Housing Act, please complete a Request for Reasonable Accommodation form.

**District of Columbia Office of Zoning**  
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